

VICTORIAN RAILWAYS

NEWS LETTER

JANUARY

VR

1969



V.R. CHAIRMAN COMMENTS ON

AN UNACKNOWLEDGED RAILWAY SUBSIDY

If asked why there was a deficit of \$11½ million last year, railwaymen can point to something that is often overlooked when railway finances are being discussed—the fare concessions given to many passengers at the expense of the Railways.

Some of the main concessions are:

- Students periodical tickets (country and suburban) for travel to schools, colleges, etc., at fares ranging from one-third to two-thirds of the adult fare; for travel to Technical Schools, Education Department's Training Classes, Intermediate, Leaving, or Matriculation examinations, return tickets for half-fare (country and suburban); for vacation travel, country fares ranging from one-third adult single fare to return tickets at single adult fare; for weekend travel, country return tickets for two-thirds adult single fare or adult single fare (according to age of student);
- Trainee nurses (male and female) weekly and annual leave, return country tickets at single fare, and concession

fares for certain suburban travel; at beginning or completion of training, country single tickets at half fare;

- Blind persons free travel on the suburban system; on country lines, single tickets for two-thirds fare, return tickets at ordinary single fare plus one-third, and free travel for attendant.

Railway revenue is not recouped for any of the above reductions, and as they are special benefits given to sections of the community, should they not be charged to the general revenue of the State?

- Pensioners, half fare.
- Although an amount is received by the Railways from the Treasury for this concession, it does not represent the full cost of the reduction. (Incidentally, pensioners get no concessions on most private buses or air lines.) It can be held that this concession has been given as a measure of social justice, and that from the community viewpoint it is well justified. This may be so, but surely such social service benefits should be a charge against the Commonwealth Government and not the Railways.*

No. 4 Shed

Another stage in the \$11 million modernization of the Melbourne yard was reached on January 2, when the rehabilitated No. 4 Shed, instead of No. 3 Shed, began to receive inwards freight from the country. The length of No. 4 Shed had been more than doubled—from 510 ft. to 1,350 ft.

As a result of the modernization work, roofing covers an outside unloading track and roadways on both sides, to protect freight while being transferred to the road vehicles. The floor area is completely unobstructed by columns, and about 5 per cent. is under transparent roof. At the south end of the shed, a modern office block houses the staff.

Good foundation in this region of Melbourne is reached only at a depth of 90 ft. and the new shed has

been built on piles sunk to that depth.

Rebuilding of No. 3 Shed will start soon. Without walls, it will have a roof 109 ft. wide, giving complete protection from the weather.

Locos for hump yard

The first of five T class diesel-electric locomotives, with modified controls to permit low-speed shunting in the hump yard, was delivered last month.

These 950-1,050 h.p. diesels will be suitable for the low-speed operations necessary to uncouple and classify wagons at the crest of the hump, 20 ft. above general ground level in the modernized yard. They will push trains up the hump at a speed of 1½ to 2 m.p.h.

To increase adhesion between rail and wheels, the locomotives have a weight of about 80 tons—12 tons

more than similar diesels of this class. The diesels can also be used to haul main-line trains when not in use in the hump yard.

The five diesels are part of the order for 11 T class locomotives built by Clyde Engineering Co. Pty. Ltd. of Granville, N.S.W.

Flashing light signals

Automatic flashing light signals with warning bells to give added protection to motorists are to be installed at another 37 country level crossings, at the rate of about five every two months.

Tenders have been invited for the design and installation of the equipment at an expected cost of more than \$300,000. Installation is expected to start in March, and the entire programme will be completed about June 1970. The works will be paid for from the special level crossing fund that is financed from motor registration payments.

There are about 400 accidents every year at V.R. level crossings, but of these, only about 20 per cent. involve collisions between trains and road vehicles.

The majority of accidents—about 80 per cent.—are caused by road vehicles crashing into gates or boom barriers, and damaging fences at level crossings, many of which are equipped with flashing lights and warning bells.

Future level crossing accidents in such cases could result in legal action being instituted to recover the cost of repairs to railway property.

A lot of wire

It has been estimated that if anyone unpicked all the single wires in the electric cables used for the Melbourne Yard scheme, and put them end to end along the tracks, they would extend for about 1,700 miles, say to the far side of the Nullarbor Plain—and a fit place to end for anyone crazy enough to do the job.

FRONT COVER

Record tonnages are being hauled on the Melbourne-Adelaide fast freight service. From Melbourne to Adelaide there are, on the average, 19 express freight trains each week; from Adelaide there are 18. The front cover shows No. 133 fast goods to Adelaide crossing a bridge near Bacchus Marsh.

WEST TOWER OPENS

On December 2, the West Tower and Melbourne Yard's new arrival yard were officially opened by the Hon. Vernon F. Wilcox, M.P., Minister of Transport. This marked the completion of the first major stage in Melbourne Yard's \$11 million modernization scheme.



In the signal box at West Tower, Mr. Commissioner L. A. Reynolds (nearest to camera) indicates to Mr. Wilcox some features of the push-button signalling system.

The Minister, Commissioners, senior railway officers, representatives of the Press, TV, radio, and contractors associated with the project, travelled from Spencer Street in a rail motor through the Melbourne Yard. A complete tour of the yard was made during the morning, in the course of which a detailed commentary on the modernization scheme, and an explanation of its principal features were given by Mr. Commissioner L. A. Reynolds through a public address system installed in the rail motor. (The investigation into the deficiencies of Melbourne Yard, and the recom-

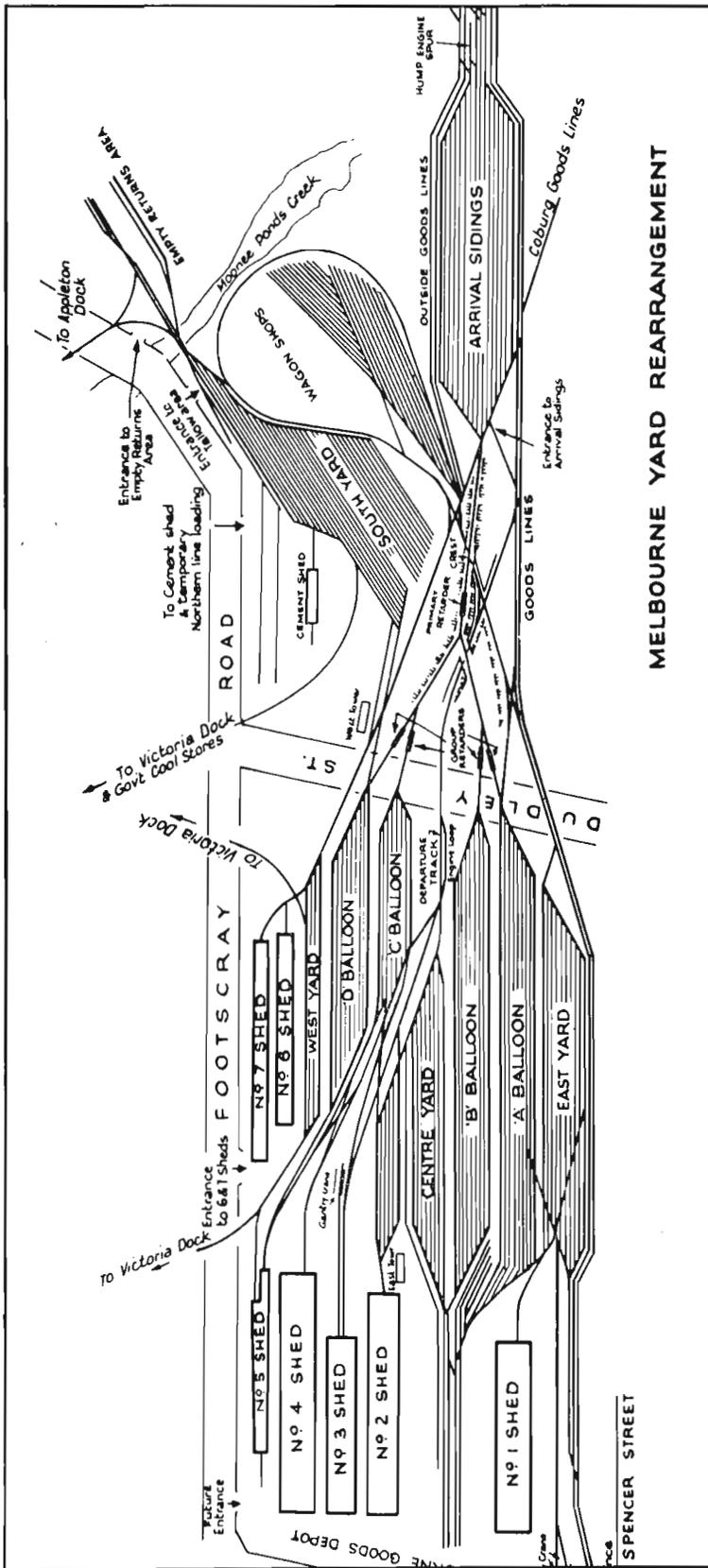
mendation to completely reconstruct it were made by Mr. Reynolds, then Chief Civil Engineer, and Mr. A. C. Brown, then Assistant Chief Traffic Manager. The scheme involves the design and construction of a hump yard with automatic switching and retarders to control the sorting of incoming trains. See *News Letter*, Sept. 1968, Aug. and Sept. 1964).

After arrival at the West Tower, the party entered the signal box, located in the Control Room on the third floor, where Mr. Wilcox officially opened the Tower and the new arrival yard, by pressing two buttons, that set the points and signals for

the first freight train to enter the new yard.

Benefits country

In the course of a short address, Mr. Wilcox pointed out that the main benefit of the \$11 million project would be felt in the country rather than in Melbourne. It was, he said, an outstanding example of how money spent in Melbourne could be of tremendous value to the whole State and the Riverina; in fact, with our interstate rail links, the benefits would be felt throughout Australia. He congratulated all those concerned—including the men who



MELBOURNE YARD REARRANGEMENT

worked in the yard—on their success in getting the construction work done while the yard was still operating.

Mr. G. F. Brown, Chairman of Commissioners, emphasized that the Melbourne Yard was unique, in that its location, close to the Goods Sheds, enabled the Railways to give the best service to customers. He added that the success of the project resulted from excellent co-operation between the branches, and said that special thanks were due to the Signal and Telegraph Division and the staff who operated the yard.

West Tower

The West Tower has been built as the control centre both for the automation and administration of the yard. Its height—48 ft. above Dudley Street—was determined by the necessity for the retarder operator to have a clear view of the humping operations.

Before a train is to be humped, a cut list will be prepared to show the siding into which each wagon is to be sorted. The cut list will be transferred to a tape and, by running the tape through a tape reader, all the routes will be automatically set.

The new signal box in the West Tower replaces the old Dudley Street Box and two other boxes. One of them—Weighbridge Junction—was unofficially known as Spion Kop. The origin of this name goes back to 1900, about the time when the Melbourne Yard was built. During the Boer War (1899-1902) Australian soldiers fought in battles at the Modder River and Spion Kop. On returning, some of them took jobs in the Melbourne Yard, and were, no doubt, responsible for the unofficial names of Spion Kop and Modder River. The latter—applied to Moonee Ponds Creek—has died out, but Spion Kop has persisted.

It is interesting to note that the total tonnage of goods and livestock now hauled annually by the railways (12 million tons) is four times that of 1900 (3 million tons).

In the West Tower signal box, the route setting and clearing of signals is, of course, done by pressing route buttons. Altogether, 368 routes can be set up, and the system includes 100 electro-pneumatic point machines (used for the first time on the V.R.), 125 track circuits, and 3,000 plug-in relays. Time of operation of the electro-pneumatic points is about one second.

Arrival yard

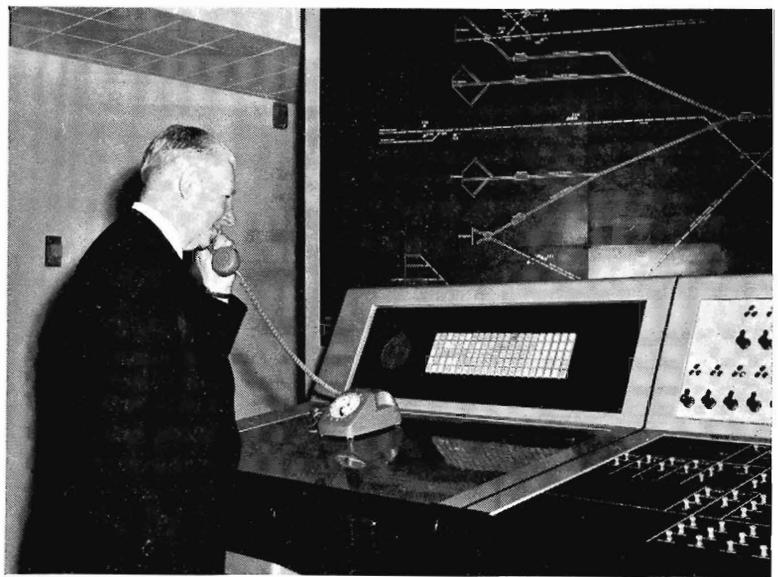
One of the major problems of the old yard was that tracks were few (nine) and short (up to 1,125 ft.). They sufficed for those days when trains were short, but were quite

inadequate for today's longer, diesel-hauled trains. The new yard now has 17 tracks, the longest of which is 2,062 ft.

"Although the provision of a better arrival yard was an urgent necessity", said Mr. Reynolds, "there were a lot of problems to be overcome. We rebuilt the goods line bridges over the Moonee Ponds Creek—1,260 ft. of permanent bridging of steel and concrete which replaced timber bridges and a forest of piles. The Melbourne City Council had given notice of its proposal to rebuild Dynon Road bridge which crossed the rail tracks at North Melbourne station. As major difficulties would have been created both for the Railways and the Council if the new road bridge were delayed, we encouraged the Council to proceed early with its proposal, and gave it generous help in achieving the new road bridge which was opened by the Premier last July."

Of the 17 tracks in the arrival yard, seven are electrified to take freight trains hauled by electric locomotives.

The overhead masts are creosoted hardwood, all prefabricated before treating, so that none of the creosoted



At the opening of West Tower, Block and Signal Inspector V. Bonighton is using the 'phone at the signal box control panel. At the back is a section of the diagram showing what signals are cleared and the location of trains.

sapwood surrounding the heartwood is damaged. It is hoped that this

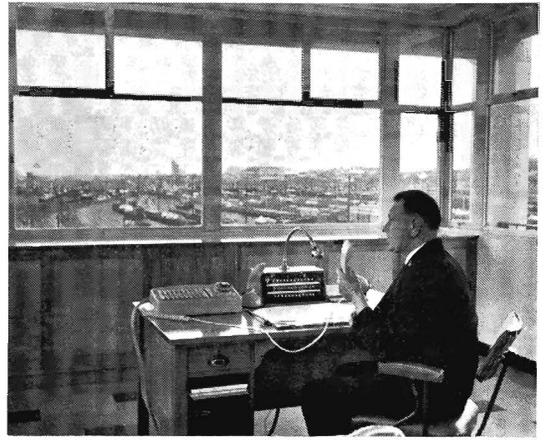
will give an exceptionally long life to the poles.



At the signal maintenance diagram in West Tower, Engineer S. White conducts tests from circuit plans. This diagram is a photographic replica of that in the signal box two floors above; it facilitates maintenance work on the system.



The inspection party passes through one of the pipe subways.



Assistant Yardmaster A. S. Kloppman in the East Tower.

The hump

All the arrival tracks converge to form two tracks, or two hump leads which will join into one track, and lead to the crest of the hump.

This man-made hill will consist of two bridges and retaining walls filled in by an earthen bank, the approach grade being 1 in 30, and the descending grade starting at 1 in 27.

The bridges will form two underpasses so that a departure track and engine track can be led under the hump.

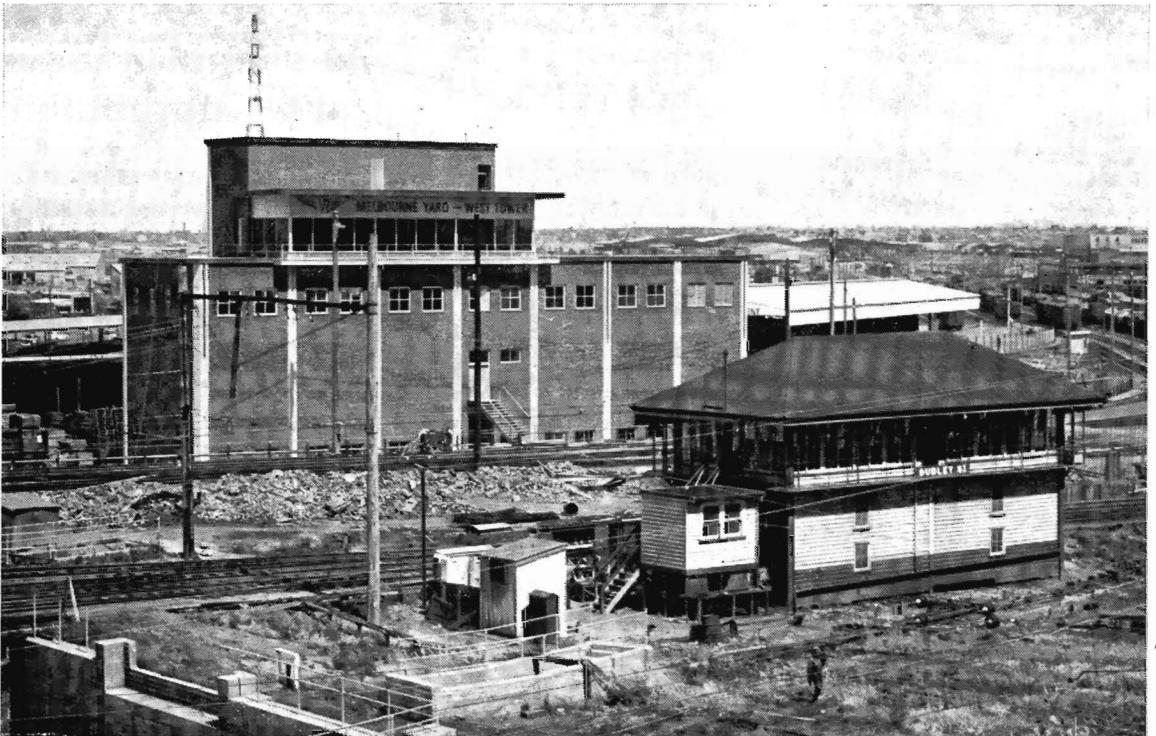
“ This underpass track will eliminate conflicting moves in the Dudley Street area, a problem which we had with the old yard ”, said Mr. Reynolds.

The single track over the crest will branch into four leads on the way down, and, at the foot of the hump, each lead will divide into eight sorting sidings. Down the hump from the crest, will be five retarders—one primary and four group retarders, one on each of the four leads. Every wagon will pass

through two of them between the crest and the sorting sidings.

The speed of the wagon will be measured by radar at several locations down the hump, to determine whether it is a fast or a slow roller. The braking force to be applied by the retarders will be controlled by computers which will also determine the speed at which the wagon should be released from the group retarder to control its run into the sorting sidings.

As each wagon passes down the hump, the points will progressively



View of West Tower and Dudley Street signal box, the work of which is now being done by the box in the Tower.

alter, to route the wagon into its correct siding as given by the punched tape fed through the tape reader.

Air for points and retarders

Points and retarders will be operated by air, supplied by three compressors (two of which could carry the load) at a rate of 500 cu. ft. per minute from each compressor, and a pressure of 200 lb. per sq. in. Pressure reducing valves will reduce this to 100 lb. for the retarders, and to 50 lb. per sq. in. for the points.

Pipe subways

To carry cables and pipes from the control equipment in the Tower to the field equipment, two subways have been built, one 382 ft. long, and the other, 137 ft. They were built from heavy duty, reinforced concrete pipes, 9 ft. in diameter, supported on a concrete cradle to distribute the load on the earth. These subways will be used by employees to pass from side to side, without crossing the tracks down which wagons from the hump will be rolling. (See *News Letter* front cover, Dec. 1966.)

Sorting balloons

Wagons will be sorted into one of 32 sorting tracks divided up into four groups of eight, each, called A, B, C, and D balloons:

- A will take wagons for Gippsland and suburban destinations;
- B will take wagons for northern, north-eastern, north-western, and western destinations;
- C will take inwards wagons with loadings for Melbourne, and empty wagons; and
- D will take wagons with export loadings, wagons to be bogie exchanged, and wagons with empty returns freight.

Length of these sidings varies throughout the yard up to a maximum of about 2,500 ft.

"Our old yard had dead-end sorting sidings which caused a lot of difficulties, and lost time in moving wagons from sorting sidings to final location in the yard", Mr. Reynolds said.

"Our new sorting sidings have a lead out at the far end to enable wagons to be readily pulled out for placing in the proper unloading track, or for lifting out to be made up into an outgoing train."

New yards and goods sheds

There will be three departure yards—East, Centre, and West—where trains will be made-up of the wagons sorted for "out of town"

destinations plus wagons loaded in Melbourne Goods Depot. East Yard will be for trains to Gippsland or eastern and southern suburbs; Centre Yard for northern, north-western, and western parts of the State; and West Yard to serve dock areas.

The new South Yard will also serve the dock area—Victoria Dock, Appleton Dock, and Swanson Dock—to give better service to shippers. Wagons can be assembled and rakes held ready to answer the call from the port for export loading, or for empties to receive inwards loading. (In this area will be located a new North-eastern Goods Depot; trains for the north-eastern and Goulburn Valley districts will start from this yard.)

Goods sheds' facilities have been expanded. A new No. 5 shed, for inwards freight in wagon loads, has been built. No. 4 shed has been extended and rehabilitated, to handle inwards freight for Melbourne (see story on page 2). No. 3 shed, for northern freight, is to be rebuilt, and a new cement shed has been erected.

East Tower

East Tower is the office of the Assistant Yardmaster who is in charge of the make-up of departing trains. It also houses train examiners and other staff. At a later date, the Assistant Yardmaster will be able to cross over to the high level of East

and Centre Yards by a footbridge.

Like other buildings in the Melbourne Yard area, East Tower is founded on piles—70 of them, 90 ft. long.

Major surgery

"Considerable thought was given to the location of the new yard", Mr. Reynolds said. Overwhelmingly, all the evidence pointed to rebuilding on the present site—to move the yard elsewhere would have produced intolerable delays, and increased costs."

"It is, of course, much easier to build a new yard in a vacant paddock; to rearrange the yard from end to end is major surgery, and requires careful planning and co-ordination between the operating branch and the construction forces, to achieve success. This has been the secret of the Melbourne Yard rearrangement."

At the present time, 21 of the planned 34 stages have been completed, or virtually completed, and work is in hand on four more stages. Soon, two more stages will be opened up, and, progressively, stages will be finished, and others started, until, in March 1970, the automatic switching and retarder equipment will be commissioned to make Melbourne Yard the first automatic hump retarder yard in Australia.

THE V.R.I. LIBRARY'S CARTON SERVICE

The V.R.I. Library prides itself on providing a service for all Institute members, no matter how distant or isolated their locations. It is a library that comes to you, if you cannot, for any reason, come to it.

This is achieved by a carton service, whereby up to six books at a time may be sent to any member of the Institute who chooses to avail himself of the service.

Cartons were introduced primarily for the use of members isolated from the library by distance, but over the years, many members at suburban locations have found it more convenient to use the carton service, rather than make special trips to Flinders Street to exchange books.

In order to keep these members informed about new books added to the library, quarterly lists are sent to all carton-users, and many use these lists to nominate the books they would like to read.

And six at a time? Any member may have two books at a time from the adult section, fiction or non-

fiction; two books from the childrens section; and two books from the technical section, which includes hobbies, handicrafts, car manuals, etc. If preferred, four childrens books may be had, in place of the two adult and two childrens books.

Whether you use the library or not, you may apply for catalogues at any time, and if you require any further information, the librarian will be pleased to supply it.

If you wish to use the library service, send your name, occupation and location to the V.R.I. Library, Flinders Street; indicate the types of books you prefer—detective, general fiction, travel, childrens (age 6-8), etc.—and a choice of over 50,000 books will immediately be available to you.

WEST FOOTSCRAY "UNDERGROUND"

The rail line between West Footscray and South Kensington, opened 40 years ago as a goods line, runs under Bunbury Street, Footscray, for a distance of 1,357 ft. Since 1962, this "underground" has been dual gauge—for Victorian traffic and standard gauge trains.



Red accents lend interest to being unloaded at Lyndhurst line.

COLO ON T SYST

Although railway freight equipment designed for efficiency and economy make a very attractive picture, a containers at Port Melbourne.



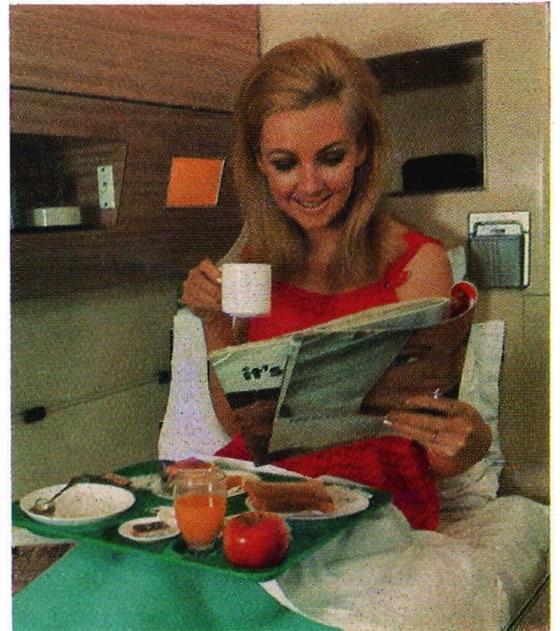
is shot of steel pipes
g the natural gas pipe

UR
HE
EM

ment is primarily de-
my, it can sometimes
this scene of overseas
ourne shows.

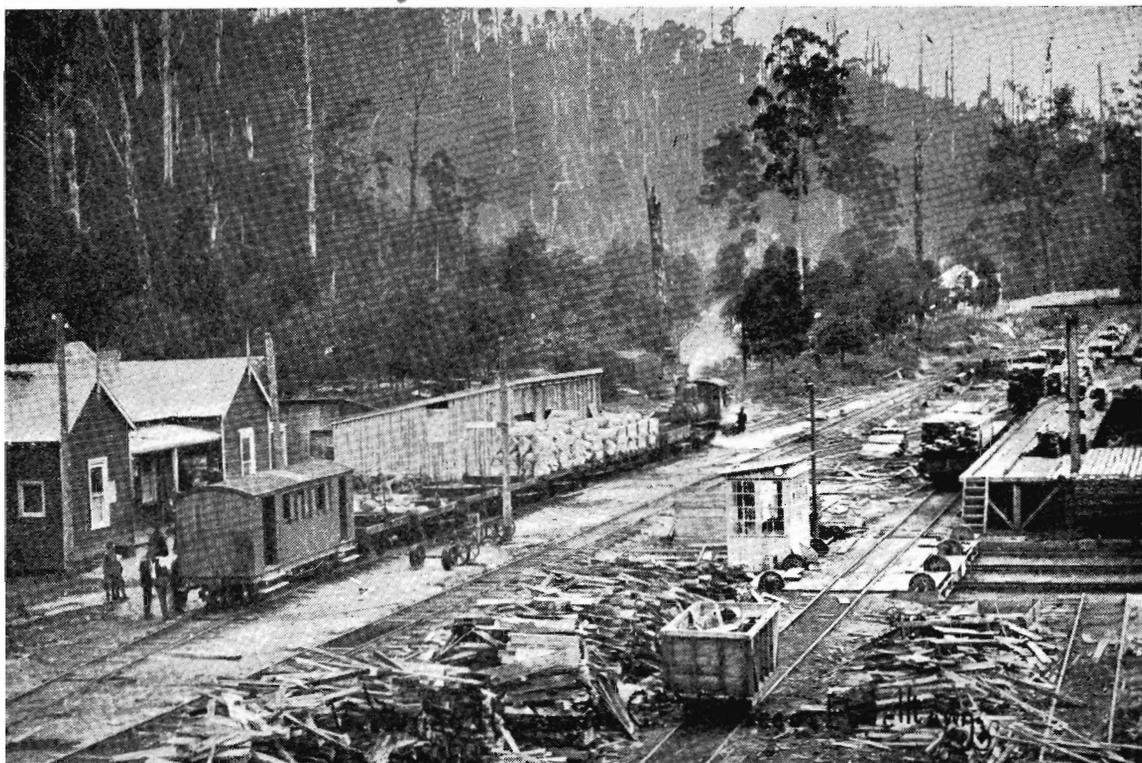


▲ These colourful publicity shots of a model in a new twinette on *The Overland* emphasize the new and larger breakfast served, as well as the comfort of overnight travel by train.
▼



VICTORIA'S TIMBER TRAMWAYS

by Frank Stamford



Powelltown in 1919. A train has just arrived from Yarra Junction. (F. Stamford Collection)

Victoria once had many hundreds of miles of privately owned railways and tramways, most of which were used for hauling logs and timber; but some lines were built for quarrying, mining and other industrial purposes.

Timber tramways were of a very primitive nature, and had most peculiar locomotives and rolling stock. Some historically-minded railway enthusiasts—regarded by many people as being as peculiar as the tramways they study—are particularly interested in recording the history of these lines. After all, timber tramways played a very important part in Victoria's transport system before road transport took their place.

Tramway historians carry out their work in much the same way as archaeologists. Defying blackberries, nettles, and snakes, they force their way through undergrowth along old tracks, taking careful note of the route the lines took so that accurate maps can be prepared.

Nine years' work

The tramway historian requires unbounded patience and persistence, for the size of his task is immense, and he can easily be misled by

inaccurate maps or wrong information. It has taken nine years to map about 225 miles of tramway in the Warburton–Noojee–Gembrook area alone, and there are more tramways in this area that have still to be plotted.

Timber tramways abounded in trestle bridges, most of which have collapsed or are impassable; crossing the swamps and deep gullies they spanned is another hazard faced by the tramway fanatic. In many cases machetes have to be used to clear a path through the undergrowth, so that important but long defunct timber tramway centres can be reached.

This type of archaeological work can be quite rewarding, as the finding of small relics, like sleepers or wheels, can indicate the gauge of the tramway, and may often help to solve puzzles concerning the ownership and history of the locomotives used.

Hours can be spent scrub-bashing in search of the remains of locomotives, almost always unsuccessfully. The tramway historian is used to disappointment, but he can never be satisfied until he has explored every avenue, no matter how

many blackberry scratches or leech bites he may get in the process.

Hire light aircraft

Some keen workers have even hired light aircraft in an attempt to find mill-sites, tunnels, bridges, and track arrangements at junctions. Old track formations show up well from the air, and aerial surveys may become quite helpful in mapping work. The expense is justified where all other methods have failed to reveal the information required.

Of course the tramway historian does not spend all his spare time gallivanting around the countryside. The rather dull and monotonous task of searching through thousands of pages in old newspaper files for historical background material can take up many evenings.

The majority of tramways were rough wooden-railed affairs worked by horses, but there were many steel-railed lines powered by locomotives. The best timber tramways had track considerably inferior to the V.R.'s lightest branch lines, but the majority did not even approach this standard. Timber tramway wheels had deep flanges and 6 in. wide treads, but on the uneven track—

which often varied up to three inches in gauge—derailments were an everyday occurrence.

They rode on logs

Some lines were a hotch-potch of hardwood and secondhand steel rails of wildly varying sizes. In many timber-getting areas the only form of transport was the tramway, and it was the custom for any passengers to ride astride the logs, or on top of a load of sawn timber. Such journeys were not for the faint hearted, as each log rested on two small unsprung trolleys, one at each end, and logs had an alarming habit of rolling and pitching while in motion.

But the passengers' situation was one of luxury when compared to the brakemen's unenviable task. Each brakeman—there were usually two to a train—had to keep the train under control by applying the handbrakes on the trolleys, and to do this they had to run along the top of the logs while the train was moving. In the early hours of dark and wet winter mornings, one slip could be fatal.

Many tramways maintained their locomotives as badly as their trackwork. One tramway which ran westwards from Noojee for about 14 miles had four locomotives. The best of these had very weak handbrakes, and the steam regulator was jammed in the fully open position. The driver became quite skilled at controlling the locomotive with the reversing gear, but the engine was far too fast for the track, and finished up by diving through the top of a trestle bridge—not a unique happening in the history of Victoria's timber tramways. Another of this tramway's engines also ended its active life in a spectacular derailment.

Hinged funnel

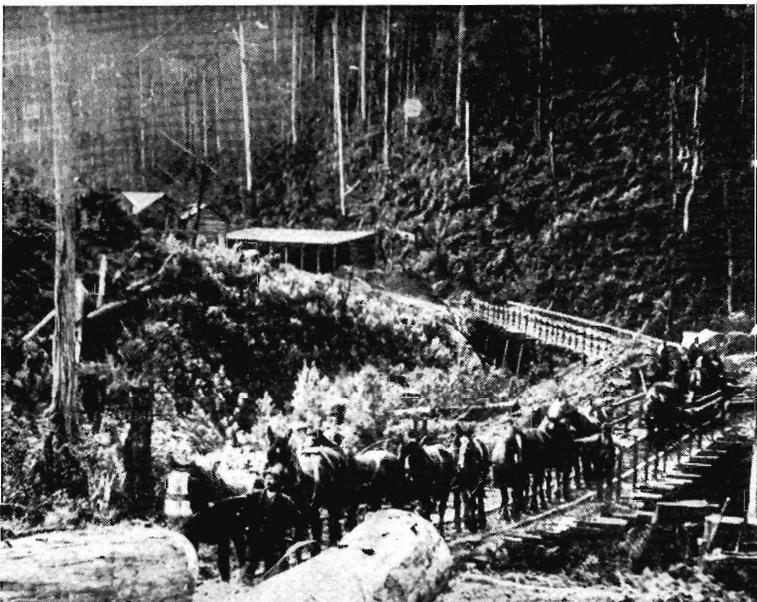
But not all tramways were quite this bad. The Powelltown tramway had many substantial and impressive trestle bridges in its 21 miles, not to mention a 1,000-ft.-long timber-lined tunnel. This tramway even had two passenger coaches. A tramway that ran from Forrest also had two tunnels, but these had restricted clearances, and the locomotive had to have a hinged funnel so that it could go through them.

One rather progressive and well maintained 2-ft. gauge tramway ran from Alexandra; it had the distinction of owning the first two diesel-powered locomotives to be built in Australia. These neat looking engines were built in Melbourne by Kelly & Lewis in 1936, and one still remains locked in a shed at a timber mill in Alexandra. For its historical interest alone, it seems worthy of preservation, though most

steam enthusiasts seem to disagree.

The end of the timber tramways came in the 1940's when the Forests Commission decided that mills should be centralized in towns, and steam locomotives removed from the bush, because of the fire danger. With better roads, and the established timber-getting areas worked out, the tramways quickly closed down. The last steam operated line, running from Collin's Siding, near Erica, closed in 1949. Fortunately the

unusual *Climax* geared steam locomotive that worked this line has been preserved at Menzies Creek, in the Puffing Billy Preservation Society museum. A few lines operated by rail tractors lasted a little longer, but as far as is known, the only tramway still in operation is about a half-mile of horse-worked, 3-ft. 6-in. gauge tramway at Barwon. The rest are now overgrown tracks gradually becoming more and more difficult to trace.



A horse train on Sanderson's line (near Forrest) about the turn of the century. (M. Plummer Collection)



A load of logs on its way to Powelltown in the 'thirties. (Forrests Commission photograph)

LINES FROM OTHER LINES

Cars will go by rail

The Bundesbahn (German Federal Railways) is preparing for the time—soon to come—when there will be more cars than the German roads can accommodate.

The Bundesbahn plans to take the cars by train. For example, it would move them from Hamburg in the north of the Federal Republic, to far South Munich, gateway to central Europe and the Mediterranean.

The special all-automobile train would do much, the Bundesbahn people say, to relieve congestion on West Germany's speedways (autobahns) and other expressways.

The high-speed train would take not only cars but their passengers; and also container traffic, together with long-distance trucks and highway transports.

The most modern methods would be used to allow a "rendezvous" technique that would pick up and automatically couple to the train additional coaches at major points en route. In fact, the network would be so planned as to include many of West Germany's most congested areas of traffic.

The line would not proceed as the crow might fly from Hamburg to Munich. Instead, it would travel 600 miles via Bremen, Hanover, the industrial Ruhr area, Frankfurt-on-the-Main, Mannheim and Stuttgart. For the more immediate future, the Bundesbahn plans to shorten travel times of its present car-carrying trains so that the motorist and his companions can remain in their own vehicle en route.

Nor is that all. The Bundesbahn also are planning inter-city trains for distances of between 180 to 240 miles, with no stops, or very few, between starting point and destination.

Floats on air

A revolutionary new vehicle which neither touches the ground nor the rail from which it is "suspended" has been successfully tested in Lyons, says the *Australian Financial Review*.

The vehicle, Urba 4, is a mini-train running on an air cushion; it is pulled along the rail above its roof through a magnetic field induced between that rail and the roof.

This linear motor (See *News Letter*, page 125, August 1963) has no revolving parts. What would be the rotor in a conventional elec-

trical motor, takes the form of the suspended rail, that is a metal beam the profile of which is a "T" turned upside down.

The field is generated between that beam and another one, the shape of a "U", fixed on the vehicle's roof.

To counteract the magnetic pull from the top beam which is necessary to keep the electromagnetic field alive, Urba 4 uses the air-cushion system.

Tanker that stretches

A rail tanker that can be lengthened or shortened as required, has been built by the Union Tank Car Co. (U.S.A.), reports the *Railway Age*. Ring sections can be added to, or removed from, a basic keystone unit to change the tanker's size and capacity. This can be done with only a few mechanical steps. The

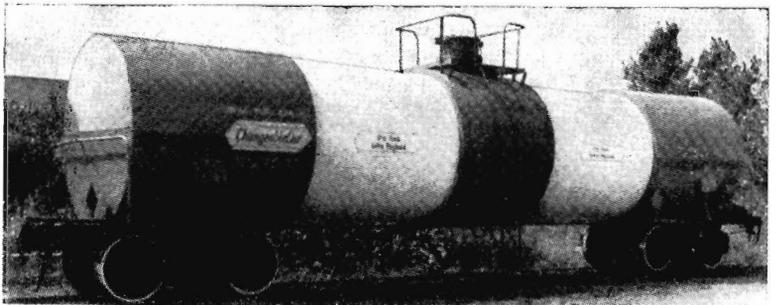
But while experimental air-cushion trains create the cushion by propelling air towards the ground, thus levitating the train, (See *News Letter*, p. 154, Oct. 1966) Urba 4 must maintain the air-cushion through suction, to prevent the vehicle from floating upwards.

The advantages of Urba 4 are that it requires no conductor, no wheels, no maintenance to speak of; it is noiseless, and creates no air pollution as no fuel is used.

ring sections are inserted at predetermined points on the tanker. Valves and other fittings can also be changed to accommodate different types of loading. The tankers are mostly rented by their users, and it is expected that the new idea will encourage lessees to take them for longer terms, thus gaining the advantage of lower rates.



The basic elements of the *Changeable Car* consist of a keystone unit at the tanker's centre, end tubs that close the tank, and centre plate assemblies which may be altered to vary the bogie capacity.



The *Changeable Car* with ring sections inserted to increase the tanker's capacity.

V.R. HELPS MARLIN

An urgent call was received by the Railways, last month, to rush supplies of barytes from South Australia, to help in plugging the gas leak from the Marlin well.

Barytes is a mineral mined in South Australia as a whitish rock. After processing, it becomes a high density powder. When mixed with bentonite—a type of clay that gells—it forms a heavy mud. As *News Letter* went to print, it was proposed to pump this mud down the well in the sea-bed, in an attempt to plug the leak.

The urgent request was received by the Department on Friday, December 6. The S.A. Railways organized the collection and dispatch of the barytes from Quorn and Gillman, and the first 500 tons of the mineral reached Welshpool early on the Monday morning. Special trains followed, until the estimated needs of 5,000 tons were met. The supplies were railed to Welshpool as quickly as B.H.P.-Esso could unload it. The rail haul from Quorn to Welshpool involved a movement of 840 miles, and from Gillman 615 miles.

FOR RAILFANS

The third and latest edition of the Australian Railway Historical Society's guide to the major exhibits in the Railway Museum at North Williamstown is an attractive booklet of 32 pages, well illustrated on art paper, with colour cover, and is a "must" for all rail enthusiasts. Packed with information about the exhibits, including a centrespread of diagrams showing how a steam locomotive works, it is available for 40 cents at the Museum, or 50 cents (posted) from A.R.H.S. Sales, P.O. Box 5177AA, Melbourne, 3001.

This year's calendar of the Association of Railway Enthusiasts is indeed "bigger and better than ever". Printed on heavy art paper, it has a dramatic 10 in. by 8 in. photograph on the page for each month, and covers five Government and three private railway systems. Copies are \$1.15 (posted) from A.R.E. Calendars P.O. Box 384, Bendigo, 3550, Victoria.

SPECIAL TO FRANKSTON



Mordialloc Creek makes a picturesque setting for this special that recently took a party from Calsil Bricks Pty. Ltd. to Frankston for the opening of a new brick making plant. Refreshments were served on the train.

LOCO HAS BIRTHDAY



Steam locomotive D3 639 had its 65th birthday celebrated last month. The Australian Railway Historical Society had the cake shown above made at the Spencer Street cafeteria. On December 14, the D3 hauled an A.R.H.S.-organized train trip to Wonthaggi, and the Society gave a piece of the cake to every passenger on the train.

FIRST TENDERS

On May, 3, 1856, the Victorian Railways issued its first call for tenders. Local agencies were requested to submit proposals for the

importation of railway material on a commission basis. The contract was let to Messrs. Dalgety, Cleve and Hamill on June 21, at a 2 per cent. premium on an estimated cost of \$146,916.

Wins Australian championship

When Mr. C. W. Paterson won the Australian individual championship at the Australian Railways' First Aid Competitions held in



November at Hobart, he became the third V.R. man to have gained that distinction. Mr. Paterson, who has been First Aid Attendant at Jolimont Workshops for the past 14 years, is now in the Ambulance Section at Spencer Street.

During his 16 years in the railways, he has obtained the bronze, silver, and gold medals in first aid, and, in the State competitions, won the novice individual event in 1965, and, of course, the senior individual event last year. Mr. Paterson also belongs to the St. John Ambulance Rescue Squad, and is superintendent of its Waverley Division which won the State championship in 1966 and 1967. In addition to first aid, Mr. Paterson has played an active part in V.R.I. wrestling.

End of an era

When Mr. J. M. Ward retired recently as Chief Telegraph Officer, his retirement almost coincided with the end of Morse on the V.R. Only one Morse circuit is left (to Mildura), and that will very soon be replaced by teleprinter. Almost all of Mr. Ward's career was in the Telegraph Division. After starting as a lad porter at Dandenong, he transferred to the



Mr. Ward

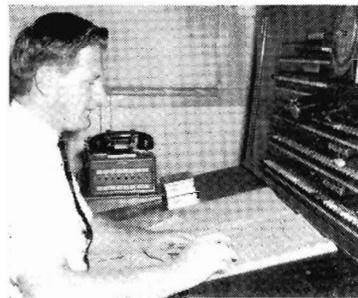
Telegraph Office at Spencer Street in 1922, and was afterwards stationed at Bendigo as a telegraphist for about 20 years. He was in the R.A.A.F. during the Second World War, and resumed in the Department as a telegraphist at Spencer Street.

Recalling the days when much of the railway communication business was in Morse, Mr. Ward said that each of the good operators had his own individual style of sending that was as recognizable as his signature.

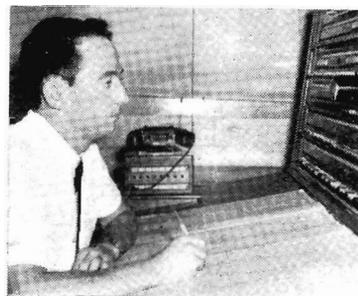
Road to success

Opportunities in the Traffic Branch for the qualified man to gain promotion were never better than they are today. Typical of young men who have profited by these opportunities are Messrs. R. E. Paterson, B. T. Spicer, and R. Philp.

Starting at Nunawading as a junior station assistant in 1961, Mr. Paterson, two years after, gained the W. R. Brown Memorial Prize for station accounts and management, and, in 1964, at the age of 20, became a relieving assistant stationmaster. Two years later, he was appointed stationmaster at Underbool, and, last year, became a train controller in Head Office. Tennis, badminton, and shooting are his sports.



Mr. Paterson



Mr. Spicer

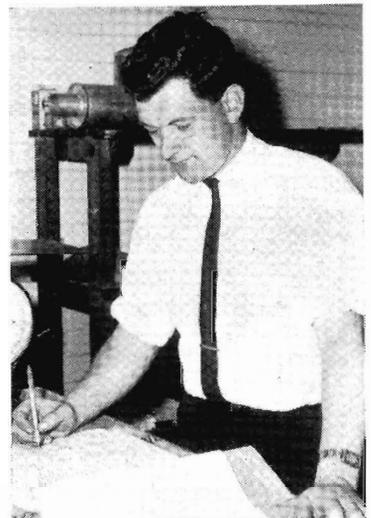
"The best of them could send with a speed and rhythm that was almost musical," he added.

Single inter-system pass

Railways of Australia inter-system passes, first and second classes, available for travel over one or more systems, were introduced from January 1. They replaced the separate inter-system passes that were previously required for travel over each system.

After joining the Department in 1961 as a clerical assistant, Mr. B. T. Spicer worked at Hampton and Sandringham until 1965 when he was appointed a relieving assistant stationmaster in the Seymour district. In 1967, aged 22, he became a stationmaster, and is now in Train Control at Head Office. As well as being industrious, Mr. Spicer would also seem to be lucky, as, about 16 months ago, just before getting married, he won a Datsun sedan in the Tommy Hanlon Show on TV.

Most of Mr. Philp's career has been in the north-eastern district. Starting as a station assistant in 1962, he went to Kyabram, and, the following year, to Seymour. Winning a W. R. Brown Memorial Prize for station accounts and management in 1965, he was appointed a relieving assistant stationmaster at Seymour in the same year. Promotion to relieving stationmaster, at the age of 23, occurred last year.



Mr. Philp



Mr. Thompson holds a piece of trunking that was damaged by the car. At the back is the embankment down which the car went.

Station Assistant Jim Thompson's knowledge of first aid helped to revive a woman motorist whose car went over the embankment on to railway property near Burwood station recently. Mr. Thompson crawled into the car through the rear window, and found the driver was unconscious and had ceased breathing. After he had placed the motorist's head in the right position, breathing began and she revived.

Mr. Thompson holds a railway first aid certificate, and belongs to the Waverley Rescue Squad of the St. John Ambulance organization. During his four years in the Department, he has been stationed at Burwood.

RETIREMENTS

TRAFFIC BRANCH

Llewellen, G. E., Maryborough, 2/2
 Browne, H. T., Melbourne Goods, 6/2
 Ciennell, F. W., Flinders Street, 9/2
 O'Brien, J. S., Bendigo, 9/2
 Herbert, R. R., Nth. Dynon, 12/2
 McMillan, J. A., c/o Metro. Supt., 15/2
 Mangan, D., Bendigo, 18/2
 Keating, F. B., Bendigo, 18/2
 Williams, F. C., Melbourne Goods, 21/2
 Holloway, E. T., Melbourne Goods, 23/2
 Beattie, J. L., Melbourne Goods, 24/2
 Currie, G. J., Spencer Street, 13/11
 Sandlant, A. J. M., Benalla, 16/10
 Boyle, A. A., Flinders Street, 27/11
 Lane, C. J., Sandringham, 30/11
 Tremayne, W., Ringwood, 29/12
 Welsford, R. J., Oakleigh, 7/12
 Abbott, E. L., Wodonga, 31/12

ROLLING STOCK BRANCH

Pywell, R. E., Nth. Melbourne, 20/11
 Stacey, S. R., Newport, 22/11
 Ratcliffe, A. L., Newport, 9/1
 Costa, V. E., Newport, 8/2
 Adams, H. J., Bendigo North, 7/2
 Harris, T. A., Bendigo Loco., 8/2
 Nolan, J., Bendigo North, 8/2
 Patrick, A., T.L. Depot, 9/2
 Fraser, S. J., Jolimont, 9/2
 Ianno, C., Jolimont, 15/2
 Ancilleri, L., Shelter Shed, 16/2
 O'Sullivan, W. D., Sth. Dynon, 16/2
 Stone, H. W., E.R. Depot, 20/2
 Helm, E., Bendigo North, 20/2
 Currie, L. A. L., Head Office, 27/2

WAY AND WORKS BRANCH

Boland, J. A., Spotswood, 29/11
 Chaplin, S. E., Sunshine, 29/11
 Fortuna, F., Spotswood, 23/10
 Manz, H. H., Ringwood, 10/2
 Parsons, S. E., Nth. Melbourne, 18/2
 Proctor, E. G., Maffra, 10/2
 Stewart, W. F., North Geelong, 6/2
 Wilson, W. G., Warragul, 7/11
 Zoch, S. H., Seymour, 19/2

COMMERCIAL BRANCH

Dabb, W. B., Head Office, 30/11
 Borisenko, M., Head Office, 7/11
 Scully, L. A., Head Office, 27/12

ELECTRICAL ENGINEERING BRANCH

Price, R. O., Distribution Div., 24/8
 Clifton, W. J. R., Distribution Div., 30/8
 Kowal, B., Lighting & Power Div., 13/9
 Bartolo, G., Overhead Div., 23/10
 Cadman, A. H., Distribution Div., 31/10
 McKie, B. D., Distribution Div., 16/11
 McAfee, J. T., Lighting & Power Div., 18/12
 Brown, J., Overhead Div., 27/12
 Brown, H. A., Distribution Div., 22/1
 Fricke, A. G. L., Lighting & Power Div., 20/12

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

TRAFFIC BRANCH

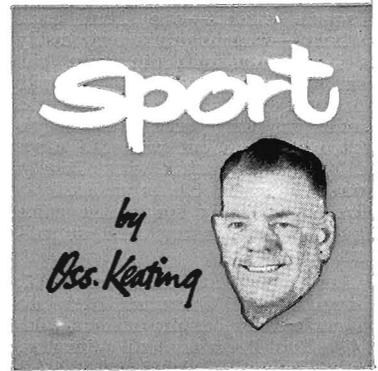
Butler, R., Melb. Goods, 29/11

ROLLING STOCK BRANCH

Bracken, J. C., Nth. Melb., 19/11
 Westbrook, J. H., Ballarat Nth., 22/11
 Tubbs, R. L., Ballarat Nth., 15/11

WAY AND WORKS BRANCH

O'Brien, W. N., Seymour, 26/11



A.P.I. v V.R.I. cricket

It was a rather dull, overcast morning when we arrived at the St. Kilda Cricket Ground to do battle against our traditional foes, the Australian Postal Institute. The ground was in magnificent shape, and the wicket, although a little on the green side, looked as if it contained plenty of runs.

Winning the toss, Ron Owen, A.P.I. captain, had no hesitation in sending us in to bat, and as events turned out, his judgement proved right. Gurrie and Cox opened the innings for us, facing the bowling of Myerscough and O'Brien respectively. In the second over, the bowlers broke through when Cox, thinking a ball from O'Brien was passing through on the off, offered no stroke only to find it cut back, hit him on the pads and he was out l.b.w.—1/8—not so good. McCalm came in, and after scoring one run, was clean bowled, also by O'Brien, and the score was 2/16. Willowwhite joined Gurrie and these two, batting determinedly, pushed the score along steadily until Gurrie skied one and was caught, and we were 3/68. However, with the exception of Stan Wallis, our captain, who contributed 14 to the score, none of the remaining batsmen managed to reach double figures, and so we were all out for the meagre total of 102, the last wicket falling at 12.20 p.m.

Our top scorers were opener Pat Gurrie (41), Ian Willowwhite, of Ballarat (26), and, as previously mentioned, Stan Wallis (14). In the A.P.I. attack, only three bowlers were used, and they were all successful. Myerscough bowled very well all the morning, and finished with the excellent figures of 5/36 from 13 overs. O'Brien took 2/26 from 5 overs, and McColl 3/30 from 7.4 overs.

Rouse and MacIntosh opened the innings for Postal, and in his first over Jenkins clean bowled Rouse for 7, and Postal were 1/9. Evans was operating from the other end, and both these bowlers were hostile in the pre-lunch session. Two

dropped catches—one from each batsman—were to prove very costly as it was not until the score had reached 84 that Jenkins trapped MacIntosh into lifting one, and the catch was duly accepted. Hogan was next to go but with the score at 3/100, the match was virtually over. Still our blokes refused to give in, and they fought the game right out, dismissing their opponents for 155.

For Postal, MacIntosh scored 47, Hogan 34 and Carracher 23, while of our bowlers Jenkins finished with 5/69 from 15 overs and Woresley, from Ballarat, finished with 3/38 from 8 overs.

At the official luncheon, we were given a very hearty welcome by Mr. Jim Carroll, General President, A.P.I. and this was suitably responded to by Mr. Lindsay McCallum, our Vice-president.

After the game, Stan Wallis reluctantly handed the trophy over to Ron Owen, and congratulated the Postal Institute on their fine win. Mr. G. N. Smith (Director of Posts and Telegraphs) attended the luncheon and Mr. Commissioner L. A. Reynolds paid a visit to the St. Kilda ground. Mr. Len Bennett, our new chairman of the sports committee, was also an interested spectator at the match. Might I mention a very nice gesture by the President and Council of the Australian Postal Institute in inviting along to the match and the official luncheon, Frank McCloskey (our former chairman of the sports committee) in recognition of his efforts in promoting the sporting contests between the two Institutes.

This match concluded our sporting fixtures against the A.P.I. for 1968, and while the results were better than in 1967, there is still room for plenty of improvement. This year saw A.P.I. winners in football and cricket, but we took the honours in bowls and golf. Let's hope next year we take out all four.

V.R.I. Cricket Association

The position of the teams in the metropolitan competition is most interesting. After the completion of the third round, the ladder was: Melbourne Yard, Codon and Loco, all on 12 points, Jolimont and Spotswood yet to win a game. It has now become obvious that Melbourne Yard and Codon have emerged as real threats to Loco in the fight for the Commissioners' Cup, and this has stimulated a lot of interest in the competition. Don't forget, the semi-finals will be played on Tuesday, February 11, and the final on Tuesday, February 18. Both these fixtures are all day games, and will be played at Royal Park.

Golf

In spite of bad weather, just on 80 players faced the starter for the 1968 Western District Championship, played on the Colac Golf Club's course. There were golfers from Ararat, Ballarat, Melbourne, Traralgon and, of course, Geelong. Geelong boys made a clean sweep of the major trophies, with Ray Darcy winning the Western District Championship, Ian Werner, the minor championship, Gordon Scholes (M.H.R. for Corio) the A grade handicap, George Lucas the B grade, and Les Meager the C grade handicap. As usual, congratulations are in order to Norm Roberts, (President), Ian Paterson (Hon. Secretary), and the rest of their hard working committee on the organization and control of this fixture.

For the last outing of 1968, the V.R.I. Golf Club visited Hepburn Springs Golf Course where an 18-hole stableford event was played. The trophies were won by J. Blee and Mrs. A. Collins respectively. On Saturday, December 14, the club held its annual Christmas party in the V.R.I., and that very popular function was attended by approximately 70 members with their wives and children. From all reports it was a thoroughly enjoyable evening.

Bowls

To date, the V.R.I. Social Bowling Club has played five games of its 1968-69 itinerary. Two country clubs have been visited—Seymour and Maryborough—and in the metropolitan area, games have been played at Toorak, Murrumbeena Park and Albert Park - V.R.I. All matches have been well supported, particularly the games played in the country centres, as many as 80 members and their wives going along on those trips. While the increased attendance is most gratifying to the committee, there is still plenty of room for new members. Bowlers should keep in mind that the annual game against the A.P.I. will be played at the Footscray City Club on Wednesday, February 19.

Volley ball

A group of Ararat blokes, reckoning they were going to seed, entered a volley ball team in a local competition, as a means of keeping fit. They were apparently much fitter than they thought, because, although only raw novices at the game when they started, they finished up winning their way into the preliminary final of the A grade section.

Members of the team were:—R. Clough, (Capt.) J. Potter, F. Parker, C. Aggett, F. Mitchell, E. Arend-

shorst, L. Styles, all from the Rolling Stock Branch, and T. Lonergan of the Way and Works Branch.

S. O. S. for basketball coach

This is an urgent appeal to anybody in the metropolitan area who has the knowledge and desire to coach our men's and women's basketball teams. In October we will be sending representative teams to compete in the intersystem carnival in Tasmania, and it has become quite obvious to those interested in the club that if we are to have any chance in this tournament, then our players must obtain some intensive coaching.

International basketball has become a very technical and complex game, and the ability to handle the ball must be supported by an expert knowledge of tactics. Only an experienced coach can help in this sphere.

Our teams play in the Business Houses Competition at the Albert Park Basketball Centre. If anybody who has played, or is playing basketball (international rules), feels he could help, then please get in touch with me on Auto. 2445 as soon as possible.

Billiards

The past year has been a great one for the V.R.I. Billiards Club. Earlier in the year we won the Willis Group Billiards Championship, and just recently we have taken out the snooker pennant. Playing in Section 2 of the Willis Group (snooker) we defeated the strong Anglers in the grand final.

One of the interesting features of our team was the average age of the players—20½ years. That so youthful a team could win a major pennant would seem to assure that we will be a power in the billiards and snooker world for many years to come. Players who represented us in snooker over the year were R. Lawrence, P. Gilligan, P. Scarmadaglia, D. Williams, W. McMahon, R. Corney and P. Lane. Probably the most outstanding performance of the season was turned in by Ray Lawrence who, at 19 years of age, was the baby of the team, and who won 22 out of a possible 24 games played. A most consistent effort, and the V.R.I. Club is to be congratulated on its very successful year.

Printers v Printers

Last month, the V.R. Printers, captained by Harry Holzer (who had just returned from Vietnam), played their annual cricket match against a team from the Government Printery. It was an enjoyable affair, held in good weather at Strathmore Oval, but the Government team proved too good for the V.R. chaps, scoring 136 against the V.R. 105. (J. M.).

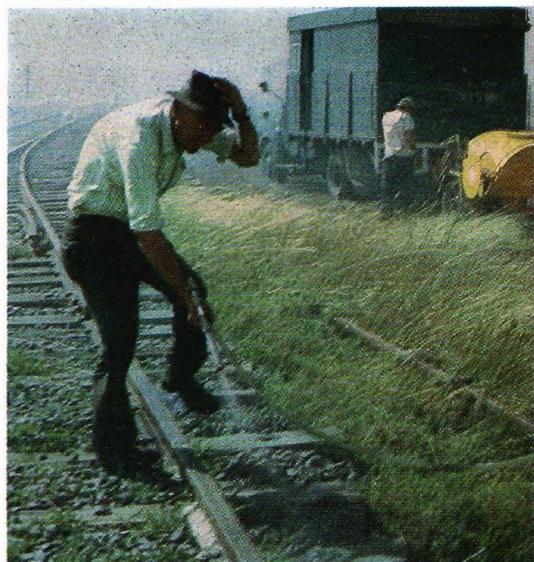
VICTORIAN RAILWAYS

NEWS LETTER

FEBRUARY

VR

1969



**SPECIAL REPORT
ON BUSHEIRES**

V.R. CHAIRMAN COMMENTS ON

THE FIRES

Once again, the State of Victoria has suffered tragedy and destruction from the scourge of bushfire. During the recent emergencies railway men and women together with the staff of other organizations, rose to the occasion and freely gave of themselves and their time in both fighting the fires and rendering assistance to those in need, but despite the efforts of so many, life and property were lost.

As a Department we suffered extensive damage to timber bridges, telephone poles and sleepers, but I am sure that this would have been even greater had it not been for the fire precautions that we had already taken. In fact, at Lara, the extensive firebreaks prepared around the station and railway houses, prevented loss of our buildings.

As the summers pass, it never ceases to amaze me that in a State which is as fire prone as Victoria, so many people accept the fact that, year after year, property will be destroyed and lives lost by bushfires, and little is said or done

to provide adequate safeguards against the ravages of fire.

An instance of the public's attitude can be seen in the number of firebreaks provided outside railway property.

Each summer we burn or poison 9,000 miles of firebreaks along railway lines, at a cost to the Department of \$500,000, and in addition, a grant of \$30,000 per year is made to the Country Fire Authority for its assistance.

Property owners adjacent to railway lines, last year provided only 635 miles of firebreaks, and this year, 648 miles.

In collaboration with the Country Fire Authority we prepared a pamphlet setting out the type of firebreaks which should be provided and action to be taken to clear land of combustible material. This we distributed to every landowner adjoining the railway line.

It is obvious from the foregoing that this Department is taking all necessary precautions, but the pity is that many people make no attempt to reach anywhere near that standard.

Dynon and save unnecessary mileage to and from Melbourne.

Rail vehicles containing tinsplate for making cans, coming from Port Kembla to the Goulburn Valley area, are bogie exchanged mainly at Wodonga. The return traffic comprises tinned fruit and milk products from the Goulburn Valley to Sydney and Brisbane.

At South Dynon, B.E. operates up to three shifts each day and, last year, changed the bogies of more than 30,000 rail wagons. At Wodonga, about 3,000 vehicles were bogie changed.

Level crossing fines

In some recent Court cases involving level crossing incidents, motorists were fined

- \$40 at Shepparton Court, for failing to stop when there was danger of collision with a train;
- \$20 at Colac Court, for crossing a railway line when a train was approaching;
- \$15 at Heywood Court, for failing to give way to a train.

1,800 trips

Mr. L. Rodda, of Warracknabeal, is certainly a consistent rail patron. According to the Warracknabeal *Herald*, he has made 1,800 trips to Melbourne by train in the past 33 years.

New all-lines ticket

A 14-day all-lines ticket for the metropolitan system was introduced from January 26.

Metropolitan all-lines tickets were previously available only on a monthly, quarterly, half-yearly, and yearly basis. It was felt that the addition of a 14-day ticket would cater for tourists, and also be convenient for business people.

The new 14-day metropolitan all-lines tickets cost \$10. Other rates are: monthly, \$20; quarterly \$50; half-yearly, \$90; and yearly, \$170.

The Department also offers 14-day country all-lines tickets that cost \$40 first-class and \$30 second-class.

Shifting the harvest

At the end of last month, as *News Letter* went to press, the railing of this season's wheat harvest that was in excess of the capacity of country silos was almost completed. It in-

cluded the moving of an estimated 40,746 wagon loads.

An all-time record was made during the week ended January 11 when 6,403 wagons were loaded.

At the same time as the wheat harvest there was a heavy demand for wagons to move other grain. Up to January 23, 4,118 wagon loads of oats (63,830 tons) were moved, of which 2,936 wagons were unloaded at Portland for export while 4,288 wagon loads of barley (77,184 tons) were railed to Geelong and other storages.

Bogie exchange

Since the V.R. established a bogie exchange centre at Wodonga, five years ago, delivery time for much of Goulburn Valley freight to and from New South Wales has been cut by up to 48 hours.

The Wodonga centre was established on January 13, 1964, to ease pressure on the B.E. centre at South

FRONT COVER

January's bushfires, which took 21 lives, badly affected some railway services. Top shows the smouldering heap that had been the hay and grain store of Geelong and Cressy Pty. Ltd. on railway land at Lara. Centre left is Geelong's fire train at Bannockburn. Bottom left shows how sleepers blazed. At right, the force of the wind that whipped up the flames can be realized as a trackman fire-fighter at Bannockburn leans into wind, and holds his hat as he operates a knapsack spray; in the background can be seen a yellow water tank hauled to the trackside by a Departmental road vehicle. See story on opposite page.

FIRES RAVAGE RAILWAYS



At No. 1 bridge, near Diamond Creek on the Hurstbridge line, railwaymen extinguish remains of fire. In the background is a water train.

In addition to the tragic loss of life, and the destruction of private property, the bushfires of January 8 caused the worst fire damage to the Railways since the 1944 fires.

The fires

- completely destroyed four bridges near Diamond Creek, thus closing the line between Eltham and Hurstbridge;
- closed 50 miles of track used by passenger and freight trains between Yea and Mansfield, and a further 19 miles of track used by freight trains between Cathkin and Alexandra;
- closed the 21-mile branch line (used for freight only) between Castlemaine and Shelbourne;
- damaged sleepers, track, and, in some cases, power signalling and telephone lines in the Manor-Lara, Sunshine-St. Albans, Bacchus Marsh-Ballan, Sydenham-Diggers Rest, Korong Vale-Borong, and Tatyoon areas;
- interrupted train services on the Werribee-Geelong, Ballarat-Geelong, and Melbourne-Bendigo lines.

The fires revealed the rapid self-healing capacity of a self-contained organization such as the V.R. While the fires were being fought, arrange-

SUMMARY OF DAMAGE

Altogether, the fires

- destroyed 14 railway bridges, up to 240 ft. long and 30 ft. high, and damaged three others;
- damaged or destroyed 12,000 sleepers and 45 miles of track;
- put nearly 80 miles of track out of use, and seriously disrupted services on other lines;
- disrupted power supply for signalling, and extensively damaged telephone pole lines.

ments were made by the Traffic Branch to bring in, where possible, emergency bus services. Engineers immediately inspected and reported on the damage. After consideration of various alternative plans, decisions were promptly made on the quickest methods of restoring services to the public. Necessary materials were either specially reserved for the jobs, diverted from routine maintenance, or urgently procured from suppliers. As an example, a Sydney company worked emergency shifts to supply the 20-ft. diameter Armco corrugated

steel culverts for replacing two of the Eltham line bridges. Men were diverted from their normal jobs, and sent to the fire-affected districts. Necessary bulldozers, cranes, etc., that were not available in the Department, were hired from contractors.

It was decided to rebuild two of the Hurstbridge line bridges, and use the Armco culverts to replace the other two. As *News Letter* went to press, this work was approaching completion.

On the Mansfield line, deviations were built around four burnt bridges between Cheviot and Molesworth. This enabled train services to be restored on the Mansfield and Alexandra lines on January 14.

Following the extensive damage to five miles of track and the destruction of four bridges, the line from Maldon to Shelbourne has been closed indefinitely.

It is worth noting that firebreaks, made as part of the normal precautions by the Railways, prevented the fires causing even greater damage.

Fire-fighting services

Over the last 10 years, the Victorian Railways have been expanding facilities to fight fires, either on railway property or adjacent to it. Two fire-fighting trains, with special equipment and a total capacity of 57,000 gal., are on stand-by at North Melbourne during the fire season. A fire-attack wagon has been built. It is equipped with a large pump,

monitor water guns, and electric generator set, and has facilities for rapid filling of road fire units. In addition, there are 14 pairs of locomotive tenders (126,000 gallons) on call for locating at danger points as required by the Country Fire Authority or the Railways, and 158 domestic water wagons with capacities from 1,500 to 2,300 gal. These can be called immediately for fire fighting, as was done at Geelong for the Bannockburn fire (See photograph).

This season, railway fire trains have also been used at the Lara, Diamond Creek, and Albion fires.

The Railways also provide, at short notice, emergency water supplies for town fire-fighting purposes, from the many Railway reservoirs situated throughout the State.

Railway district officers have been instructed to work closely with Country Fire Authority Officers in connexion with fire prevention and mutual aid.

Hundreds of Country Fire Authority posters have been displayed on country stations, and a leaflet giving information on fire protection and fire breaks, has been printed by the Department and issued to land owners adjacent to railway lines.

All the Department's main and branch-line diesel locomotives have been fitted with cyclone type exhaust manifold spark arresters.

Passengers helped

The following two letters from members of the public show their



Gang works on a deviation near Molesworth, on the Mansfield line. The burnt bridge was at the left of the track.

appreciation for help given by Railway staff in a time of crisis.

I had occasion to use the Victorian Railways for the first time, last Wednesday, the day of the disaster of the bushfires at Lara. It was the 6.14 from Footscray to Geelong, the rolling stock was terrible, but I have nothing but praise for your staff.

The young woman booking clerk at Footscray went to great lengths to find out if the train was running. The journey from Footscray to Wer-

ribee was uneventful; from there on the delays got more frequent and longer.

At all times the guard kept us informed of what was happening, and on one long delay the train crew pooled their own tea, sugar, and milk, obtained hot water, and gave every passenger a cup of tea. As most of us had nothing since mid-day, you can guess it was appreciated.

What a sacrifice to make, because none of the crew would be in a position



Geelong's fire train, shown near Moorabool, speeds to a fire at Bannockburn.



Burnt pole on the Hurstbridge line

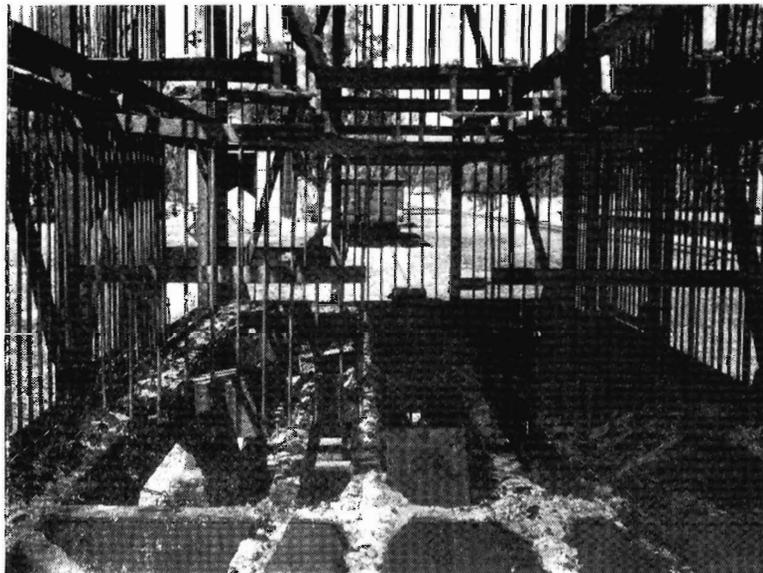
to know what time they would be relieved to get a cup of tea.

At Little River we were finally brought to a halt; then another of the staff went to great lengths to explain why we could go no further, and to inform us buses were being sent to take us to Geelong.

I arrived home at midnight, tired but full of praise for a group of people that had experienced a trying day, kept their good spirits, and paid great attention to the comfort and safety of their passengers.

If you would be kind enough to pass on to the staff concerned, the appreciation of the passengers for a job well done, it would be a small reward for their efforts.

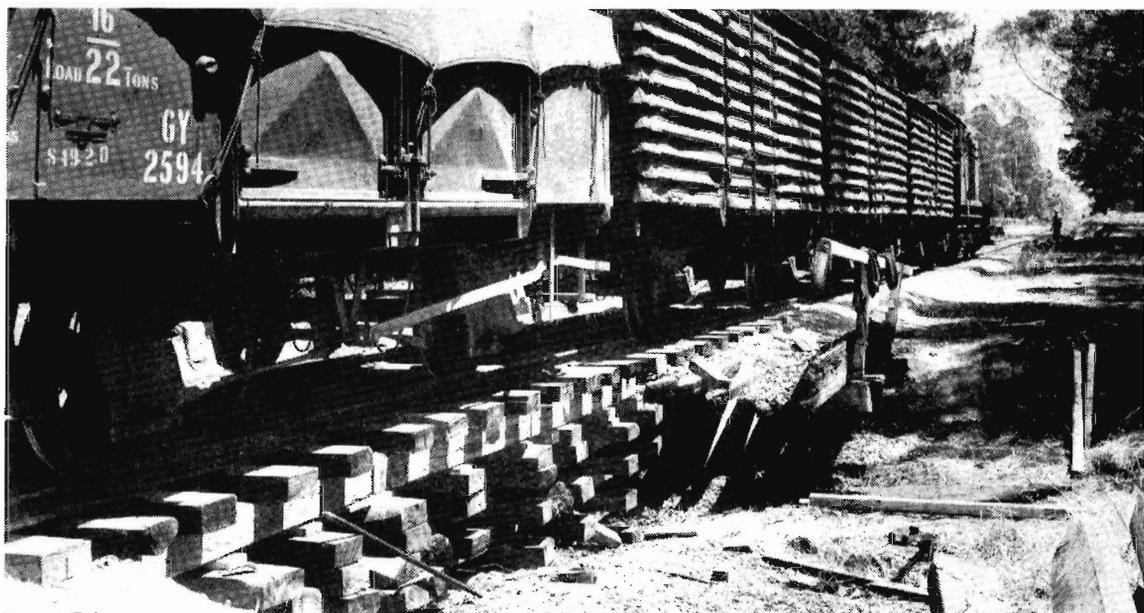
—G. W. Tant, Purnell Road, Corio, writing to the Commissioners



Burnt sheep van at Cheviot

This brief and rather inadequate note is to thank you very much for the assistance and help you gave me over the 'phone on January 8. At the time, I was ringing to speak to my sister who was in a train that had been halted on the Werribee line because of the fires. My mother was very anxious, and I was very grateful for your trouble in getting my sister on the 'phone for us.

—(Miss) K. Briggs, Tintern Avenue, Toorak, writing to the Stationmaster, Werribee



The first train on the re-opened Alexandra line crosses track on temporary supports at Koriella. The burnt bridge was at right of track.

TRACK MACHINE FOR MELBOURNE YARD

A machine to maintain the tracks in the re-arranged Melbourne Yard will eliminate much of the back-breaking work which used to be associated with this type of job.

Tenders have been invited for the machine which will be similar to the type used for packing country rail tracks. The many complex points and crossings in the yards, however, necessitate the use of a special type of machine.

Trackwork which would normally take a track gang many hours to complete will be tampered by the machine in less than an hour.

The machine will be self-propelled, with a travelling speed of about 10 m.p.h., and capable of ascending steep grades. It will have power assisted off-tracking to enable removal from the rail track in about three minutes. Operation will be by one man.

When not in use in the yard, it could be used for maintenance work in other areas.

NEW FARES

New suburban and country rail fares operated from January 26. However, not all fares were increased; some actually fell. The increases were the smallest in post-war years.

Freight and parcels rates were not increased.

Passengers travelling off-peak on Mondays to Fridays from suburban stations to Melbourne still receive a 30 per cent reduction on normal fares with a minimum fare of 25 cents.

Following the decision late last year, that tram and train fares were to be increased, the opportunity was taken to completely overhaul fare scales. Anomalies that had crept in over the years were eliminated, and recognition given to the expanding metropolis, with an extended *commuter zone*.

The one-class suburban scale of fares was extended to include all stations within 32 miles of Melbourne.

This embraced stations as far as Little River, Bacchus Marsh, Clarkefield, Wallan, Yarra Glen, Officer, Clyde, and Baxter.

Passengers from these stations now pay less than previously for single and return fares, and, for the first time, are able to obtain weekly periodical tickets and concession off-peak tickets.

Extension of the suburban fare area has necessitated a review of the fringe zone between the suburban and country districts.

Consequently, single and return fares have been reduced at many stations in the *commuter zone* outside the 32-mile suburban area, and for country journeys up to 50 miles, but periodicals have been increased.

No increases have been made in the daily fares for suburban journeys passing through Melbourne or reversing direction of travel at a junction station, except on Sundays when special low excursion fares apply.

Where higher fares do apply, the increases are moderate.

For the majority of passengers using periodical tickets the increase is less than five cents a day. With a yearly periodical ticket, the increase amounts to only two or three cents a day.

For casual suburban travellers to Melbourne, the cost has been increased on a graduated scale according to distance travelled. Single tickets rose by one to three cents, day return tickets by two to five cents, and off-peak tickets by one to four cents.

Concession travel granted to pensioners continues at half fare. The maximum increase for any pensioner's ticket is three cents.

For pensioners moving on two lines, or through Melbourne on Mondays to Saturdays, there is no increase at all.

Increases in longer-distance country fares are also moderate. For example, the second-class return fare from Ballarat to Melbourne has increased by 20 cents; from Bendigo by 30 cents; from Sale by 40 cents; from Warrnambool by 45 cents; from Hamilton and Horsham by 55 cents each; and from Mildura by 65 cents.

Railway charges were last increased on August 14, 1966. Since that date, the annual wages bill for the Railways has increased by \$9½ million, apart from the increase in the cost of materials.

SPENCER STREET

Latest figures show that the average number of passengers using Spencer Street station daily is about 34,000, and the maximum at holiday peaks in the region of 56,000. Normally, the maximum number of trains arriving and leaving daily is 668, including 584 suburban electric trains and parcels coaches. Last financial year, the station had a passenger revenue of almost \$2 million, and parcels traffic brought in another \$1 million.

Arapiles station derives its name from Mt. Arapiles which, in 1836, was named by Major Mitchell after a place in Spain overlooking the battlefield of Salamanca. Mitchell ascended the mountain on the anniversary of the battle.

Monash men at Mine

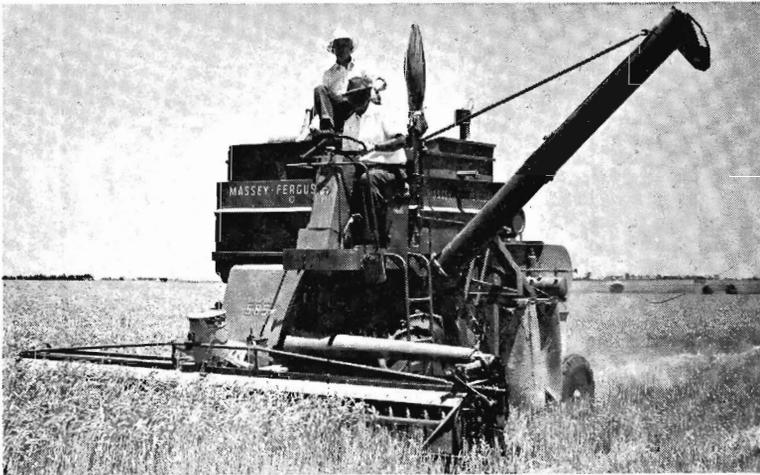


Shown at the Kirrak Mine, this rail motor carried members from the Monash University Railway Club to the Kirrak area at Wonthaggi on December 20, the day the State Coal Mines closed.

MINISTER SEES WHEAT HANDLING



Shown near Bealiba as it speeds towards Dunolly, is the train on which Mr. Wilcox travelled for part of the way.



At Cope Cope, Mr. Wilcox rides on a header driven by Mr. Bugge.



At the Dunolly sub-terminal, Mr. Wilcox rakes wheat from a GY wagon. In background is Jim Sisson, a student from Maryborough.



After inspecting the Dunolly sub-terminal, Mr. Wilcox (left) thanks Mr. Ken Anderson, O-in-C of the sub-terminal, and Mr. Tom Hilder, Dunolly's stationmaster (centre).

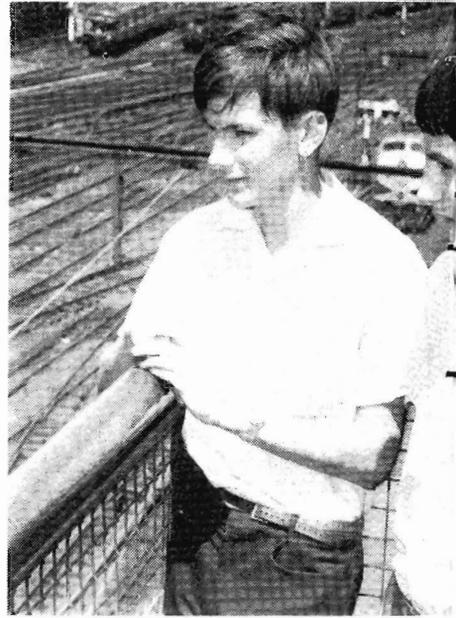
Last month, the Minister of Transport, the Hon. Vernon F. Wilcox, M.P., toured the Dunolly district to see how the wheat harvest was moved from farm to seaboard.

During his inspection, Mr. Wilcox:

- saw Ballarat Train Control Centre;
- rode on a header with Mr. Les Bugge as he stripped a paddock of his property, *Kan-imbla*, at Cope Cope;
- travelled in the bulk grain truck, driven by Mr. Bugge's son Robert, from the farm to the silos at Swanwater railway siding;
- saw the wheat unloaded from the truck; and wheat from the silo loaded into a GJX bulk grain rail wagon that, with others already loaded, was picked up by a train bound for the seaboard wheat terminal at North Geelong;
- travelled in the train to St. Arnaud;
- inspected the 10½ million bushel sub-terminal at Dunolly, where 7 million bushels of wheat had been unloaded.

While at Dunolly, Mr. Wilcox tried his hand at raking wheat from a GY wagon into the hopper below. He also climbed the tower and walked along the overhead gallery to see wheat lifted from the hoppers up the tower and transferred by belt to the bulk wheat shed.

"The inspection turned out to be as interesting as I thought it would be. The job done each year by the railways with the wheat harvest, is amazing," said Mr. Wilcox.



NEW PLATFORM HEIGHT: A passenger alights at the new Canterbury station. This is the first platform to be built with a height of 3 ft. 9 in. above rail level. Where possible, this standard will be progressively adopted when new platforms are provided, when renewals are being undertaken, or when extensive repairs to existing platforms are required.

APPRENTICES WELCOMED: William (left); new apprentices welcomed to the Department Chairman of the Staff Board (right). The apprentice, and their grandfather an engine apprentice electrical fitter (workshops), and...



AROUND
THE
SYSTEM



AT WONTHAGGI: No sooner had K 170 been placed in position than waiting children eagerly clambered on to it. The locomotive is permanently preserved in the town.





and Gregory Ludge were among the 230
ment, last month, by Mr. C.S.Morris,
father of these two lads was a railway
driver at Korumburra. William is an
Gregory an apprentice fitter and turner.



STREET SWEEPER: This street sweeping machine has been obtained for use at Dynon and Melbourne Goods areas.



NEW LOCO: The first H class diesel-electric locomotive enters service near the site for the hump in the re-arranged Melbourne Yard where the locomotive will ultimately work. It is a re-classified T class loco with modified controls for hump shunting (see January *News Letter*, p. 2).

STATE COAL MINES CLOSE

On December 20, mining ended at the State Coal Mines, Wonthaggi. On that day, the Deputy Chairman of Commissioners, Mr. E. P. Rogan, made a special trip to Wonthaggi to meet the miners as they came up from their last shift in the mine.

"I thank you and all the men before you, for the part played in fuelling the Railways," he told the miners.

The closing of the State Coal Mines ended an era of nearly 60 years of mining at Wonthaggi. The remaining mine that closed on December 20 was in the Kirrak area about three miles from the town.

The existence of large coal-bearing areas in southern Gippsland was known in the early 1840's, but the fields were not exploited. During the 'fifties, Government and private enterprise conducted surveys, borings and other investigations on these fields. Small quantities of black coal were mined, but lack of adequate transport prevented any successful extension of production.

During the early investigations for coal, Government bores had been sunk about the Powlett River region in 1858, near the site of what later became the State Coal Mines. Boring was resumed in 1908, and some large coal seams were located. A shaft (No. 1) was sunk, from which a small quantity of coal was mined for test purposes.

When a strike of New South Wales coal miners completely stopped supplies late in 1909, Victorian industries were so seriously affected that the Government found it imperative to obtain as much local coal as possible for the Railways and other essential services. Under conditions bordering on a state of emergency, the Wonthaggi mines came into being. The No. 1 shaft, idle since the test samples were taken, was re-opened on November 22, 1909. Coal winning operations were so expeditious that the first consignment for Melbourne was dispatched three days later.



Mr. M. Campbell (Secretary of the Powlett River Branch of the Coal and Shale Employees Federation) is holding a lump of the last load of coal from the State Coal Mines. Others in the picture are (from left) Messrs. E. P. Rogan (Deputy Chairman of Commissioners), J. Byrne (General Manager, State Coal Mines), and W. Tait (President, Powlett River Branch).

There being no railway, the coal was hauled 10 miles by bullock wagons to Inverloch for transport by sea.

Railway extended

Three additional shafts were sunk adjacent to No. 1. The increased output being greater than the primitive transport methods could handle, coal began to accumulate in large quantities. In December 1909, Parliament authorized the extension of the Nyora-Woolamai railway to Powlett Coal Field (State Coal Mines)—a distance of 13½ miles. Construction was so rapid that the railway reached the mines on February 22, 1910, and the first coal was

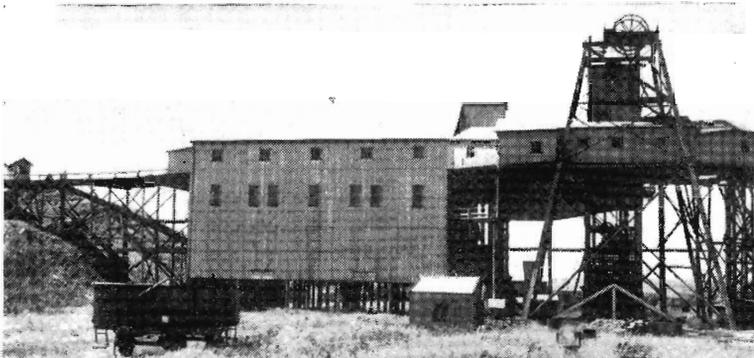
sent to Melbourne by direct rail the next day.

By this time, 2,500 persons were living at a Government "canvas town" near the mines. The town of Wonthaggi (aboriginal for "to carry" or "to procure") was laid out in March 1910.

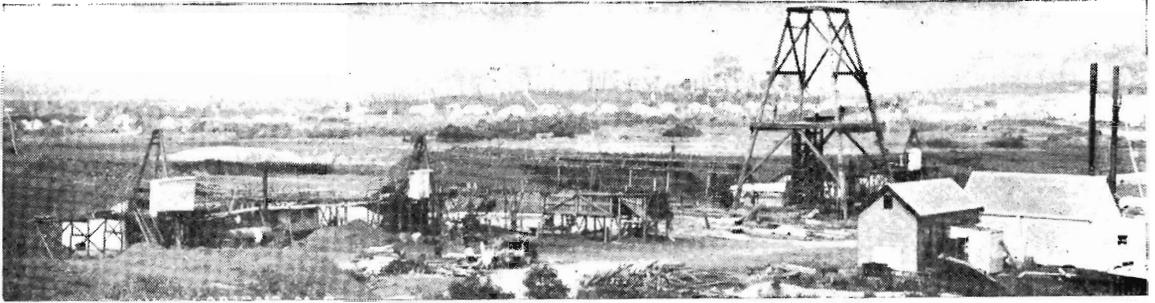
In the first year of production, 41,000 tons of coal were extracted. The highest yearly output was 662,000 tons, in 1930. A progressive decline followed, and over the last few years about 40,000 tons have been mined annually. Thermal efficiency of the coal was substantially below that of high grade New South Wales locomotive coal.

Control of the State Coal Mines was vested in the Railways Commissioners on July 1, 1911. Local working was directed by a General Manager. The staff, comprising all the grades associated with coal-mining operations, reached a peak of 1,821 in 1926.

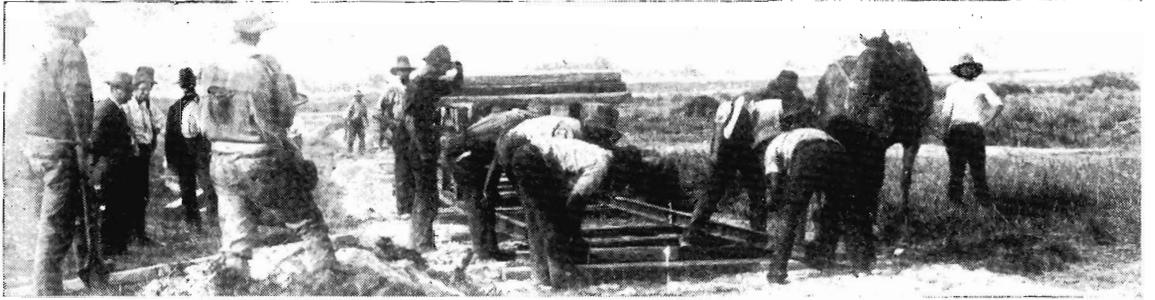
Though the mines incurred heavy financial losses, their true value to the State was in providing a substantial coal supply independent of interstate fields and shipping. This has been appreciated by every Government since the State Coal Mines were authorized, but this aspect has had diminishing importance as oil and brown coal development reduced the State's dependence on imported black coal.



The Kirrak mine



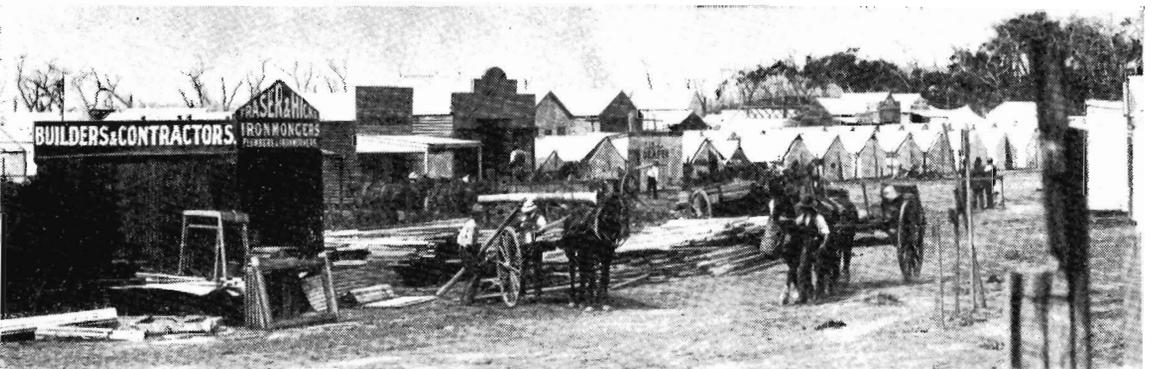
The Powlett River Coal Field in 1910 (Wonthaggi)



Completing the line to the Powlett River Coal Field, February 22, 1910



The first coal to be sent by rail from the Powlett River Coal Field is ready to be loaded into the rail wagon.



Described as the "view from Post Office steps" at the Powlett River Coal Field

STRAWBERRIES GO BY TRAIN



On the Brierley property, strawberries are being packed into boxes prior to being cooled and taken to Spencer Street station.

Since early December, Mornington Peninsula strawberries have been moving by train to Sydney, Bendigo, Kaniva, Kerang, Colac, Horsham, Euroa, Warrnambool, and Bairnsdale.

The strawberries are being railed 596 miles to Sydney every Monday, Wednesday, and Thursday, in a ventilated bogie van attached to *Intercapital Daylight*. They arrive in Sydney the same night ready for marketing the next morning.

Strawberries to Victorian country towns are railed on morning passenger trains almost daily. They arrive in less than 24 hours from picking, and are on the menus of hotels, motels and restaurants the same evening.

The peak of the traffic occurs during the week before Christmas and early January when up to 2,000 punnets are railed to Sydney on each of the three days of the week. Larger quantities are railed to the country at that time.

The strawberries, grown on seven acres by J. B. and L. M. Brierley, Mt. Pleasant, Flinders, are picked in the late afternoon, and pre-cooled overnight to 35° before being taken to Spencer Street station.

Continuity of supplies is assured because the Brierleys have more than 100,000 plants growing on the property. Picking is done by university students who have finished their exams.

The Railways captured the Sydney strawberry traffic last season from the airways. Mr. Brierley was so pleased with the speedy service provided by the Railways that he decided to use the train for sending supplies to the country.

"The Railways always get them there in tip-top condition," Mr. Brierley said.

BUS MEN BACK UNDERGROUND

The Road Passenger Service Operators' Association supports moves to build an underground railway in Melbourne. The organization's executive director, Mr. Lindsay C. Freeman, says his association believes the expenditure of \$81 million on an underground is a relatively cheap means of doubling the capacity of Melbourne's railway system.

Mr. Freeman says an underground will enable trains to carry more passengers during peak periods and will probably necessitate some of the members of his association increasing services . . .

"All we ask", he says, "is that the authorities plan now for bus parking bays, signposts, clearways, and adequate traffic lights at, and in the vicinity of suburban railway stations, and along bus routes."

DUDLEY STREET BOX

A reader has inquired about the history of the Dudley Street Signal Box (now replaced by the box in the West Tower, see January *News Letter*). As far as is known, a box was first established on the site in 1884. It was known as Spencer Street Goods Signal Box, and was altered in 1903, presumably for the construction of Melbourne Yard. It is believed that the interlocking machine was replaced in 1914, and, later on, further modifications were made—one when control of the suburban lines was transferred to the Franklin Street Box, and another for an extension of Melbourne Yard to Cowper Street Sheds.

Anderson (Wonthaggi line) was named after a family who were old residents of the locality. One of the family reported the discovery of a coal seam in 1837.

Chief Inspector Pilgrim retires



At the Police Depot, Mr. Pilgrim (left) shows Mr. L. A. Reynolds (Railways Commissioner) the silver baton that was presented to him in 1938, when he graduated dux of his recruit squad. (Photograph; *Police Life*)

Senior Detective C. W. J. Pilgrim, who has retired as Chief Inspector of the Railways Investigation Division, was recently presented with a Chief Commissioner's Certificate by Mr. N. Wilby, then Deputy Commissioner of Police. As usual, the presentation was made at a graduation parade of recruits at the Police Depot.

Addressing the parade, Mr. Wilby said, "You have all seen Senior Detective Charles Pilgrim receive a Chief Commissioner's Certificate. It is a well deserved award for 30 years of valuable service to the Police Department and the Victorian community. Not only has he given service, but by his own personal character and conduct he has reflected great credit upon the Victoria Police Force".

Mr. Wilby then advised the new

recruits to use Mr. Pilgrim's career as an example to follow.

"I've no doubt that he has derived great satisfaction and holds many memories of that 30 years in the force. You will each receive the same satisfaction if you do your job as well as Charlie Pilgrim did his," Mr. Wilby added.

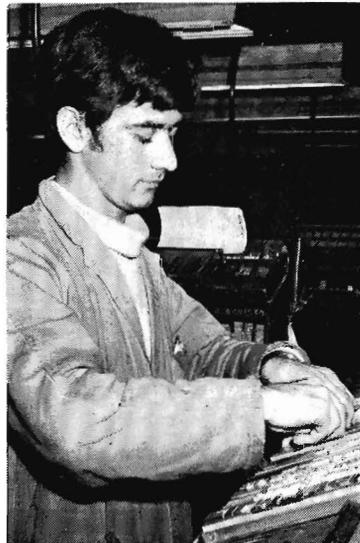
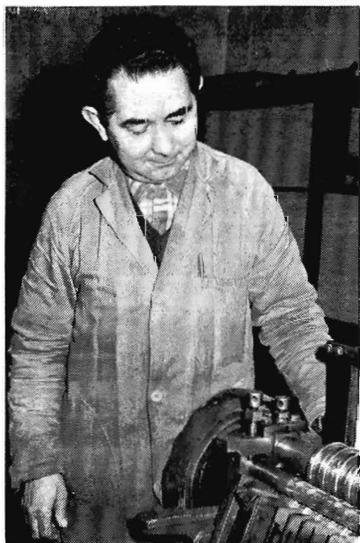
Mr. Pilgrim was seconded to the Railways Investigation Division in 1945, and was Chief Inspector for nearly 16 years.

Cheques for transport graduates



As a bonus for completing the 4-year transport administration course of the Institute of Transport, conducted at the Royal Melbourne Institute of Technology, these five railwaymen have just been presented with a cheque for \$200 each, by Mr. G. F. Brown, Chairman of Commissioners. (From left) Messrs. G. F. Brown, C. A. Gorman, N. E. Kellam, G. E. Smith, P. H. Asker, and R. T. Barden.

From Greece



Phone callers to the Departmental Printery will most likely be answered by the cheerful voice of Mrs. Kathy Chronis (centre). Working inside the Printery can be found her father, Mr. S. Halkias (left), and her brother, Apprentice Compositor Basil Halkias (right). Mr. Halkias and his family came to Australia in 1955. First of them to join the Printery was Mr. Halkias who has worked there for 12 years; Mrs. Chronis started in 1956, and Basil in 1965. Now in his fourth year of apprenticeship, Basil, in his spare time, is a keen soccer player. He plays with Glenroy United; coaches juniors; and has also played Australian rules—with Broadmeadows in the Essendon and District league. Last season he was judged best and fairest player for Glenroy United.

Suburban passes

Consequent on the extension of the suburban area as from January 26, 1969, the Commissioners have directed that *suburban all lines* passes be available to all stations

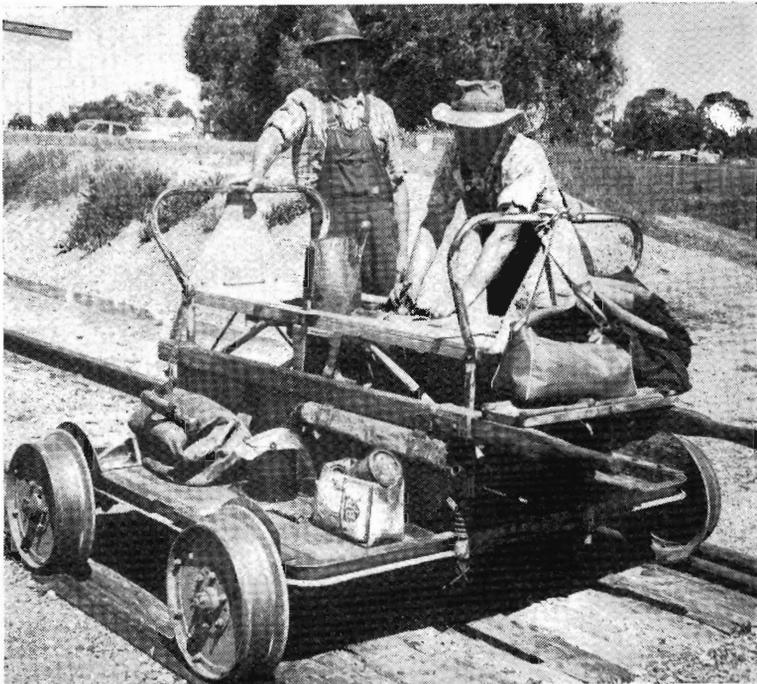
within the extended suburban area as defined in the Passenger Fares and Coaching Rates Book and in addition, to stations on the Healesville and Stony Point lines. Such passes are also to be available for

travel on the Mornington line. They are not, however, to be available on Departmental Road Motor Services.

Privilege tickets will continue to be available for Victorian *country* journeys only.

Privilege tickets are again being issued for the Mornington line.

At Koyuga



Fencer W. Sperling (left) and Ganger E. G. Chandler are about to remove a track motor at Koyuga. Mr. Chandler's gang maintains about 14 miles of track—from Tongala to Echuca. In the last competitions, he received first prize in the non-piped water section for the Seymour district departmental residences. Mr. Sperling is stationed at Shepparton.

Poetic farewell

A framed page of verse, autographed by his colleagues at North Melbourne Workshops, was among the gifts presented to Mr. F. J. Duke when he retired as a boiler-maker after 50 years service in the Department.

Mr. Duke began with the railways at the Signal Shops, Newport, on May 1, 1918. After a period in the Way and Works Branch, he went to the North Melbourne Loco Depot in 1927, and remained in that area until retirement.



Mr. Duke and his framed page of verse.

43 years at S.G.S.



There was a good muster in the Bookkeeper's office at Spotswood General Storehouse, to farewell Miss Phoebe Tickle—one of the Storehouse's best known personalities. Miss Tickle had worked in the Bookkeeper's office ever since the Storehouse was opened—in 1925. The gifts she was presented with included a bouquet for which almost every member of the staff had brought in one or two flowers. Ballroom dancing, fishing, reading, and travelling keep Miss Tickle young in mind. She has been to Perth 15 times, has travelled all around Australia, and is now considering a visit to the United States. Miss Tickle is shown being presented with the bouquet by Mr. A. W. Thomson, Comptroller of Stores.

One of the first

Miss Eileen Kinsella, typist in the Secretary's Branch, who retired last month, joined the Railways on October 1, 1923, and was one of the first four girls recruited by the Department for typing. Up to that time, all the typing was done by youths and men. Three years after joining, Miss Kinsella began typing Commissioners' correspondence and has been engaged on it ever since. At the time she started, Mr. (later Sir) Harold Clapp was Chairman of Commissioners. Altogether, she has worked for five Chairmen. In retirement, Miss Kinsella hopes to have more time for her lifelong hobbies of water colour painting and gardening.

Melbourne Goods, Spencer Street Parcels

I would like to express my appreciation for the courteous, and helpful service I have always received from your staff. The staff in the Outwards Parcels at Spencer Street are always pleasant to deal with.

Recently I had occasion to make inquiries at the Goods Yard, Spencer Street, about goods to Colac. The people at the Goods office were very efficient and patient, especially a Mr. Devine . . .

—(Mrs.) N. Hallifax, Hyslop Street, Burwood, writing to the Commissioners

A.R.H.S. Tour

I would like to bring to your notice the excellent steam enginemanship exhibited by Driver Jim Silletto and his mate, on the occasion of the Australian Railway Historical Society tour to Wonthaggi and Kirrak, on December 14.

The climbs to Nyora from both directions were masterly, and a lesser man would have abandoned the trip over grass covered track between Wonthaggi and the mine at Kirrak . . .

—Leslie Poole, Oak Avenue, East Malvern, writing to the Commissioners

RETIREMENTS

TRAFFIC BRANCH

Conroy, L. P., Footscray Group, 9/3
Boucher, C., Melbourne Yard, 12/3
Davidson, H. B., Melbourne Goods, 12/3
Gemmill, C. A., Echuca, 13/3
Leahy, E. W., Melbourne Goods 13/3
Bracksley, N. S., Melbourne Goods 17/3
Lewis, J. H., Melbourne Goods 21/3
Yemm, L. M., Flinders St. E. Box, 22/3
Bull, S. E., Bendigo, 23/3
Mitchell, L. R., Flinders St., 27/3
Tosic, V., Melbourne Goods, 28/3
Kennedy, D. E., Geelong, 24/12
Morelli, A. W., Port Melbourne, 2/12
Smith, F. A., Euroa, 13/12
Hall, H. C., Flinders St. B. Box, 2/1
Ormsby, G. F., Oakleigh, 3/1
Mathers, (Mrs.) M.M., Sth. Yarra 21/12
Dolan, G. H., Frankston, 1/1
Callick, L. W., Lilydale, 1/1

RETIREMENTS (continued)

ROLLING STOCK BRANCH

Melville, F. P., Newport, 1/3
Nicholls, R. F., Portland, 6/3
Hosking, A. E., Nth. Melbourne, 7/3
Prebble, E. C., Bendigo Nth., 7/3
Lockett, W. J. A., Bendigo Nth., 8/3
McArdle, J. J., Ballarat Nth., 8/3
Cameron, A. J., Jolimont, 22/3
Farrell, E. J., Jolimont, 23/3
Mitrovic, M., Shelter Shed, 23/3
Cerra, F., Nth. Melbourne, 24/3
McCormack, M. A., Sth. Dynon, 24/3
Richards, A. E., Head Office, 25/3
Wyatt, E. A., Newport, 27/3
Hennett, A. G. R., Jolimont, 28/3

WAY AND WORKS BRANCH

Blake, F. W. A., Camberwell, 11/3
Brittain, S. C., Head Office, 24/12
Clark, M. W., Heywood, 4/12
Feltham, H. G., Benalla, 27/11
Ford, M. E., Warragul, 13/12
Lane, A., Garvoc, 13/12
Lynch, J. V., Ballarat, 19/3
Marr, W. J., Talbot, 4/10
Mathison, E. R., Camberwell, 20/3
McDonald, A. F., Maldon, 14/3
McDougal, R. D., Jewell, 2/3
McFarlane, E. P., c/o Foreman Painter 13/11
Sinclair, V. F., Murchison, 29/3
Statikopoulos, A., Ironworks Div., 9/12
Steinmach, Q., S'wood Workshops, 13/3
Tokatlidis, L., Power Sigs. C'field, 2/12
Villani, A., Laurens St., 4/3
Whitton, J. H., Colac, 28/3
Wood, A. E., Koroit, 20/12

STORES BRANCH

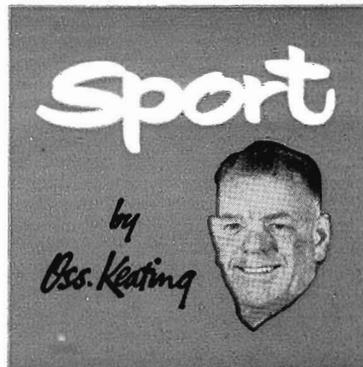
Rose, W. H., Dynon, 1/11
Lettiere, M., Newport Workshops, 6/11
Rogotowicz, W., Perm. Way Depot, 10/2
Tabacco, C., Perm. Way Depot, 13/2
McCann, J. B., Spotswood W'shops, 16/2
Grant, N. S., Reclamation Depot, 24/12
McElhinney, R. R., Spotswood W'shops Storehouse 5/3

ACCOUNTANCY BRANCH

Deakin, J. D., (Miss), Head Office, 25/11
Merrifield, W., Ticket Supply, 20/1
O'Haire, R. E., North Loco. A. O. 31/1

ELECTRICAL ENGINEERING BRANCH

Harvey, R. S., Head Office, 12/3



Tennis

The completion of the home and home matches saw Melbourne Yard and Commercial finish on top of the list with four wins each, followed by Sunshine with three wins and Codon with one win. Melbourne Yard and Commercial will now play off for the Dunkling Shield while the winner of the Pimm's Cup will be decided at a later date.

Of interest to all tennis players should be the cabaret night arranged by the V.R.I. Tennis Association for March 14. Bookings should be made through either Messrs. B. Pearce or M. Harford who can be contacted on auto. 1812 and 1224 respectively.

Although it is still 12 months away, players should remember that the next intersystem tennis carnival will be held in Perth, late in February 1970. Those interested should start planning for the trip now. Further information can be obtained from Bruce Pearce, Hon. Secretary, V.R.I. Tennis Association, auto. 1812 or the Sports Secretary, V.R.I., auto. 2445.

Golf

The following golfers will represent Victoria at the intersystem carnival in Melbourne next April-May. J. Kennedy (Captain), L. Barlow, E. Brown, S. Climpson, D. Forde, A. Jack, E. Kenny, A. Knight, J. Manning, L. Petch, I. Patterson, W. Short, H. Whelan, and B. Williams. Jack Kennedy was appointed manager of the Victorian party, and Jack Williamson will be the Institute representative at the Carnival. The last time Victoria was successful in the Commissioners' Shield and the Tintara Cup was way back in 1957 at Adelaide. The selectors are hopeful that the side named above will be good enough to end the twelve year drought. Incidentally, the courses to be used during the Carnival are Common-

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

TRAFFIC BRANCH

Brown, G. H., W. Richmond, 11/1

ROLLING STOCK BRANCH

Paraskevas, P., Shelter Shed, 12/12
Prielaídas, M., Jolimont, 12/1
Cochrane, A. J., Wodonga, 20/12

WAY AND WORKS BRANCH

Hooley, G. W., Traralgon, 7/12
Medwik, A., Maffra, 19/12

STORES BRANCH

Obradovic, M., L.S. & T. Store, 18/11
Bennett, B. L., Maryborough, 23/11

wealth, Keysborough, Kingston Heath, Kingswood, Metropolitan, Rossdale, Victoria and Yarra Yarra.

Cricket

The team to represent Victoria at the 1969 intersystem carnival in Perth from February 20 to March 7 is as follows: Stan Wallis (Ballarat) Captain, Les. Hill (Caulfield) Vice-captain, Lou. Balcombe (Boort), Ted Barnes (Flinders St.), Brian Brister (Ashburton), John Cooney (Flinders St.), Bob Coyne (Dimboola), Tony Evans (Huntingdale), Ron Jenkins (Head Office) John McAllister (Melbourne Yard), Kevin Schickerling (Warragul) and Harold Worsley (Ballarat). John Cooney has been appointed manager of the team—as well as being available to play if required—and Bill Crowe was appointed by Council as the Institute representative for the trip. On paper, it looks a pretty good side, and should be capable of improving on our 1967 performance in Brisbane.

I think another item of interest to the Railway cricketing world, and to Bob Figgis's many friends in particular, was Bob's appointment as a Sheffield Shield Umpire during the current season. This achievement is even more remarkable when it is remembered that Bob, who is a clerk in the Stores Branch at Newport Workshops, is only in his second season as an umpire in the district competition, and I think it speaks volumes for his ability that he should be picked for this honour after such a short apprenticeship. It is not so long ago that he was a permanent member of our carnival teams, and he had the distinction of winning the all-Australian bowling average in the 1965 Melbourne Carnival. At the present time Bob is president of the V.R.I. Cricket Association. Congratulations, Bob, and I sincerely hope that it is not too long before we see you "standing" in a test match.

Basketball

An invitation has been received from Tasmania to compete in the next intersystem basketball carnival which will be staged in Hobart from October 26 to November 5, inclusive. The invitation covers both a men's and a women's team. As it is the intention of the V.R.I. basketball club to form the interstate squads no later than the end of March, players, both men and women

who would like to participate in this carnival are asked to contact Mr. Dennis Kerby (auto. 1403) who is the hon. secretary of the basketball club or myself, c/o V.R.I. (auto. 2445) no later than March 28. Further details will be published as they come to hand.

Country Weeks

Country Cricket Week will be held on March 17-21 on the turf wickets at Royal Park. Country Tennis Week will be held on March 24-28 on the V.R.I. courts at Parkville. Entries for both cricket and tennis close on Monday, February 24.

Rugby

Ballarat Goods Sheds can boast a Rugby player of some note—Goods Trucker John Darroch, from New Zealand. Since arriving in Australia four years ago, he has played Rugby in N.S.W., S.A. and Victoria. He coached North Adelaide in 1966 and 1967, and represented S.A. at the Perry Lakes Stadium Carnival in 1966. After coming to Victoria last year, John joined the Kiwis Rugby Union Club and played in seven Victorian sides including one against his own countrymen—the famous All Blacks. (J.M.)

Bowls

The fourth Transport Industries Bowling Tournament was played at the St. Kilda Bowling Club on Sunday, January 26. Host for this year's fixture was the Melbourne and Metropolitan Tramways Board. Competing organizations were Ansett-ANA, Australian Postal Institute, Department of Civil Aviation, Trans-Australia Airlines, Victorian Railways Institute and, of course, our hosts. Winners for the third time in the four years the tournament has been played, the V.R.I. team has built up a record they can justifiably be proud of. Skippers of our winning fours were Alan Cowling (who was also runner up for the best rink trophy) 4 wins, Ray Laycock, 4 wins, Don Howard, 3 wins, and Gil Sergeant 3 wins. Altogether an excellent team effort.

In recent weeks, the V.R.I. Social Bowling Club has visited Footscray City and Sunshine V.R.I. Bowling Clubs, and a good attendance at each game was recorded. Winning rinks were: at Footscray—C. Voutier, M. Penny and E. K. Williams (Skip.); and at Sunshine—W. Fairweather, H. Walker and H. Watts (Skip.).

V.R.I. chess club

The V.R.I. chess club, formed last April, has held its first championship tournament. It was won by John Meke; Alex. Crusiz was runner-up. Special prizes were

awarded to James Hurley, Joe Pawinski, and Eric Martin.

The club has a membership of 18, and meets in Room 11, V.R.I., Flinders Street, every Tuesday, from 1 p.m. to 4 p.m. At present, a B grade tournament is in progress.



(From left) Mr. E. P. Rogan, Deputy Chairman of Commissioners, presents chess tournament trophies to Messrs. J. Meke and A. Crusiz.

VICTORIAN RAILWAYS

NEWS LETTER

MARCH



1969



V.R. CHAIRMAN COMMENTS

Friday February 7, was indeed a black day for Australia's railways, more particularly Victoria's, for, in this State, nine persons died—four of them railwaymen—when *Southern Aurora* hit the 1.25 a.m. down freight train head on at mileage 104.40.

It was the first accident with passenger fatalities for almost 17 years; then on April 12, 1952, one passenger died at Moriac, in the Western District.

The tragedy found railway men and women, as is ever the case when they are put to the test, ready, willing and able to give of their best. They rose to the occasion in true railway tradition.

To our staff, we, the Commissioners, are fully grateful. To all the outsiders who gave such wonderful and untiring help we are all grateful.

The imminent crash was seen on the C.T.C. board in Melbourne and an immediate alert was able to be sounded, which no doubt resulted in speeding the help, that included Stage Two of the State Disaster Plan; this plan, too, met the test.

Much has appeared in the news media about safety devices, but with the pending report from the Department's Board of Inquiry and the

Coronial Inquest, we could make no comment.

We can say to you, however, that most safety devices quoted are known to have some deficiencies, if only that they can be made inoperative, deliberately or accidentally, by some of the very people they would protect. Other devices reported as safety devices overseas are, in fact, used here.

The pre-eminent safety of rail travel is seen by the press coverage given to *Southern Aurora*—page after page—whereas a similar or higher death roll on the roads or in Vietnam (areas where casualties have become an accepted way of life) rate only a few inches of newspaper space.

The comparative safety of the rail way has been discussed in news media, as well, and the reprint on this page, from *The Australian*, under the heading "It's still safer to travel by train", emphasizes the continued very high safety factor of rail travel.

If statistical proof is needed, then in the last half century only 11 passengers have died in Victorian Railways accidents, during which time well over 7,827 million passenger journeys have been made. That's a risk rate of just one in 700 million. Where else could one have better safety odds?

handle springs back and the brakes are automatically applied. However, these devices are normally confined to suburban electric trains.

But diesel locomotives have other safety features.

All N.S.W. main-line locomotives, including the one that hauled the express to Albury, are fitted with a vigilance system: unless it is activated every 90 seconds a siren sounds in the cabin and the brakes are applied.

The Victorian locomotive which took over the Aurora at Albury was fitted with a similar, though not identical, "vigilant control" system.

What happened in the loco's cab immediately before the collision may never be fully known.

But, whatever their other shortcomings, Australia's railways certainly live up to the claim made by the British author, L. T. C. Rolt, in his book, *Red for Danger*, that "railways are the safest form of transport devised by man".

If you don't believe me, take a look at last week's road accident figures.

OUR STRANGE VALUES

Editorial from Bendigo "Advertiser" 11.2.69

PEOPLE everywhere were appalled by the railway disaster last week in which the death toll as far as is known was mercifully limited to nine.

Such a happening, perhaps because of its unusualness in a system with such a safety record and because of the spectacular if awe-inspiring consequences, invariably captures the imagination of most people to the extent that the crash was on everyone's lips, and telegrams and cables of sympathy poured in from the most distinguished sources.

Almost every aspect of the accident was most dramatically covered by communication media, and the State disaster plan was invoked and quite properly. The result was to make an indelible impact on the public consciousness.

It was almost a coincidence that yesterday's newspaper noted that the Victorian week-end road toll amounted to nine people dead.

But what a contrast in the impact with which this news was publicly received! With the exception of police officers whose duty it is to attend accident scenes and who become involved in all the grim details inherent in such cases, medical and nursing teams in hospitals everywhere and relatives of the deceased and injured, the fact that there were nine road deaths during the weekend caused scarcely a ripple in the public mind.

This despite the fact that week by week similar toll is exacted from road-users to the extent that this year at the current rate Victoria will record 1000 road deaths and anything up to 25,000 people injured.

But who cares? Irresponsibility, carelessness and thoughtlessness continue relatively unchecked.

Because there is no public conscience in this matter and because there is no disgrace attached to driving convictions—often in fact the culprit becomes something of a hero among a class of associates—the road carnage will continue.

Young drivers are often excused on the ground that "you cannot put an old head on young shoulders". Why then put a lethal weapon in immature hands? But imagine the outcry if it were proposed to raise the legal driving age!

So until the public reacts to the road toll in much the same way it did to the Violet Town disaster there is little likelihood of reducing one of our greatest scourges.

IT'S STILL SAFER TO TRAVEL BY TRAIN

*by David Evans
("The Australian" 15-2-69)*

... The amazingly low casualty figures are a tribute to the resilience of the human body—and also to the construction of the Aurora and to Australia's magnificent rail safety record.

The all-steel Aurora was built in New South Wales (it made its maiden run in April 1962) to standards laid down by the Australian and New Zealand Railways Conferences—standards among the highest in the world.

Its stainless steel sides undoubtedly minimised fragmentation in the smash, and its steel base-frames prevented telescoping.

One point made during the week was that the engine hauling the express had no "dead man's handle"—a device on which the driver must maintain pressure when the train is moving. Should he collapse at the controls and relax the pressure, the

From Police Chief

The Honourable the Chief Secretary (Sir Arthur Rylah) has asked me, as the Co-ordinator of the State Disaster Plan, to convey to you his profound admiration and appreciation of the ready co-operation given and assistance afforded other operating services by your Department at the scene of the tragic train collision at Violet Town on Friday, 7th February, 1969.

I have much pleasure in carrying out the wish of the Minister, and would like to add my own deep appreciation of the co-operation and assistance rendered by your officers. This proved a wonderful aid during rescue operations and the various other activities which became necessary.

—N. Wilby, Chief Commissioner of Police, writing to the Chairman

Interference with signal equipment

Last month, the Commissioners asked the public to report any unauthorized people seen near Railway equipment or on rail tracks, to the police, the Railways Investigation Division, or the nearest Railway authority.

The appeal was made following delays to motorists after interference with complex electrical equipment had caused boom barriers and signalling to fail.

As railway staff know, boom barriers and flashing light signals are designed on *fail safe* principles; a failure causes the booms to descend and the lights to operate.

On February 12, tampering at Middle Brighton resulted in four sets of boom barriers between Brighton Beach and Middle Brighton remaining lowered. They were at Bay Street, Church Street, Dendy Street, and South Road.

On the same morning, similar interference between Watsonia and Greensborough affected boom barriers at William Street, and Grimshaw Road.

The Department had flagmen quickly on the scene to direct road traffic. The booms were then latched in an upward position until the equipment could be repaired.

On January 31, signalling between Moonee Ponds and Ascot Vale was interfered with, during the rail strike.

Persons committing such offences risk serious injury or even death from the electrical equipment. If detected, they can expect to be charged to the limit of the law.

Washroom units for works staff

More mobile washroom units with showers, wash basins, and L.P. gas hot water storage will be provided for certain country camped-out staff.

Tenders were called last month for a further three of the units to supplement the four already in use.

They will measure 9 ft. x 7 ft. x 7 ft. high, be shock resistant and clad with aluminium. Interior fittings will include two shower cubicles, two hand basins, and a storage cupboard.

The L.P. gas hot water unit will have a water storage capacity of 30 gallons and be capable of operating on direct mains pressure. Gas capacity will enable the units to operate for two weeks when used by 20 men. L.P. gas lighting will be used should the units be in an area where a power supply is not available. Four retractable jacks will support each unit when parked, and there will be a retractable step at the doorway.

Canberra service

The direct overnight service between Melbourne and Canberra was increased from four to six days a week, from February 23.

As previously, a special air-conditioned sitting and sleeping carriage is attached to *Spirit of Progress*, and transferred at Goulburn.

More luggage lockers

The Department will provide another 301 large automatic luggage lockers at Melbourne's two city stations. Installation will start in about two months.

At Spencer Street station, 253 lockers will be added, and at Flinders Street 48. Each locker will measure 16 in. wide x 22 in. high x 30 in. deep.

Provision of more lockers follows the continuing popularity of the 150 small and 50 large lockers that have been in use at the two stations since 1955.

Space for the lockers at Spencer Street will be found by altering the present cloak room on the ground floor concourse; at Flinders Street the lockers will be provided at the Swanston Street entrance near where the old cloak room was.

The charge for the small lockers will remain at 5 cents; and the larger ones 10 cents.

Both small and large lockers are available for use until 11 p.m. each day, when they are cleared and the contents transferred to the station cloak room.

Tons of Easter eggs

As *News Letter* went to press, the Department had just completed the raiting, in iced vans, of about 50 tons of Easter eggs and similar chocolate novelties from Melbourne to 32 country cities and towns.

The haul, comprising about 20,000 cartons of various sizes, began on February 24 and continued for a fortnight.

Shepparton received the biggest consignment—2,000 cartons. Morwell and Bendigo got 1,500 cartons each, and 1,000 cartons went to Geelong, Warragul and Mildura. Easter eggs were also railed for stores in about 27 other country towns.

As well as the traditional Easter eggs, consignments included such items as *Humpty Dumpty's* smiling rabbits and ducks, hens on nests, bunnies in caskets, and so on. Their size varied from that of a hen's egg to almost a football.

Assault on railwayman

In the Chelsea Court last month, a man was fined \$40 for unlawfully assaulting a railway employee. The magistrate added that character evidence given was the only thing that saved the man from gaol. Two other men were each fined \$20 for unlawful assault arising from the same incident.

Costly drink

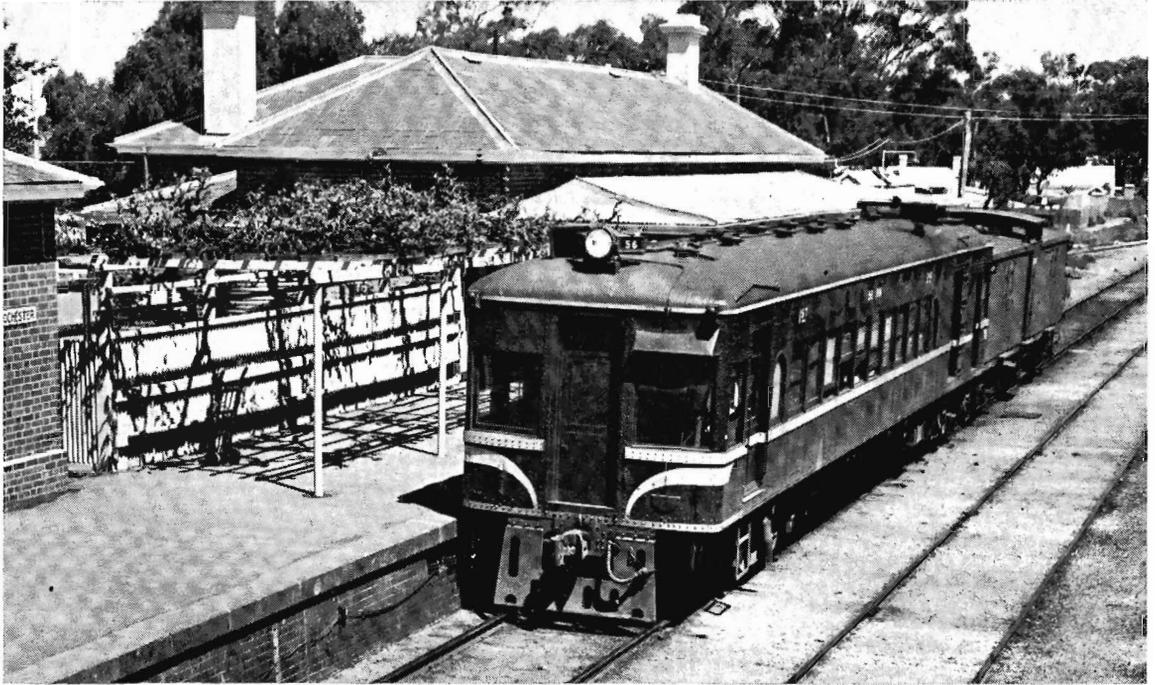
For drinking intoxicating liquor in a railway compartment, using indecent language on a railway platform, and giving a false name and address, a man was fined a total of \$40 at the Box Hill Court last month. Another man was fined \$30 in the same Court, for using offensive language in a train compartment.

FRONT COVER

shows the new GJX, steel, bulk-grain hopper wagon. The first four entered service in November last, and as *News Letter* went to press, 30 out of the 100 ordered had been received. With a capacity of 55 tons, they were a welcome addition to the wagons needed for the record wheat harvest. They can be filled in about 25 minutes and discharged in two minutes.

Nestling on the banks of the Campaspe River about 17 miles to the south of Echuca is

ROCHESTER



Denliquin bound rail motor at Rochester; at left is the station's famous grape vine.



Assistant Stationmaster N. Boyer checks a consignment.

The town is on the 55-mile Bendigo-Echuca line that was opened in 1864. Today, Rochester has a rail motor to and from Bendigo connecting with Melbourne trains, twice a day on Monday, Tuesday and Friday, and once on other days.

Last financial year, station revenue was \$100,761, inwards goods tonnage was 9,380, and outwards 16,321.



Before taking over at Rochester, Stationmaster J. F. Dunne had been at Ouyen, Dimboola, Leongatha, and Mitiamo.

There were 1,568 outwards passenger journeys.

Stationmaster J. F. Dunne, who has been at Rochester for 11 years, is assisted by a staff of three: A.S.M's E. N. Boyer and L. J. Sutton and Shed Assistant K. Commerford.

Much of the station revenue is received from the booming parcels traffic. The goods traffic is also a good dollar earner. And the service given to the public is certainly appreciated. In fact, while *News Letter* was at the station, a grateful client told Stationmaster Dunne how he had phoned an urgent order to a Melbourne firm at 4 p.m., and his consignment was at Rochester station at 8 a.m. the following day.

Two of the regular items railed from Rochester are casein and milk powder; last year 6,000 tons were sent.

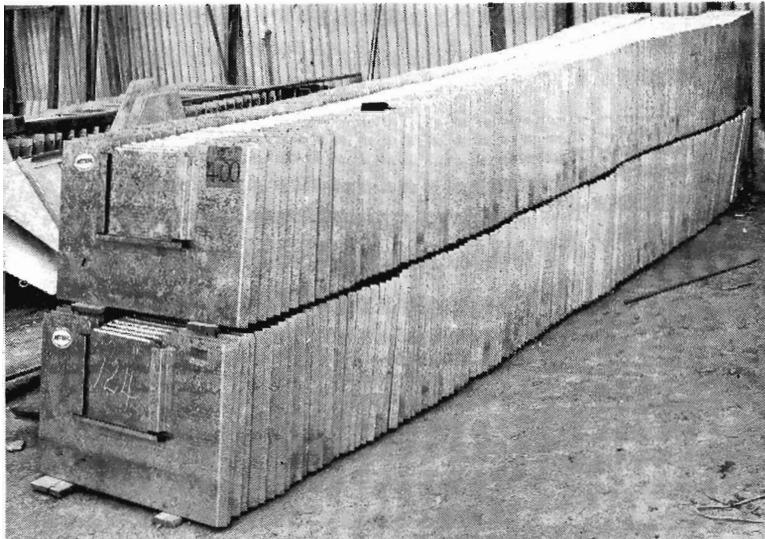
Items received by rail include medicines and blood for the local hospital, chlorine for the swimming pool, gelignite for the Shire quarry, and most of the supplies for the town's schools.

Ginger beer

Ginger and other ingredients for the brewing of the Hotel Rochester's



Shed Assistant K. Commerford prepares to unload galvanized steel sheets that will ultimately be made into irrigation channel stops.



The finished product—steel irrigation channel stops—awaiting railing from Rochester.

famous ginger beer are regularly railed from Melbourne. The brew is a closely guarded formula, the recipe having been handed down from the original licensees about the turn of the century. Brewed in 9-gallon barrels, and kept on tap, the ginger beer is widely famed.

An interesting consignment received at Rochester consists of specially galvanized steel sheets for the local engineering firm of L. F. Nelson. The steel is used to make irrigation channel stops under licence from the State Rivers and Water Supply Commission, and field bins for farmers. Many of the manufactured products are railed from the station.

Rochester's wheat storages comprise a concrete silo and two steel annex bins, with a total capacity of 215,000 bushels.

The horizontal bulk storage oat bin, built in 1961 can hold 75,000 bushels.

In charge of the Rochester track gang is Ganger A. E. Rosenow, who with two repairers, attends to about 8½ miles of track. The gang has won several prizes in recent years.

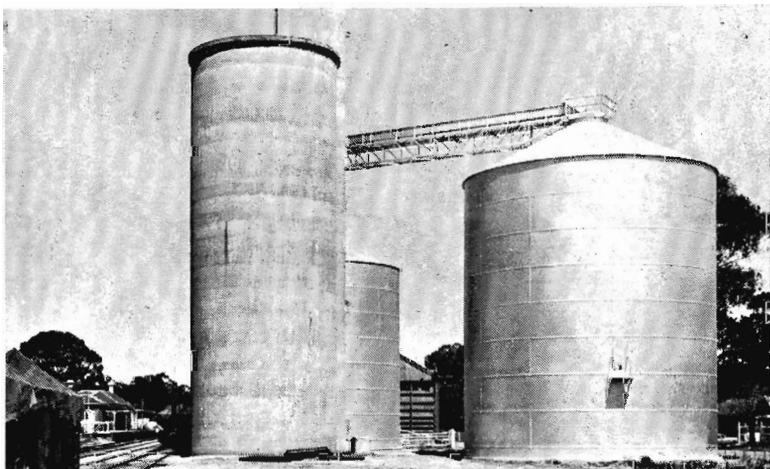
The grapevine growing on the station platform is 104 years old. It has been carefully tended by the station staff ever since it was planted by a guard named J. Skane. The vine has an Australia-wide fame, and, in season, an occasional passenger picks a bunch of grapes before continuing his journey. And it has even been the subject of Royal comment. When the Queen visited Rochester in 1954 she remarked that it was the only railway station on which she had seen a grapevine.



Ganger A. E. Rosenow (left) and Repairer A. J. Davis are packing the track near Rochester.



In the goods shed, Assistant Stationmaster L. J. Sutton examines the waybill for a spray tanker.



Grain storages at Rochester.

PNEUMATIC TUBES IN MELBOURNE YARD

A three-point pneumatic tube document-conveying system is one of the labour-saving features of the Melbourne Yard re-arrangement. Two parts of the 3-point system are already operating around the clock, thus obviating miles of walking each day. The system links the West Tower with the arrival yard office 2,000 ft. away.

Two different sizes of pneumatic tube have been used for maximum efficiency. A 4½ in. diameter system, with carriers having an internal diameter of 3 5/8 in. and a length of 11 in., operates between the arrival yard and the general office on the second floor of the West Tower. Freight documents are removed

from incoming wagons in the arrival yard and passed swiftly and safely through this system to the general office.

A second (2 1/5 in.) system, using carriers of 1½ in. internal diameter and a length of 8¾ in., links the general office with the control room and the room for the computer, on the floor above.

These two systems are both part of the first half of the modernization project. Following the construction of the hump itself, and the completion of the project in 1970, a third system, also 2 1/5 in., will link the general office in the West Tower with the Hump Yard Foreman's cabin about 1,000 ft. away. Documents, after

being processed in the general office and control room will pass through this system for action by the Hump Yard Foreman.

The pneumatic tube system has been powered with three turbine exhausters one for the 4½ in. system, one for the two 2 1/5 in. systems, and a third as a stand-by for all three links. Carriers in all three systems can move simultaneously at more than 15 m.p.h.

P.V.C. tubing is used; it is virtually damage-proof and eliminates condensation and freezing. Most associated fittings are aluminium alloys, plastic, or stainless steel. The tubes can thus be operated under most conditions, indoors, outdoors and underground.

THE WELCOME STRANGER

On February 9, Moliagul (near Dunolly) celebrated the centenary of the weighing of the world's largest nugget of gold—the Welcome Stranger—found at Moliagul on February 5, 1869, by John Deason and Richard Oates. When weighed at Dunolly, four days later, it tipped the scales at 2,520 oz. and was then worth \$19,110. Today, of course, it would bring much more. It is interesting to note that the celebrations were organized by a committee chaired by Mr. Richard Deason, a descendant of John Deason.

This celebration recalls the impetus given to railway expansion in Victoria by the discovery of gold.

The gold rush began about 1851, and traffic to the goldfields was

chaotic, as the unmade tracks became quagmires in wet weather.

Proposals for railways were formulated in 1852 by groups of investors whose extravagant optimism was typical of the times. Although money seemed to be available in unlimited supply, private railway ventures, generally, were not successful.

The collapse of the Melbourne, Mt. Alexander and Murray River Railway Company, which had been given a virtual monopoly to build railways anywhere in the Colony, was mainly responsible for the establishment of the Victorian Railways Department in 1856.

In 1857, a railway to Geelong was completed, and in 1858, because of the gold finds, contracts were let

for an extension of that line to Ballarat and for the building of a line from Melbourne to Bendigo (then Sandhurst).

In 1851, the population of Victoria was 77,345; by 1861 it was 450,322. During those 10 years, gold to the value of \$186 million was discovered.

As the railways were built, much of the gold was brought to Melbourne or Geelong by train for shipment overseas. Each consignment was closely guarded by police and escorts.

From Bendigo and Ballarat, gold was railed by the train load; from some fields it was brought on horseback to the train. Where the rail had yet to build, Cobb and Co. coaches carried the gold.



Train loads of gold about to leave Bendigo during the gold rush days, for Melbourne, from where it was shipped to London.

SOUTH DYNON LOCO PICNIC



Mrs. O'Connor and E. Anastasia (second from right) win the second heat of the wheelbarrow race.

Over 600 went to the Lilydale sports ground last month for the South Dynon Loco Employees' picnic, an event that has been held annually for 35 years. The venue has varied; in the earlier years, Diamond Creek, Eltham, Aspendale and Bacchus Marsh were favoured, but, for the past five years, Lilydale has been chosen.

Despite the decline in popularity of picnics, over the years, the Dynon Loco event still draws a good crowd. Committee members point out that many of the younger men become picnic conscious when they have a family, with children looking forward to that event as one of the big days of the year. Last month the kiddies' expectations were fully realized with tractor rides, trick bikes, ponies, swing boats, and, of course, a merry-go-round. And, by all accounts, adults also had a pretty good time.

The hon. president and hon. secretary of the picnic committee are Messrs. N. De Pomeroy and F. J. Moore.



Eddie Wilson (second from right) breasts the tape to win the final of the Loco Gift.

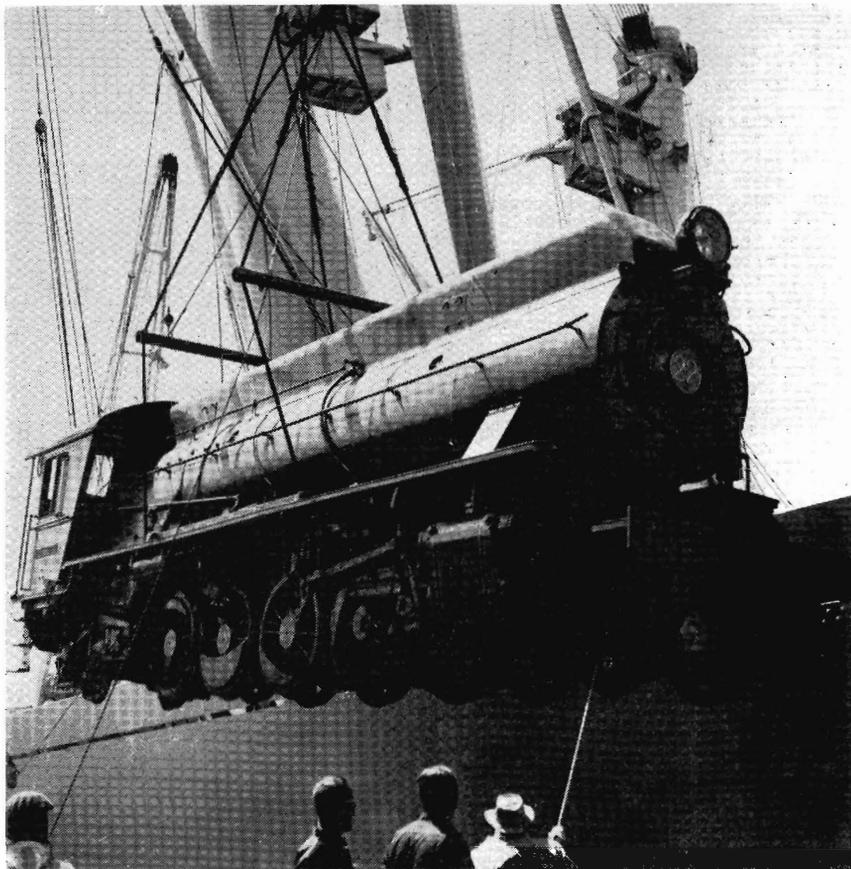


Mrs. Marengolcas (second in the married ladies race) receives her prize from Norm De Pomeroy.

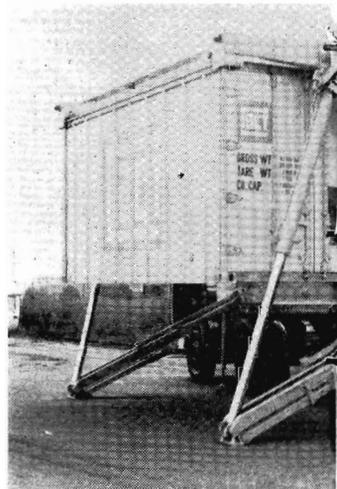


Winner of the Old Buffers race was Jim Allan (second from left).

AROUND THE SYSTEM



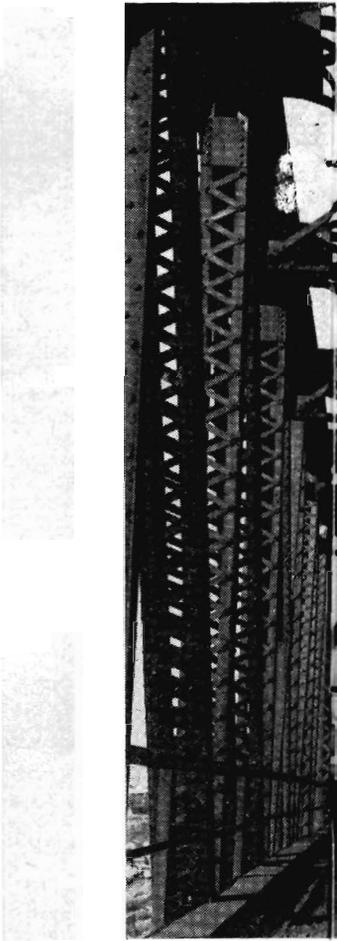
BIG LIFT: This 97½ ton, 3 ft. 6 in. gauge locomotive from the Silverton Tramway (Broken Hill to Cockburn) is here being unloaded at the Appleton Dock for display in the Puffing Billy Preservation Society's museum at Menzies Creek. The locomotive was donated to the Society by the Silverton Tramway management, towed on rail to Port Pirie, and from there was carried free by M.V. Australia Star. (Photograph: Alan Maggs)



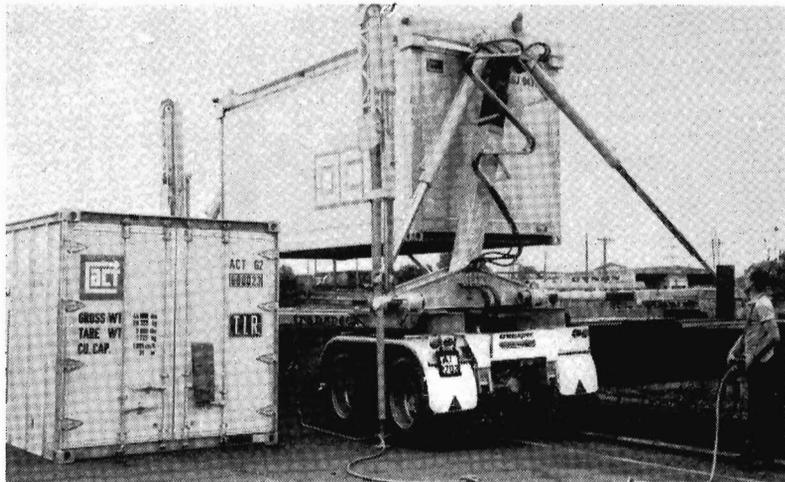
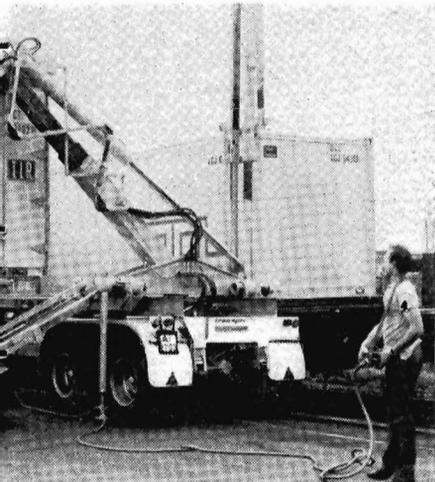
SWINGLIFT: A swinglift truck mounted on a rail



NITROJET is another example of the increasing number of products carried in specialized rail freight vehicles. This liquefied ammonia is railed from Port Kembla to Yarraville for Imperial Chemical Industries of Australia and New Zealand Ltd.



PATTERNING the



demonstration of the Goldhofer *Swinglift* was given last month at South Dynon. Using remote-controlled hydraulic power, and a road vehicle, this versatile piece of equipment can load and unload containers from rail wagon to road vehicle and vice versa. It can also stack containers and handle other goods.



In the open space of the sky, the steelwork of the Maribyrnong River bridge lends interest to this picture of X 31 hauling a down interstate goods.

HOMES FOR PIT PONIES

Homes have been found for the State Coal Mines pit ponies that were surplus after closing of the Mines in December (see February *News Letter*, page 26).

Most of the ponies went to the Victorian Animal Aid Trust and the Blue Cross Animal Society of Victoria, while a few were retired locally.

After it was announced that the ponies would be offered to people who would guarantee to properly care for them, the Mines Manager (Mr. J. Byrne) received about 700 applications—mostly from children—for the animals.

Although called ponies, they are virtually small draught horses, and are considered unsuitable for children.

The ponies are in the Dandenong Ranges, in refuges owned by the two animal care societies. Their



Some of the pit ponies at Wonthaggi.

shoes have been removed, and in a happy retirement after their 10 kind hands, the ponies will have to 15 years in the pits.

THE V.R.I. LIBRARIAN

TALKS ABOUT BOOKS

Australian fiction has been one of the most popular sections of the V.R.I. Library for some years now, and fortunately the supply of new titles continues at a good rate.

In the latest batch, one of the most popular will be *Andy*, by the well-known critic and broadcaster, Geoffrey Dutton. Andy enlisted in the R.A.A.F. during the last war, learning to fly in Tiger Moth aircraft (obviously much loved by the author), and his adventures and misadventures are recounted here with much gusto. Andy is a zestful character, moulded in the traditional Australian near-caricature fashion, earthy, resentful of authority, willing to have a go, one of the boys, etc. I think *Andy* will be popular.

Three new novels, set in the future (but not science fiction), are worthy of attention. Two are by established authors, and one a first novel. *Day of the Republic*, by Geoff Taylor, a popular established author, illustrates how the processes of democracy can be eroded and opposition repressed, and brings this home to us, in Australia, by setting his story here, in the waning hour of freedom, after Australia has been proclaimed a republic. *When the Enemy is Tired*, by Russell Braddon, another well-known author, is a frightening story of the "brainwashing" interrogation of an Australian Army officer, captured by the Chinese in Malaysia in the 1970's. The two main characters, the Australian and his Chinese interrogator,

are subtly drawn, and there is a twist in the tail of this story to startle you. The third story set in the future, this time by a new author, John Hay, is *The Invasion*. This is basically a character study; and to highlight his characters, the author projects them on to an Australian rural setting after a successful invasion by an Asian power. The main characters are an Australian grazier, a New Australian engineer, an aborigine, and the beautiful (of course!) female Asian colonel. This is an exciting story, but it is the characters who people it who provide the greatest interest. We shall hear more of this author in future.

The Passions of the Ring, by Arthur Acred, has been classified as an Australian novel, although the author is, I believe, an Englishman, now resident in Australia. This is a novel which will build a large following for the author, as it has already been bought by American and British publishers, and is to be made into a film. The Ring of the title is a circus ring, for this is a story of an Irish circus, and the author has drawn on his own experience of life with an Irish circus to lend authenticity to his book. An eventful and suspenseful novel, with very believable characters, this has been acclaimed the best circus story for years.

Another established Australian author, Jon Cleary, has written another exciting adventure story in

Season of Doubt. Set in the Middle East during the Arab-Israeli confrontation, there is no "message" in Cleary's work, just thrilling excitement.

Finally, I should like to advise of the addition to the library of several titles by Lucy Walker, who is a great favourite among the readers of romance fiction. Only one of these is a new title, but, for some reason, the others were not added to the library when first published, so we have taken the opportunity to correct this omission. The new title is *Home at Sundown*, the others being *A Man Called Masters*, *A Man from Outback*, *The Moonshiner*, and *Sweet and Far Away*.

FREIGHT RECORD

A new weekly freight record was established between Melbourne and Adelaide when 51,119 gross tons were hauled during the week ended February 8.

The previous weekly tonnage record was for the week ended December 14, 1968, when 50,674 tons were hauled.

INTERCAPITAL DAYLIGHT

During busy periods, a second buffet carriage will be attached to *Intercapital Daylight*.

The extra buffet carriage will be added when the train has eight or more sitting carriages. The usual consist of the train is six carriages.

RAIL USERS SAY...

Graziers' gratitude

We, the undersigned, are graziers from the Little River district whose properties were totally or partially burnt out in the recent bushfires, and we are writing to convey our sincere thanks for the wonderful, efficient job carried out by your Department in railing 3000 sheep from our blackened pastures to the Lismore and Derrinallum areas for agment.

And special thanks to the Live-stock Division in Melbourne who organized a special train, to the stationmaster at Little River for his co-operation, and to the train crew who gave us the utmost co-operation and courtesy while unloading at our destinations.

To us our job was made easier by your efficiency and co-operation, and once again we say "Thank you".

—Ian M. McNaughton, R. W. McNaughton, K. L. McNaughton, Thomas J. Richmond, writing to the Commissioners

Kerang

In a letter to the Secretary, Mr. J. W. Casey, Lake Boga, thanks the staff at Kerang station for help given in locating a boy who left the train at the wrong station.

Korong Vale fire

Messrs. W. G. Jurey, A. F. Nesbit, P. J. Fahey, W. R. McConachy, G. T. Hunter—all railwaymen—assisted the Korong Vale Rural Fire Brigade and other fire brigades during the fires on January 8.

This was a very serious fire, but did not get much publicity, possibly due to the more serious fires in other parts of the State.

These five men are just a few of the many railwaymen who assisted our Brigade, and we do not wish to forget the men who were here during the wheat season, and were able to give us so much assistance.

It is not possible to know just how much a man may contribute towards

a fire such as this, but may we mention the very valuable assistance given to us by Signalman W. Jurey with his knowledge of first aid.

Captain A. E. Tonkin, his officers, and members of this Fire Brigade would like to express their grateful thanks to all who assisted us during this fire, and to the railwaymen who take a keen interest in our Fire Brigade work throughout the year.

—Ken Proctor, secretary, Korong Vale Rural Fire Brigade, writing to the District Superintendent, Bendigo

The Overland

The conductor in Car No. 5 of The Overland from Adelaide to Melbourne on 1.11.68 (Mr. W. R. Farr—Ed.) was excellent; nothing was too difficult for him.

—B. Wilcynski, Box 587, G.P.O., Sydney, writing to the General Traffic Manager, S. A. Railways

ST. KILDA'S 1884 ROSTER

Porter	4.30 am	till 12.0 noon	— 1.0 pm till 5.30 pm.
Landman	5.0 am	till 12.0 noon	& 2.0 pm till 6.30 pm.
Signalman	First train	till 3.0 pm	} Alternate weeks
— " —	3.0 pm	till finish	
Guard	6.45 am	till 3.0 pm	} Early & Late Guards
— " —	7.0 am	till 3.20 pm.	
— " —	3.0 pm	till finish	
— " —	3.20 pm	till finish	} Alternate weeks.

As far as practicable the above mentioned men take alternate shifts weekly — S. No. 1 is always on duty when H. Porter is off —

Sundays.

S. No. 4 Head Porter — also remainder of Staff alternate Sundays off duty

Signature: Mr. Chief of Head W. J. ...

Signature: ...

Pasted on the back of a cupboard door at St. Kilda station is a staff roster dated 1884, a section of which is reproduced above. It appears that in those days the station had a staff of 18; today it has 12.

LINEs FROM OTHER LINEs

Underground news

Many of the world's transport authorities are turning to underground railways as the most effective way to move large numbers of commuters in traffic-choked cities.

As with Melbourne's proposed underground railway, the missing ingredient in most of the cities appears to be finance.

In Chicago, the present elevated line surrounding the Loop district will be replaced with an underground. A belt line will link suburban railway stations ringing the Loop, and connect with other underground lines. If \$US600 million can be found, the new system will operate by 1975.

While test drilling continues in Copenhagen for an underground, the Government proposes that 75 per cent. of building costs be met by the State, and the balance by boroughs near the capital.

Plans for building an underground in Helsinki are active again after having been dormant for over a year.

The Soviet Ambassador to Brazil is reported to have offered the State of Guanabara financial and technical aid in building an underground railway in Rio de Janeiro.

After a 2-year study of the public transport needs of Hong Kong, an underground railway costing \$490 million is proposed. The system will take 16 years to complete, and carry a third of all public transport passengers.

In Germany, undergrounds are being built—or expanded—in Berlin, Hamburg, Munich, Frankfurt, Cologne, Stuttgart, Hanover, Düsseldorf, Essen and Bonn.

The Toronto underground in Canada is being extended.

Calcutta and Venice are also considering underground railways.

In Sydney, tunnelling for the Eastern Suburbs underground railway passing under Kings Cross has started

Melbourne's proposed underground railway is a vital stage of development of an overall fixed rail transit system designed to carry

passengers to a number of city stations.

Without the underground, the potential of the suburban railway network, no matter how much is spent on improving it, will be restricted because of the limitations of the two existing terminals on the outskirts of the city proper.

The Victorian Railways are ready to go—the missing ingredient is money.

The heaviest trains

While the average train in U.S.A. hauls 1,700 tons, many haul 10,000, and some 20,000 tons. However, the Pennsylvania has run experimental trains that each carried over 30,000 tons of ore in 300 wagons. The trains, three miles long and powered by eight locomotives, are believed to be the heaviest ever operated.

—(*New Ways to Run Railroads*)

* * *

An expert is someone called in at the last minute to share the blame.

Detectors in U.S.A.

By helping to prevent small troubles from turning into big ones, detectors installed on United States railways are proving they could save much more money if used more widely. Hot box detector installations had increased from 73 in 1962 to 221 in 1965. Other detector systems in use reveal such potentially

dangerous conditions as loose wheels, dragging equipment, floods, fires, bridges out of alignment, falling rocks, and earthquakes.

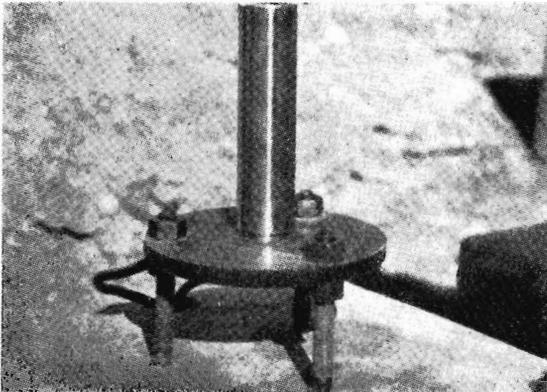
Some of the reasons for the increasing use of detectors are :

- today, less staff patrolling the tracks to look for hazardous situations;

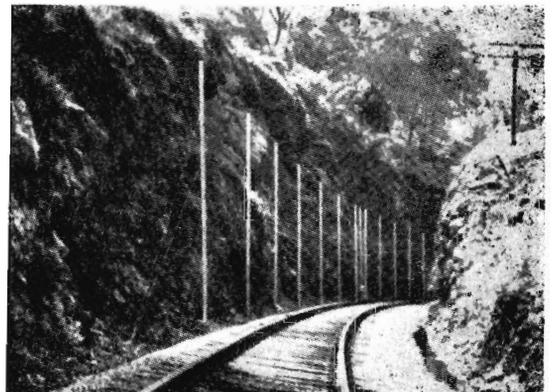
- high speed of freight trains makes it more difficult for the human observer to notice defects;

- high cost of trains and their freight makes derailments very expensive.

—(*Railway Age*)



Earthquakes : When the earth shakes, a pendulum swings, and earthquake information is relayed by this detector on an Americal railway. One U.S.A. railway has a device that will detect 12 different ranges of earthquake intensity.



Falling rocks : This fencing along a railway track in U.S.A. emits a signal when struck by falling rocks or earth slides; this also sets the stop signal, and alerts the train controller to the presence of a dangerous situation. (*Railway Age*)

AMONG OURSELVES . . .

150 years of printing

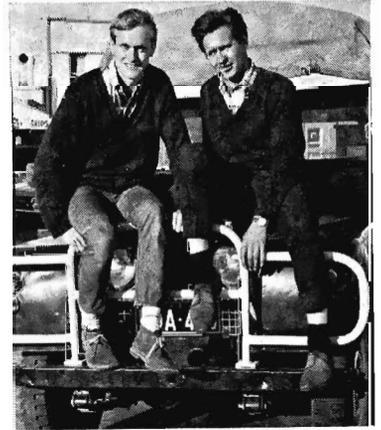


When Mr. J. P. Garland (left) retired last month as Sub-foreman at the Department's Printing Works, he had completed just a week over 50 years service. Two others in the Printing Works, Messrs. W. J. S. Penington (centre) and J. P. Timmons (right) have also completed 50 years service. All three joined the Printing Works in 1919 when it was in the basement of Head Office, and, together, their careers add up to over 150 years of railway printing.

Dust found its way into everything, and coupled with the potholes and gullies made driving very hard work.

The only occasion they were lost was in the McDonnell Ranges, about 150 miles west of Alice Springs. Their track petered out in a dried-up river bed just as darkness fell. Camping nearby for the night, they found with the aid of a compass, next morning, that the river bed was actually part of the track.

"The journey increased our knowledge of Australia, and I am certainly glad to have done it . . . but I wouldn't care to repeat the trip", says Mr. Evans.



Messrs. Barry Evans (left) and Lawrence Stendt on the Land Rover. (Photograph: "Geelong Advertiser")

9,000 miles

Two railwaymen at Newport Workshops—Messrs. Barry Evans and Lawrence Stendt—recently returned from a 9,000-mile trip through central, northern, and Western Australia. Both are fitters and turners, a trade that helped them to cope with the mechanical troubles they experienced because of the age of their vehicle, a 1950 Land Rover, and the very rough country.

Places they visited on their trip included Coober Pedy, Alice Springs,

Ayers Rock, Darwin, Broome, Mt. Tom Price, Perth, Kalgoorlie, and over the Nullabor Plain back to Melbourne.

A powerful radio kept them in touch with civilization, but a watch was hardly needed, says Mr. Stendt.

"The setting sun told you when it was time for bed, and your stomach when it was time for a meal. And the sun was a very effective alarm clock; it made it too hot to sleep after about 8 a.m.," he adds.

Stayed 26 years



Parcels Assistant Mrs. Dorothy Doherty started in the Outwards Parcels Office at Flinders Street during the Second World War, and remained there for 26 years. Mrs. Doherty is holding a box containing a 400-day clock that was one of the gifts presented to her by the staff on her retirement. "It's been a good job at the O.P.O.," said Mrs. Doherty, "and now, my chickens, dogs, and a big garden will keep me busy".

Rice arriving

April sees Echuca's stationmaster, Mr. L. Carey, busy with the rail handling of the rice harvest. His station is the focal point for that operation.

The grain is grown over the border in N.S.W., and railed to the mill in Echuca. From there it is sent, after milling, in bags to Melbourne. Only about one-sixth of the rice grown in the Murray Valley is consumed in Australia. The balance is exported to the United Kingdom, New Zealand, New Guinea, the Pacific Islands, and South-east Asia.

Harvesting begins early in April, and within six weeks is completed. Last season, the Railways hauled 50,000 tons of rice.

Mr. Carey, who controls a staff of 35, has been at Echuca since 1964.



Mr. Carey

Three of the old lifts have now been replaced. The new lifts have 50 per cent more speed (300 ft. a minute), and answer calls—in the same direction—in the order in which they are made.

Four days after the last of the old lifts was "retired", Liftman M. D. Keily who had been driving it for several years, also retired.



Mr. Keily drives the last of the old H.O. lifts.

Wins Transport prize

For the second year in succession, the prize for the top first-year student in the Transport Adminis-

tration course at R.M.I.T. went to a clerk in the Metropolitan Superintendent's Division at Flinders St. The 1968 prize was won by Mr. M. J. Michell. That for the previous year went to Mr. G. O. Evans. The prize, \$50, is donated by the Bus Proprietors' Association of Victoria.



Mr. Michell (left) receives his prize from Mr. T. H. Furlonger, President of the Bus Proprietors' Association of Victoria.

Flinders Street first aid

... I became ill during my train journey to work, and, on arriving at Princes Bridge, Mrs. Ward, a First Aid Attendant at Flinders Street Station, took me into her care. While resting in the First Aid room, Mrs. Ward contacted my employers and notified them of my sickness. My doctor was also telephoned and advised of particulars concerning my condition . . .

—M. G. Jamieson, Herbert Street, Mount Waverley, writing to the Chief Traffic Manager

Statutory declarations

The stationmaster or person acting as stationmaster of any Victorian railway station is empowered to take and receive "statutory, solemn, or voluntary declarations".

Before witnessing declarations, all stationmasters should familiarize themselves with their obligations contained in General Order 118.

Retired together

The last of the four old lifts in the Head Office was recently removed, and the installation of the new automatic lift to replace it, is now in progress.

When the building was completed, in 1893, hydraulic lifts were installed. Following the wiring of the building for electricity, in 1921, the hydraulic lifts were replaced three years later, by four electric ones.

RETIREMENTS

ROLLING STOCK BRANCH

Townsend, J. F. B., Ballarat Nth., 24/1
Buhagiar, C., Jolimont, 7/1
Di Paola, G., Jolimont, 15/1
Grigula, P., Geelong, 24/1
McGillivray, A. G., Head Office, 9/2
Carter, J. McG., Nth. Melb., 28/2
Climas, W. H., Bendigo North, 28/2
Evans, L. A., Cohuna, 3/4
Turner, A. T., Bendigo Loco, 7/4
Tsoupanargias, G., Shelter Shed, 8/4
Gilbert, E. C. W., Newport, 11/4
Sherriff, L. C., Nth. Melb., 17/4
Falcone, G., Nth. Melb., 19/4
Ingram, C. W., Jolimont, 21/4
Townsend, J. N., Bendigo Nth., 25/4
Quayle, A., Ballarat Nth., 25/4
Mead, H. O., Bendigo Nth., 26/4
Western, G. R., Newport, 28/4

TRAFFIC BRANCH

Vosti, A. A., Caulfield, 1/4
Cummins, R. H., Mentone, 2/4
Capp, G. W., Carrum, 9/4
Thies, H. J., Bendigo, 12/4
Davey, M. J., Ballarat, 14/4
Greenwood, T. W., Head Office, 14/4
Hayes, M. J., Flinders Street, 22/4
Thompson, C. R., Geelong, 6/3
McIntosh, M. D. T., Caulfield, 10/3
Meehan, P. H., Heidelberg, 30/1
Hare, J. C., Yarraville, 22/1

WAY AND WORKS BRANCH

Aylett, E. S., Tatura, 20/4
Brown, C. M., Bendigo, 13/1
Buckley, E. G., Moulamein, 15/1
Cameron, J., Hamilton, 8/1
Crosher, J. M., Overhead, 31/1
Doyle, T. J., Ironworks Division, 13/4
Evans, G. McK., Mangalore, 6/4
Finlay, D., Glenalbyn, 15/4
French, A. G. R., Benalla, 22/4
Jones, C. J., Flinders Street, 7/4
Marocik, I., Caulfield, 11/4
Murray, A. A., Bendigo, 3/4
Pearson, L. J. T., T & T Engineer, 21/4

STORES BRANCH

Grech, E., Reclamation Depot, 30/12
Norris, A. H., Electrical Depot, 31/1

ELECTRICAL ENGINEERING

McKenzie, H. S., Lighting & Power Division, 5/4
O'Brien, M. I. (Miss), Head Office, 25/4

SECRETARY'S BRANCH

Brennan, W. E., Head Office, 13/5
O'Hara, C., Flinders Street, 3/6

Keen on gardening



Mrs. A. E. Chandler, caretaker at Koyuga (on the Toolamba-Echuca line) is interested in gardening. Her station was awarded first prize in its section of the 1967 station decoration competitions. Before coming to Koyuga, Mrs. Chandler was at Rosedale.

Award for apprentice

Third-year Apprentice Carpenter and Joiner Johann A. L. Slee, of Spotswood Workshops, has received his second Duke of Edinburgh Award—the silver medallion. These awards are granted for public service (such as ambulance or rescue work), physical fitness, and completion of a project.

Easy way to save

Undoubtedly the least painful way of saving money is to join the National Savings Group. Under this scheme, an amount requested by you will be deducted from your pay by the Department, each fortnight, and paid into your Savings Bank account at regular intervals. Further details can be obtained from your staff office.

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH

Saliba, C., Newport, 8/1
Bowden, J., Wodonga, 7/2
Rosevear, L. N., Wodonga, 7/2
Ramondetta, V., Newport, 9/1
Whelan, J. J., Nth. Melb., 14/1
Singh, S. J., T. L. Depot, 2/1

TRAFFIC BRANCH

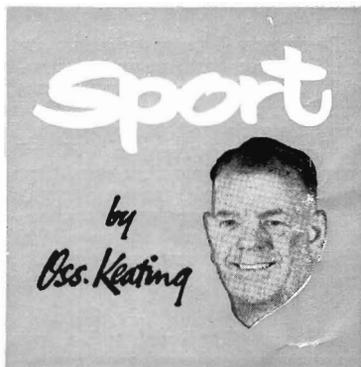
Hunter, F. W., Flinders Street, 15/1
McLeod, W. D., Warrnambool, 1/2
Pepi, S., Melbourne Goods, 1/2
Simms, F. I. A., Parkdale, 1/2
Surmas, J., Spencer Street, 4/2

WAY AND WORKS BRANCH

Cardilini, P., Korumburra, 5/2
Ferguson, J. P., Head Office, 9/2
Murray, M., Laurens Street, 14/1

ELECTRICAL ENGINEERING BRANCH

Longino, M. W. H., Lighting &
Power Division, 26/1



Wimmera bowling tournament

The 1969 tournament was played recently at Ararat, where teams from Maryborough, Donald, Dimboola, Horsham, Hamilton, Portland, Branxholme and Ararat competed for the V.R.I. trophy. Teams from Head Office and Central Council also participated by the invitation of the organizing committee. The only flaw in an excellent day's bowling was—as mentioned by Maurie Walsh (genial President of the Ararat V.R.I. Bowling Club)—the weather which was overcast and showery. However, Maurie assured all competitors that this factor would be taken care of at future tournaments.

In the semi-finals Cluff (Ararat) beat Maskiell (Maryborough) and Walsh (Ararat) beat Veit (Ararat). In the all-Ararat final, Walsh defeated Cluff after a great tumble.

Jack Williamson (V.R.I. Councillor) presented the trophy to the winning Skip. Mr. Pianta (President Ararat V.R.I.) presented the individual trophies, and Charlie Morris (Chairman of the Staff Board) thanked the Ararat V.R.I. Associates for providing such an excellent buffet tea. At a meeting of delegates during the afternoon, it was decided to hold the 1970 tournament at Dimboola.

Snooker

The annual V.R.I. Snooker Championship was again won by Les Williams who beat Bert Moulday in three straight frames in the final. Although a straight frame victory, it was a pretty keen and even contest, and the three games were won on the black ball. At the annual meeting, Kevin Dunne was re-elected unopposed as President for the next 12 months, and Les Williams took over as Secretary from Vic Alembakis who has transferred to another club. Captains for the new season are Les Williams and Bill McMahon, and together with Bert Moulday, they form the selection committee. Probably their first job will be to select a team good enough to beat

our old friends from South Australia who are due to visit us in the near future.

Cricket

The final of the metropolitan competition for the Commissioners' Cup, was played at Royal Park last month. Finalists were Loco and, for the first time ever, Codon. Winning the toss, Keith Hopkinson, (Codon captain) sent Loco in to bat, and immediately had the engine drivers in trouble. After losing 3 for 27 Loco settled down and a good hand by R. Chapman enabled them to put up a reasonable score. At 12.45 p.m. they were all out for 141, and at lunch Codon were 0/13 and looked to have a great chance to take out their first premiership. However, the long break proved too much, and in the first few overs after lunch, Loco managed to crash through, removed four of the oppositions best bats, and so tied the game up. Final scores were Loco 141, (G. Allen 26, R. Chapman 25, J. Mitchell 21, Webster, 4/19, Stanley, 2/17) beat Codon 84 (E. Stanley 27, K. Hopkinson 16, M. Franke 5/25, G. Allen 3/25, P. Gibbs 2/12).

At the presentation ceremony after the game, Mr. E. P. Rogan, (Deputy Chairman) presented the Commissioners' Cup to the Loco Captain, Ray Chapman, and congratulated him on a fine team performance. Our General President, Mr. M. L. G. McKenzie, who with Mr. Rogan and Mr. L. Bennett, (Chairman of the V.R.I. Sports Committee), had witnessed some of the afternoon's play, welcomed the Deputy Chairman and thanked him for making the presentation.



Codon's captain, K. Hopkinson, bats against Loco in the final of the Commissioners' Cup at Royal Park.

Table tennis

At the annual meeting of the V.R.I. Table Tennis Association, Mr. Jack Crouch was re-elected President, and Mr. Graham Smith Secretary. This means, of course, that these two gentlemen will play a major part

in the planning of the inter-system carnival to be played in Melbourne between July and September 1970.

Arrangements for the playing of the internal winter competition have been completed, and it should be under way within the next few weeks.

It is also of interest to report that the four teams competing in the Northern Table Tennis Association's pennant competition are in the top four of their respective sections, and we are fairly confident that at least two section championships should come our way.

Life Membership was conferred on Dave Catchpool, immediate Past President of the Association (our membership clerk at the V.R.I.), for services rendered. Dave, I feel, has thoroughly deserved this honour, as during his chairmanship, our Association has risen from among the also-rans in the inter-system carnivals to be runners-up in the last two, and certainly a power to be reckoned with in interstate competition.

Golf

The Sunday outings are still proving popular, and the latest, to Clifton Springs Golf Club, was no exception. Forty-seven members and 12 ladies turned up to do battle in windy conditions. The 18-hole stroke handicap event was won by D. Neville on a count back from A. Collins. Both these players returned a net 68.

The elements proved a bit too much for most of our players, as is evident when A. Jack, who returned an 80 off the stick, won the trophy for the best gross score. Mrs. M. Blue won the ladies trophy. A progress report from the carnival committee indicates that arrangements are proceeding satisfactorily.

A.P.I. v V.R.I. bowls

Every year I begin to think that the four inter-institute trophies will at last finish up in our trophy case, and after a bit of a pep talk from Alan Cowling and Harry Watts I felt—well, this is the year. We will start off with bowls, then clean them up at footie, hold the golf trophy, regain our lost prestige at cricket—and, what happens! In spite of all the pre-match assurances, we finish up getting tossed in the first contest of 1969.

Footscray City Bowling Club was the venue of this years bowls match—a delightful setting if ever there was one. The weather, although on the hot side, nevertheless was perfect for bowling, and the green was a credit to both the Footscray Club and the greenkeeper.

There were two matches, 18 ends in the morning and 18 ends in the afternoon.

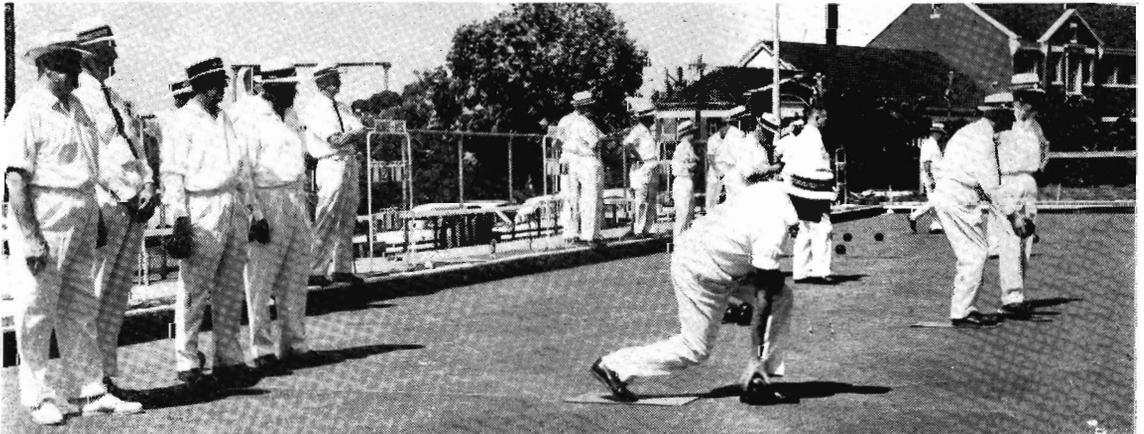
There's no doubt about it, we are terrific beginners. At the six end mark we led by an overall 10 points, increased our lead to 19 points by the time 12 ends had been played, and then promptly folded up—allowing our opponents to score 38 points to our 14 points in the last six ends—to go to lunch 5 points down. Scores at this stage were A.P.I. 85, V.R.I. 80.

In the afternoon, the game assumed the same pattern as did the morning match. After six ends we had wiped off our five points deficit, and had taken a 6-point lead; at 12 ends this was increased to 14 and victory looked in the bag, but again the A.P.I. boys staged a grandstand finish to reduce our lead to such an extent that we could only win the afternoon section by one point, and thus were beaten by 4 points on the day. The final points were A.P.I. 85, 80, total 165 to V.R.I. 80, 81, total 161.

Seriously, in spite of our defeat, it was a great game, and some excellent bowling was produced by both sides. For our team, Harry Watts and his boys turned in two good efforts winning 19-14 and 24-13, Perce Smith won 21-10 and 16-14 and Alan Cowling who in the morning won 18-16, was leading in the afternoon when he had to retire due to illness after a few ends had been played.

For the posties, Dicker's great win over Laycock, 27-9 in the morning, was instrumental in giving them the lead, and Tiller carried on the good work in the afternoon by winning 21-12.

An excellent luncheon was enjoyed by all, and we were welcomed by Mr. W. Matter (Superintendent of Post Offices Branch) who represented the Director of Posts and Telegraphs, and by Mr. J. Carroll, (President, Victorian Division, A.P.I.). Mr. E. P. Rogan (Deputy Chairman) and Mr. M. L. G. McKenzie (General President V.R.I.) suitably responded. Other guests included Messrs. R. McKay (Manager, Victorian Division, A.P.I.), A. Crawford (Chairman, Sports Committee, A.P.I.), R. I. Baggott (Acting General Secretary, V.R.I.) L. Bennett (Chairman, Sports Committee, V.R.I.), J. Griffiths (President, Footscray City Bowling Club) and A. Neilson (Secretary, City Bowling Club). The day concluded when our Captain, Alan Cowling, handed the trophy over for, as he said, safe keeping for 12 months, to Val Wagner, Captain, A.P.I. Our thanks to our hosts, A.P.I., for a most enjoyable day of bowling, and congratulations to them, and in particular my old friend Bob Martindale (Sports Secretary, A.P.I.) for an excellent job of organizing.



Play during the V.R.I. v Australian Postal Institute bowls at Footscray.

VICTORIAN RAILWAYS

NEWS LETTER

APRIL



1969



V.R. CHAIRMAN COMMENTS ON

OUR APPRENTICESHIPS

Victorian Apprenticeship Week (May 26-30) will provide an opportunity for railway men and women to introduce to the community the many advantages of the railway apprenticeship. This can be done mainly by encouraging apprentice-minded lads, their parents and friends to visit the displays ably prepared by railway staff.

During May 27 to 29, these displays may be visited at Newport Workshops, Melbourne's Lower Town Hall, the Victorian Government Tourist Bureau in Collins Street, and at Bendigo, Ballarat, and Geelong.

Railwaymen play an important part in apprentice recruiting — of the recent intake, about one in five

apprentices has his father in the railway service, and another one in five has a relative or friend in the service.

The Victorian Railways Institute thrives on the support of youthful railwaymen in its sporting, social, educational, recreational, and library activities. The Ballarat team in the recent Country Tennis Tournament included six apprentices.

The vast railway service offers many careers, the basis of which can be an apprenticeship, and I recall with pleasure my apprenticeship days.

Railway staff able to visit these displays will be warmly welcomed. The details are being given wide publicity.

Misled—but not by the Railways

Complaints made by rail users to the Department or published in the Press, are inquired into most searchingly. A typical case resulted from the publication in the *Donald Times* of December 24 of a statement that a 14-year-old boy travelling by the morning train to Donald from the Box Hill Salvation Army Boys Home "was informed at a suburban station that he could go by train as far as Ballarat, but would thereafter have to travel by bus"!

The lad accordingly left the train at Ballarat, went looking for a bus, and by the time his search had unsuccessfully concluded, the rail-car from Ballarat to Donald had left.

The Ballarat station staff then did all they could to help the lad; they contacted the Salvation Army authorities at Ballarat with the result that the boy was taken by car to his destination.

Efforts to obtain from the *Donald Times* the name of the suburban station that was alleged to have given the wrong information were not successful.

A railway officer then visited the Box Hill Salvation Army Boys Home and ascertained that the information was not given by any member of the railway staff and, as an Army official

said, "the railway staff were in no way responsible".

Salvation Army officers at the Home added that they "receive the best of service from the staff at Box Hill Railway Station, and the station was in no way concerned in the incident".

Transport Information Centre

A new Transport Information Centre will open at the Victorian Government Tourist Bureau, Collins Street, on April 21, to provide co-ordinated information on bus, tram, and train passenger services anywhere in the State.

Inquiries may be made personally, or by telephone—63 0141 for metropolitan services and 63 0202 for country services.

For some months a committee within the Ministry of Transport has been working on the setting up of the Centre. Represented on the Committee have been the Bus Proprietors Association, Victorian Railways, Melbourne and Metropolitan Tramways Board, and the Transport Regulation Board.

It is planned to issue easily readable time-tables with strip maps showing linking services and other useful information, as well as maps showing all transport routes.

MotoRail service increased

The overnight *MotoRail* service between Melbourne and Mildura, that takes passengers' cars on the same train, was doubled from March 24, and now runs each night except Saturdays, in each direction. Un-accompanied cars will also be carried.

At the same time, twice as many cars can be handled each night by using an upper deck of the *MotoRail* wagon; special ramps have been provided at Melbourne and Mildura to allow cars to be driven on to this deck.

The demand for the *MotoRail* service, introduced last August, has necessitated the increase. Broken Hill residents are coming to Melbourne by driving to Mildura and loading their cars on the overnight train.

It is expected that a number of business firms will receive or send unaccompanied cars; the transport cost is \$15 compared with \$10 for a car accompanied by a passenger who also pays the usual train fare.

Prize stock for Sydney Show

Nineteen Berkshire pigs — the biggest weighed more than 500 lb.— for Sydney's Royal Easter Show were among the prize stock sent from Dynon last month in a special express train to Sydney.

A horse and 115 head of prize cattle from Victoria and South Australia were also on the train.

The stock travelled in 17 spacious bogie vans that were tarpaulin-covered, and had deep straw bedding for the animals' comfort.

Two sleeping carriages and a sitting carriage were attached to the train for grooms and stock attendants.

Record harvest

Last season's wheat crop from the area served by the Department was the biggest ever harvested, amounting in round figures, to 93 million bushels.

FRONT COVER

One of the monitor water guns on the V.R. fire train's new attack wagon is operated by Repairer Spiro Janis (see story on page 52).

Overseas opinion on undergrounds

The ever-increasing constriction of surface space in modern cities highlights the need for, and importance of, underground rapid transit systems. Their high cost of construction is justified by the economic results, says Mr. A. H. Grainger, President, International Union of Public Transport, Brussels (Belgium).

In a special report in *International Railway Progress*, he points out that it is difficult today to think of a capital city which is not either planning or constructing an underground railway, or adding to an existing system.

The size of, and shortage of housing accommodation in many modern cities make it necessary for people to travel some distance to and from their places of work. There are also those who, preferring to live in the outer suburbs or in the country, accept that they must spend some time travelling. There are few cities which are not directly or indirectly concerned with mass urban transport.

The motor car is part of our life today and will be tomorrow, but, in cities of a million inhabitants or more, the trend is for an underground to become the mainspring in the transport system and a major factor of the whole urban and regional development policy.

This does not mean that it is the solution to all problems of mass transport. Buses, trams and, perhaps, some new systems which the ingenuity of man may devise, will have their place as feeders.

While the need for undergrounds has been accepted by planners and transport people, undergrounds are now also accepted by national and local authorities as one of the means of escape from the slow strangulation of economic and social life by the motor car.

The cost of underground development is high, but the alternative—the provision of more road facilities—is greater, even if physical space were available.

Much, if not all, money for building roads comes from national or local authorities, and the fact that a double-track underground railway is the equivalent of a 10-lane highway is not the least of the factors which appeal to authorities.

The underground plays a dominant part in the provision of the economic and social needs of a community but, because of political and public reluctance for individuals to pay the cost of their ride, that cost must be provided out of public funds to the extent that it is not paid by the passenger.

There is evidence that, in America, part of the automobile-generated tax revenues may be increasingly used for underground purposes.

Construction of dozens of new undergrounds or extensions of existing systems is world wide. Work is being done in locations as far apart as Munich, Kharkov, Toronto, and Osaka.

Ready for containers

A 1½-mile railway line built to serve the new container wharves was opened on March 14, when a special train of empty rail wagons arrived at Swanson Dock and loaded empty containers. They were railed to country areas, mainly Shepparton, Mildura, Mooroopna, and Kyabram, but some went to Adelaide and Berri in South Australia. The first container ship was due in Melbourne just as *News Letter* went to press.

The railway is an extension from Appleton Dock and will serve at least two sidings which are now being built.

At this stage, the line, with marshalling sidings, is broad gauge. However, a standard gauge track, can be added in the future, if needed.

Overseas Containers (Aust.) Ltd. and Trans Ocean Containers Pty. Ltd. will operate from the Seatainers Ltd. terminal at Swanson Dock, and a community container area will be at Appleton Dock 'B' berth.

A siding will be provided in the wharf area, near Footscray Road, for a consortium of wool brokers who intend to compress wool into smaller bales than usual and export it in containers.

Since overseas containerization was decided upon, two large container consortia have planned to use rail for large tonnages of export canned and dried fruits, milk products, etc., from country areas.

The Department expects to handle about 300,000 tons of container traffic during the first year of operation.

And no amusement tax

"Mr. Ron Yeates, of Highbury Road, East Burwood, reports that he boarded the 8.07 a.m. train from Syndal.

"There was a man in his mid twenties, dressed in pyjamas and dressing gown on the seat opposite.

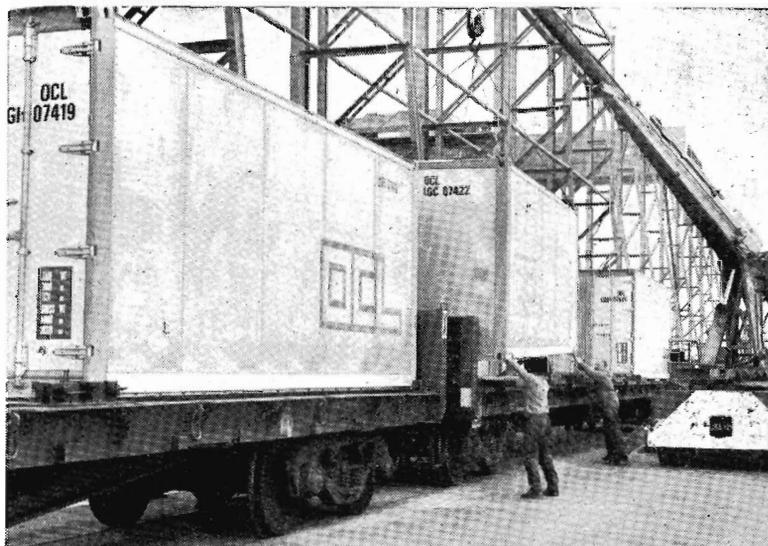
"A butler then appeared with shaving gear which the gentleman used between Syndal and Mt. Waverley. At Mt. Waverley, another butler appeared with a basin for his wash up. At Jordanville, there were two girls in white coats with after-shave powder.

"At Holmesglen, another butler dressed him. At further stations he received his manicure, his cereal, his hot toast and marmalade. At Burnley, he got his toothbrush and paste, and at Richmond a girl passed in his brief case and umbrella. At Flinders Street station, out he went followed by his army of servants in double file. The organization was superb according to Mr. Yeates.

"It's not often you get entertainment like that on the Syndal line."—Keith Dunstan "The Sun" 18.3.69 (A students' prank?—Ed.)

View from the train

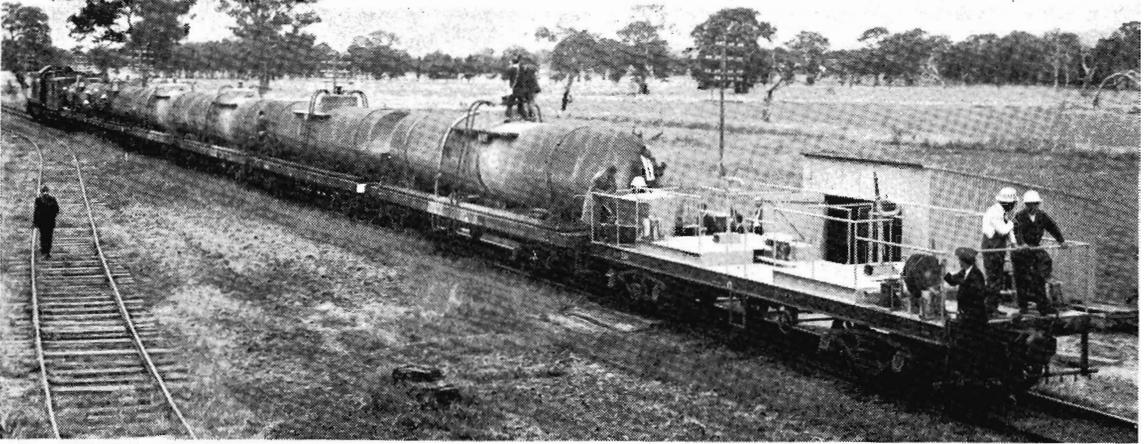
Three Nunawading firms with properties extending to the railway line, have been asked by the Nunawading Council to clean up their back yards. This is an extension of the council's campaign to keep Nunawading Beautiful.



The first train to arrive at Swanson Dock is being loaded with empty containers for railing to country areas. After filling with dried and canned fruits, the containers were returned to Melbourne for loading into the first container ship.

FIRE ATTACK WAGON

The Department's recently completed attack wagon was in action for the first time, at Lyndhurst on February 27. It is the only vehicle of its type in Australia, and possibly, in the world.



Fire train at Lyndhurst, with attack wagon. In action, the attack wagon is at the front.

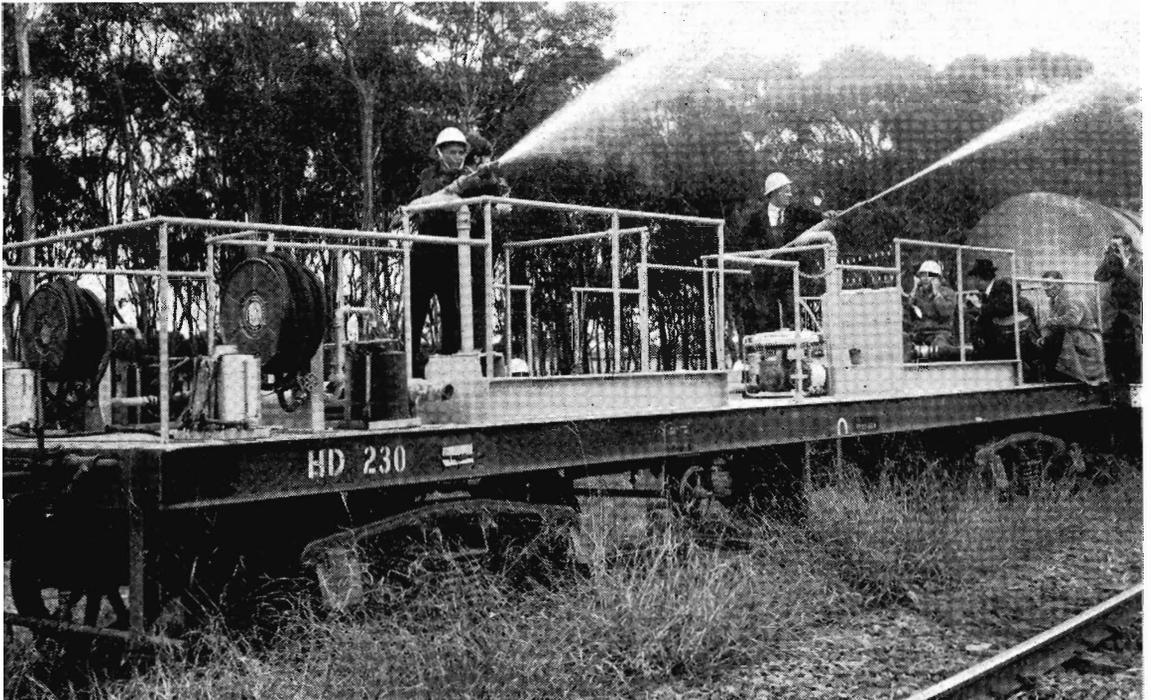
It took part in a demonstration of one of the Department's fire emergency trains.

Senior Government officers, fire fighting and police chiefs, and media

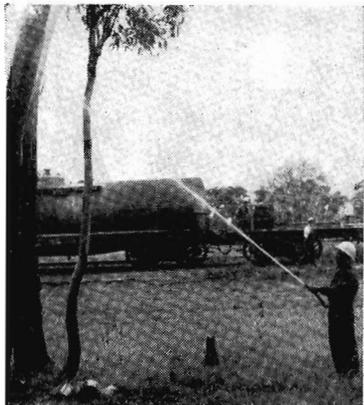
men were among those who attended the demonstration. Other keenly interested onlookers included children and teachers from Lyndhurst State School.

The demonstration (with smoke generators adding realism) showed how

• water from the fire train is



Attack wagon with both monitor water guns in action.



Hose run out from train is used for fires at side of track or under bridges, etc.

used to extinguish burning railway sleepers—vitaly important to save the line so that the train can run;

- hoses are run from the fire train to deal with fires under bridges and culverts;
- water can be transferred from the 7,000 gal. tankers on the fire train to fire units of the Country Fire Authority;
- fires in line-side trees and on telephone poles are fought some distance from the train and from several points at the one time.

The demonstration concluded with a display given by the attack wagon, with its two monitor guns mounted on revolving turrets. The wagon was built by the Railways after the Upwey fires last year showed the need for such a wagon.

Together, the water guns use about 200 gal. of water a minute, and can shoot it in any direction up to 100 ft. from the railway line. For attacking oil fires they can be fitted with foam making equipment. They are fed through 2½ in. diameter hose, and operate at 100 lb. per sq. in. pressure.

What may be described as the heart of the attack wagon is a pump driven by a petrol engine. It has sufficient capacity to cope simultaneously with all the operations of which this versatile wagon is capable—extinguishing sleeper fires, filling C.F.A. tankers, etc.

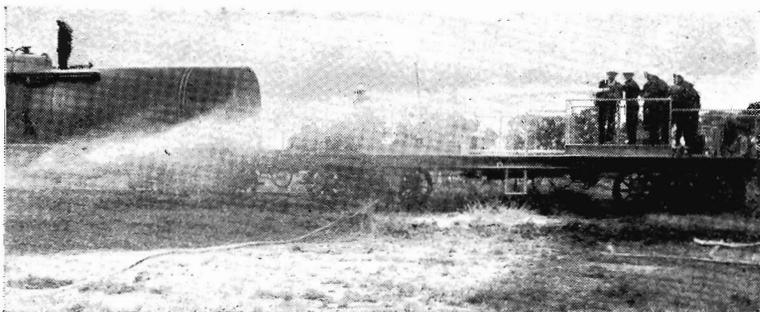
Official visitors at the demonstration commended the Department on the new equipment.

The Chief Commissioner of Police (Mr. N. Wilby) said the wagon was a very effective weapon for fire fighting.

“The set up has great value, with the obvious advantage of versatility”, commented Mr. C. Howe (Senior Assistant Chief Officer of the Country Fire Authority).



Water boom at front of wagon extinguishes fires in sleepers.



Water gun uses foam for oil fires.



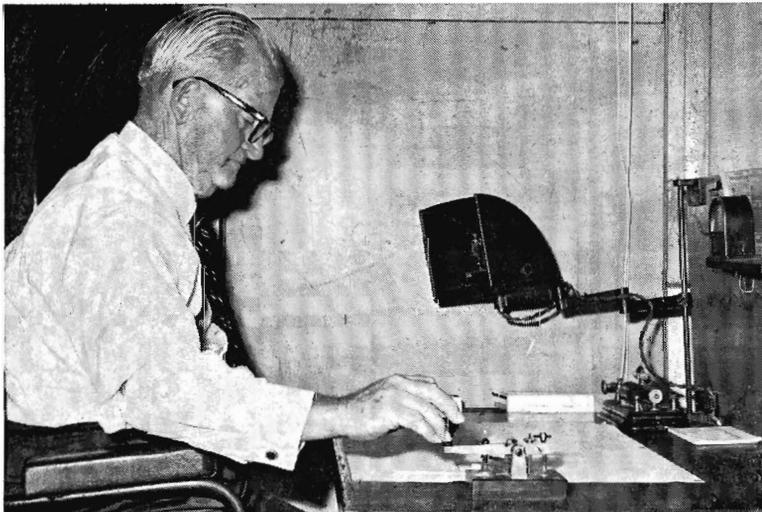
Hoses are run from fire train to fill C.F.A. tankers.

“The Commissioners are to be complimented on a very fine contribution to fire safety in Victoria”

said the Chairman of the Australian Fire Protection Association (Mr. H. W. Marryatt).

END OF MORSE

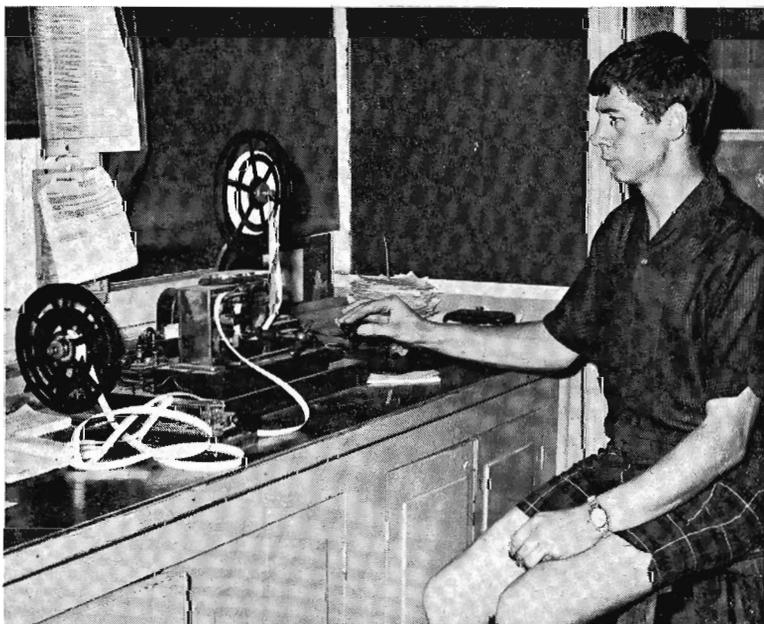
After chattering countless millions of words, the last Morse instruments on the V.R. tick-tacked their final messages on March 3, and ended over a century of Morse on the Railways.



In Melbourne, Mr. O'Connor acknowledges receipt of the last Morse message from Mildura.



Mr. James sends the last Morse to Mildura.



At Mildura, Mr. G. J. McCormick is shown with the machine on which the last message was sent; all the staff took part in sending it.

The last Morse link on the system was between Mildura and Melbourne. So, at 9.57 a.m. the following message to Mr. T. A. James, Chief Traffic Manager, was received in Melbourne from Mildura.

This is the last message transmitted by Morse from Mildura and brings to an end more than a century of this type of communication.—Station-master and staff.

Mr. James then personally sent the following reply to the station-master, Mildura.

This final message ends the era of Morse telegraphy that has extended over more than 100 years of Railways in Victoria. My good wishes go to you and your staff on this historic day.

Silence then descended on the Morse line, and, a few minutes later, the first teleprinter message was received from Mildura.

The operator who received the last message from Mildura was Mr. E. S. O'Connor, aged 68, who began with the Department as a Morse telegraphist in 1917. After receiving the last message, Mr. O'Connor was presented with an inscribed Morse key by Mr. James.

Commenting on the change caused by the advent of the electronic era, Mr. O'Connor recalled that when

he was transferred to the Spencer Street telegraph office in 1921, there were 50 telegraphists employed there, and the office was located at the Spencer Street station. For a first-class certificate, operators had to achieve a speed of 30 words a minute. Each man developed his own individual style of sending, which was as personal as his signature, and enabled his immediate identification by colleagues.

"We have been sending about 30 Morse messages a day, and getting the same number back", said Mr. C. Hynes, Mildura's stationmaster.

"I suppose the old machine has knocked out some dramatic messages over the years, but in my time they have only been the usual ones about train loads and bookings," he added.

The Mildura machine has been given to the Rio Vista museum at the Mildura Arts Centre.

History

When the electric telegraph (as it was then known) was invented in 1837, railways overseas were the first to recognize its potential as a communications medium.

Telegraphy was first used between Euston and Camden Town, on the London-Birmingham railway in England. The equipment was based on a 5-needle system; the combinations set up on the needles pointed to letters of the alphabet.

The telegraph system caught the attention of the public when, in 1845, a murderer was apprehended by police who had received a telegraph message sent ahead of the train.

Samuel Morse soon improved on the invention and devised a system that used only one needle instead of five. Letters were represented by a series of dots and dashes in what became known as Morse code. Morse also devised a method of making permanent records of telegraph messages on a moving tape.

Railways throughout the world quickly availed themselves of Morse's invention, and the telegraph expanded rapidly along thousands of miles of railway tracks. For example, when the east and west coasts of America were linked by rail, the accompanying telegraph also linked the coasts; this was of great social significance to the growing community.

The railways of America soon found that their telegraph systems were very profitable when used for public telegrams. Western Union developed into a company through the demand for railway telegraph services. Even today, Canadian National Railways operate a profitable public telegraph service.



In Melbourne, the first teleprinter message from Mildura is received.

Morse on the V.R.

In Victoria, telegraph lines were built from the earliest days of railways.

During 1866, the electric telegraph was connected to Melbourne, Sandridge, Windsor, and Brighton Beach, and it was announced that other stations "would be supplied with Morse's patent ink-recording instruments as soon as possible".

The telegraph section developed so quickly in the early days of the Victorian Railways that a Telegraph Branch was established in 1878. As country railway lines were built, so the number of locations with Morse facilities increased. At its peak, it is estimated that about half of the Victorian railway stations had a Morse instrument.

Teleprinters

Soon after World War II the first teleprinters were installed at main country centres and linked with the telecommunications centre in Melbourne.

In the intervening years there has been a continuous programme of modernization in communications until only one country centre—Mildura—had Morse equipment.

Mildura has been particularly difficult for teleprinter installation as there are no trunk telephone circuits that can use modern carrier techniques beyond Maryborough. The teleprinter must therefore operate on the old Morse lines, and, to achieve this, special repeaters have had to be installed.

YOUNG GIRLS' JOURNEY

Recently my wife sent our two daughters from Wangaratta to Sydney by *Spirit of Progress*.

Jill and Patricia were travelling alone, on student's concession, their first time away from home, at eight and 10 years of age on a long journey.

Through the services of the Glenrowan Stationmaster and the staff at Wangaratta, arrangements were made for the forward and return journey. Name tags were attached to the girls, including emergency phone numbers, and who would meet them . . .

The girls were received with confidence by station staff. The hostess and conductor put us at ease, "We will look after them". This they did (including crew change over

the border) beyond the call of normal service. How pleased we were. The girls will remember the hostess and conductor for ever, they still talk about it.

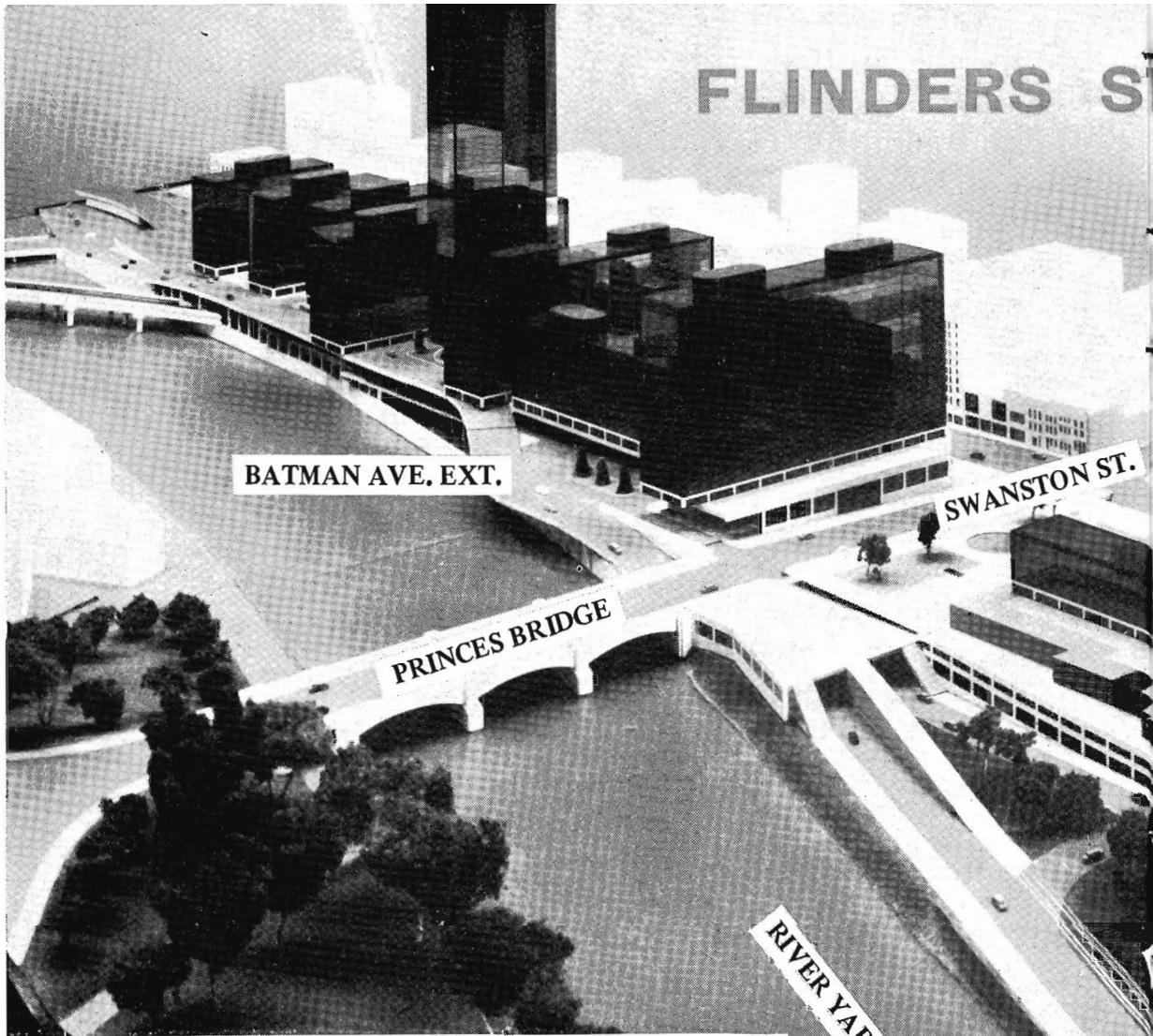
Two little girls will be travelling on *The Overland* next Christmas to see their grandmother, all made possible by your school concessions and the reliable service extended by your staff.

—Jim and Nancy Tanner, Esmond Street, Wangaratta, writing to the Commissioners

RESTAURANT

Just a word of appreciation for the constant courtesy and attention of your staff at the Princes Gate Restaurant.

—Charles Rumbold, Wharton Street, Surrey Hills, writing to the Commissioners



Last month, the go-ahead for planning a multi-million dollar re-development of Flinders Street station and the Jolimont yards was announced by the Premier, Sir Henry Bolte, and the development company, Meldon Properties Pty. Ltd.

The model gives an idea of how the Flinders Street area could appear after the completion of the plan, that is flexible and would change as feasibility is investigated.

The initial submission includes multi-storey buildings over the station platforms and tracks, pedestrian concourses, shops and bus and taxi terminals on two decks over the rail tracks.

The planners say the work would

take up to 20 years to complete, and the total cost is "anybody's guess". However, the amount of \$30 million has been suggested as the cost of the initial stage.

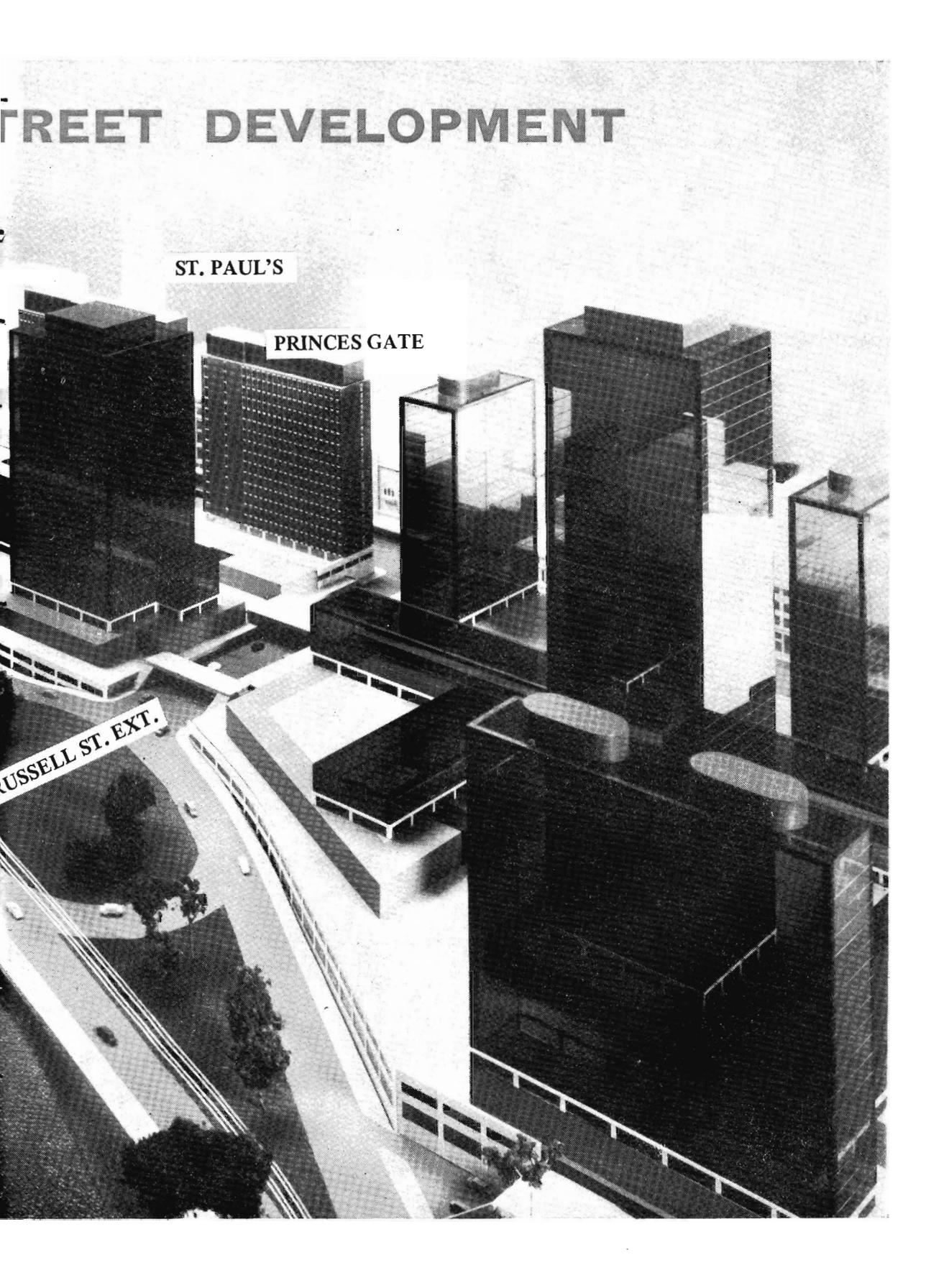
An option to submit a proposal for the development of the area west of Swanston Street is held by H.K.J. Pty. Ltd. which is now a wholly owned subsidiary of Meldon Properties Pty. Ltd. (see *News Letter*, June 1963).

STREET DEVELOPMENT

ST. PAUL'S

PRINCES GATE

RUSSELL ST. EXT.

An architectural rendering of a city street development project. The scene is viewed from an elevated perspective. In the foreground, a road labeled 'RUSSELL ST. EXT.' curves through the scene. Several modern, multi-story buildings are shown in various stages of construction or completion. One building is labeled 'ST. PAUL'S' and another 'PRINCES GATE'. The buildings feature a mix of dark facades and glass panels. The overall style is a high-contrast, black and white architectural illustration.

Probably the majority of domestic users are familiar with only one variety of electric supply—240 volts, 50-cycle alternating current. But a big organization such as the Railways needs many kinds. In fact, the V.R. uses at least

31 VARIETIES

In the Railways there are many types of electrical apparatus, ranging from high power rectifier units supplying traction loads, to small signal relays operating on a few volts.

The unusual features of all these items are the supply voltages and frequencies on which they operate.

Excluding the voltages that are generated in various units of equipment and do not appear outside the equipment, the following list, while it is not exhaustive, gives some idea of the range and typical applications.

VOLTAGE	TYPICAL APPLICATIONS
---------	----------------------

50-cycle alternating current

22,000	Supply to traction and industrial sub-stations,
6,600	Supply to industrial sub-stations,
2,200	Metropolitan and Gippsland signal supply,
440	Signal supply for north-eastern and western lines,
415	Industrial power supply,
240	Industrial and office lighting and power,
32	Safety industrial lighting,
12	Degraves Street subway indicators,

25-cycle alternating current

20,000	Supply to traction and industrial sub-stations,
6,600	Geelong line signal supply,
2,200	Metropolitan signal supply,
440	Industrial power supply,

VOLTAGE	TYPICAL APPLICATIONS
---------	----------------------

254	Yard and certain other lighting, single phase power,
110	Manual traction sub-station lighting,
55	Geelong line supply to signals,
6	Signal equipment,

Direct current

1,500	Traction supply to trains,
750	Traction auxiliaries in Tait and swing-door carriages,
120	Emergency lighting in standard gauge trains,
115	L class loco and Harris Train auxiliaries,
110	Manual traction sub-station emergency lighting,
90	F class loco auxiliaries,
74	Diesel-electric loco auxiliaries,
48	Traction sub-station supervisory control, air-conditioned carriage supply, telephone exchanges,
32	W class loco auxiliaries, diesel-electric rail motor lighting,
24	Brake van and car lighting, signal and point motors, telephone exchanges,
18	Signal switch operation,
16	Boom gates,
12	Signal equipment, road motor vehicles,
6	Signal equipment,
3	Telephones, hand torches.

OFFICERS' CONFERENCE

Twenty-eight top railway executives and engineers met in Melbourne on March 24, at the 1969 Officers' Conference of the Australian and New Zealand Railways.

In officially opening the Conference, the Chairman of Commissioners, Mr. G. F. Brown, said that the 12 years since the last conference had been held in Victoria had been one of the most challenging, and in some ways particularly frustrating periods, in the history of Australian railway systems.

Competition from interstate road hauliers was just getting into its stride in 1957 as a result of the 1954 Hughes and Vale case which had ruled that, under section 92 of the Constitution, interstate trade could not be restricted by State legislation that had earlier curtailed the operation of road hauliers crossing State borders.

"I don't think that in 1957 any of us, and particularly those of us unfortunate enough to conduct a lot of business within 100 miles of a State border, realized a tenth of the potential ramifications of that decision."

The decision forced the railway systems to increase their efficiency and to work more closely together to counter the intensified competition it brought about.

"However, in spite of the huge increase in cost levels that has taken place in the meantime, our average return per net ton mile now is less than it was in 1954."

"Very soon a new era in overseas trade will begin with the arrival of the first all-container ship from England, and once again we will all be on our mettle to prove that the railways are capable of meeting the further challenge of this innovation", said Mr. Brown.

After the official opening, the delegates got down to a week of commercial, technical, and administrative discussions and exchange of information under the chairmanship of the V.R. Chief Traffic Manager, Mr. T. A. James.

Officers' Conferences have been held regularly since 1898, at intervals of about 18 months. All railway systems act in turn as hosts.

The conference produced recommendations for consideration by the Australian and New Zealand Railways Commissioners, at their own conference to be held later this year, in Victoria. (See April 1968 *News Letter*, page 51).

TURKEYS TAKE THE TRAIN

From March until early in June, turkeys are being continually sent by passenger trains from Spencer Street and Koo-wee-rup to country farmers throughout the State.

The turkeys are from Tooradin Park turkey farm, reputed to be the biggest in Victoria.

Consignments of day-old turkeys in quantities of from 100 to 500 are dispatched two or three times a week from Spencer Street. Older and full-grown birds, are usually railed from Koo-wee-rup.

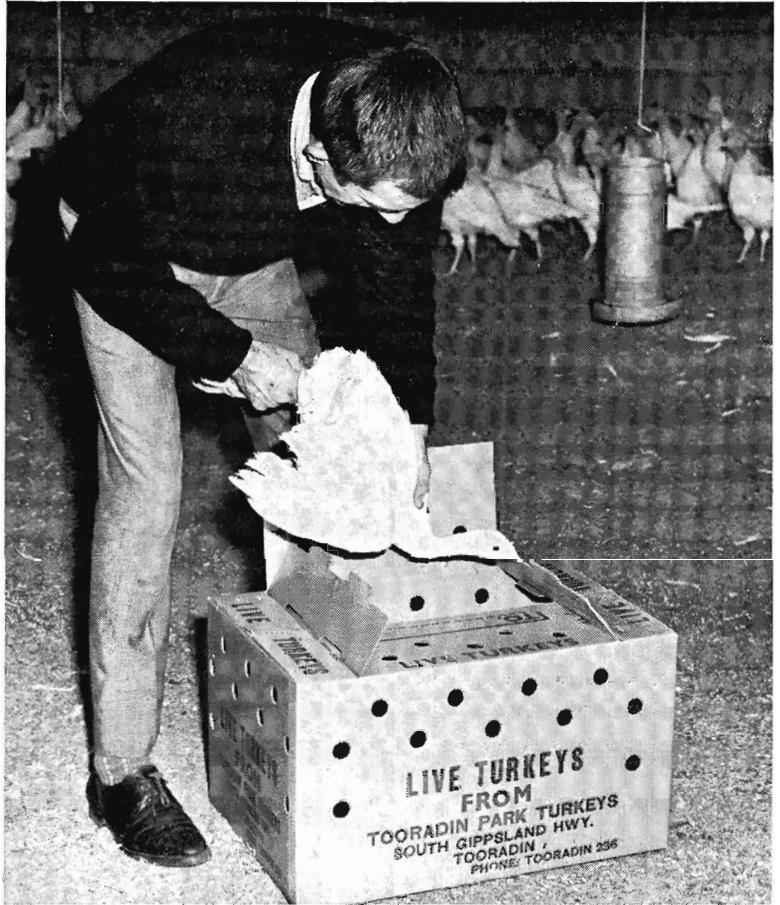
The main areas that receive turkeys by train are Bendigo, Horsham, Ballarat, Geelong, Mildura, Woome-lang, and as far away as Wagga Wagga (N.S.W.).

Turkeys are dispatched almost all the year round, but as autumn is the time when many farmers restock, it is consequently the busiest period.

The owner of the farm, Mr. K. Davies, expects that about 10,000 of his turkeys will be sold this year.

One of Mr. Davies's sheds has 3,000 ft. of floor space. Controlled lighting dictates the laying period of the turkey hens; they are brought into lay at eight months, and continue for about 100 days. However, only a small number of hens reach that age because of the demand for small table birds. An 8lb. or 10lb. turkey reaches that weight when 16 to 18 weeks old.

"For day-old turkeys speedy transport is essential" Mr. Davies says. "The Railways provide the fastest of all transport systems I have used, and, besides, their service is tops."



At Tooradin Park turkey farm, turkeys are being packed for dispatch by rail.

NEW CONTAINER WAGON

The first two of 70 new 63 ft. long standard flat wagons, suitable for railing containers and general traffic and designated FQX, went into service on March 21. Others are following at the rate of about three every two weeks.

By agreement between the Australian and New Zealand Railways, the Victorian Railways are the design authority for the new wagon, and specifications were submitted to all Australian railways. This has saved duplication of effort by design staff of all systems.

To keep the tare weight of the wagons as low as possible, they have been built with low-alloy high-tensile

steel. This permits a maximum pay load to be carried.

The wagons can carry any of the following combinations of containers:

- three 20-ft. dry containers;
- one 40-ft. and one 20-ft. dry containers;
- two 20-ft. refrigerated containers with clip-on refrigeration units and motor alternator sets.

The containers are secured to the wagon by means of special anchor brackets, with horizontal keys similar to those that have been used in Victoria for some time.

In addition, provision has been made for lashing rings, stanchions, bolsters, and securing chains that are required when the wagon is used for general traffic.

For intra-state traffic, two 20-ft. containers can be placed one at each end of the wagon with doors facing inward. The space in the centre of the wagon can then be used as a loading platform for fork-lift handling into containers. (See picture in *News Letter*, June 1967, p. 86.)

This avoids the provision of expensive gantry cranes or other equipment in country areas to lift containers from and to the rail wagons.

RAIL USERS SAY...

Bendigo

A note of thanks for the kindness and courtesy shown to me by you and the staff when I found myself, inadvertently, at Bendigo instead of Maryborough . . .

—(Mrs.) G. Cooper, *Glen Eira Road, Caulfield, writing to the stationmaster, Bendigo*

Canoe trip

We, of 1st Nunawading Senior Scouts, would like to thank your staff for the very good service they gave us when we went from Melbourne to Yarrowonga, and then from Echuca to Melbourne, and also for transporting our three canoes. We would especially like to thank the staff at Nunawading, Box Hill, Yarrowonga and Echuca for the consideration given to us . . .

If any other scouts go canoeing, we will certainly recommend the train . . .

—John Newman, W. Sutherland, Andrew S. Coban, Don Bosche, Haydn Gerceldene, Peter Willison, *writing to the Commissioners*

Lost Property

Recently my daughter left her spectacles in the train on her way back to school in Ararat. Some days later I phoned the Lost Property Office, who sent them to Mont Albert station.

They were in good condition, well wrapped, and I would like to thank all concerned who contributed to their return . . .

—(Mrs.) T. M. Donegan, *Barloa Road, Mont Albert, writing to the Secretary*

Frankston train

I would like to thank, through you, the guard on the Frankston train leaving Flinders Street about 3 p.m. on 18.1.69. I asked him if he would keep a watch on a 14-year old girl and 12 year-old boy travelling to Frankston alone.

I placed them in the carriage next to him, and he was very kind. He kept a check on them, and they arrived safely, thanks to him.

It is people like him that make this world a good place to live in . . .

—(Mrs.) M. Ledley, *Wheeler Street, Pascoe Vale South, writing to the Secretary*

Tooronga

Writing to the Commissioners, Mr. R. Nugent, Director of Harris and Nugent Pty. Ltd., Malvern, expresses appreciation for the "courtesy and excellent service rendered to our firm" by the stationmaster and staff at Tooronga station.

Stawell

. . . As wheat growers, we appreciate the time and effort you spent in assisting us in many ways.

A record number of rail trucks has been sent this year, and it was due to your efforts that they moved so smoothly . . . Alan Tepper, *Secretary, Stawell Wheatgrowers Committee, writing to the stationmaster, Stawell*

Ringwood

On January 24, I was going to Boronia for the week-end, and my next door neighbour offered me a lift by car to Ringwood.

When I went to the booking window to buy my ticket I found I had omitted to put my purse in my handbag. I had no money at all, and knew no one in Ringwood.

The stationmaster very kindly issued me a single ticket to Boronia—telling me to give the cost to the S.M. at Boronia on the return journey

I greatly appreciated this kindness, and would say that men such as he are invaluable on your staff, and help to give the railways the prestige they deserve.

—Nellie Putland, *Rathmines Street, Fairfield, writing to the Secretary*

Children's camps

I welcome this opportunity of conveying to you my personal thanks, together with that of our Camps and Holidays Committee, for the co-operation and assistance extended to us by the stationmasters and their staffs of Flinders and Spencer Street stations during the recent Christmas holiday period. . . .

This year, approximately 620 children had the benefit of a fortnight's holiday in the country, while 230 country children attended Melbourne Legacy's annual camps. . .

A movement of this size would have little chance of success if it was not for the support we receive, in many and varied ways, from your organization. I would, therefore, be extremely grateful if you would extend our sincere thanks to the stationmasters, conductors, guards, station assistants, the Man-in-Grey, and also to those men at Head Office who were so helpful in the planning stages. All of them went that "extra half-mile" that means so much in an operation of this complexity at your busiest time of the year . . .

—J. E. Cooper, *President, Melbourne Legacy, writing to the Deputy Chairman*

THE PORTLAND RAILWAY

The Portland Railway, by Keith W. Turton, a handsome publication of the Australian Railway Historical Society, is a history of the railways serving Portland, including the lines to Coleraine, Casterton, and Mt. Gambier. Actually it covers the period from the arrival of Edward Henty, in 1834, to the recent developments in 1968.

It is pointed out that Portland saw what was probably the first attempt by a Victorian Government to enter the railway or tramway business. This was in 1856 when the Government authorised the building of a 3 ft. 10 in. gauge tramway between Portland and Mount Eckersley (Heywood). (The first Government-owned railway, between Melbourne and Williamstown, was not opened until three years later, in 1859). The first turf of the tramway was turned at a ceremony at Portland on December 3, 1857. Although many difficulties were encountered, it was recorded that the first eight miles of the tramway were completed by October 1858. However, the scheme was abandoned, and eventually the material for the line was sold at auction in 1865.

Mr. Turton is one of Victoria's most active railway historians. He is editor of *Divisional Diary*, the monthly news magazine of the Victorian Division of the Australian Railway Historical Society, and is a regular contributor to the Society's national journal. He spent five years researching and writing *The Portland Railway* which started out to be a short article about railways in the Hamilton district.

In hard covers, with 180 pages on art paper, the book is well illustrated with carefully selected photographs from the best collections, and includes a good number of maps and diagrams. Although it lacks an index, there is a useful appendix of railway technical terms. It is a book that will be appreciated not only by railfans but also by that ever increasing number of readers who want to know more about the history of their own country.

A limited number of copies is available at \$5.25 each, from the Victorian Division Book Department, A.R.H.S., G.P.O. Box 5177AA, Melbourne, Victoria 3001.

Helio from Hotham

The coming retirement of Mr. W. E. (Bill) Brennan of the Secretary's Branch, recalls that he is one of the few railwaymen left in the Department who were actively concerned with the chalet at Mt. Hotham—*Hotham Heights*—when it was operated by the Railways.

Mr. Brennan worked in the Victorian Government Tourist Bureau for 25 years, and was, for a period, on loan to the Refreshment Services Branch. While with the latter, he was Acting Manager of *Hotham Heights* chalet for several months in 1946. Access to the chalet, in those days, was by pack-horse to the snow-line and then by ski. A horse-drawn sled took skiers' packs over the snow; the horse was equipped with snow shoes and made light of the work.



Mr. Brennan

The snow falls were so heavy during 1946 that the alpine road had to be opened for Christmas traffic by blasting the icy snow drifts with gelignite. In this work, Mr. Brennan and his staff were, he says, ably assisted by the late Mr. Bill Spargo who owned the rich Red Robin mine at Mt. Loch.

The manager of *Hotham Heights* also acted as the postmaster, and recorded meteorological data. During the summer months, Mr. Spargo lived at his mine, a couple of miles from the Hotham chalet. To advise him whether any mail was on hand, Bill Brennan would flash a large mirror from the balcony of the chalet at precisely 9 a.m. each Monday, weather permitting. Bill Spargo would climb to a vantage point and watch for the improvised heliograph. Some time later he would appear over the horizon hiking to the chalet for his mail and provisions.

Other positions held by Mr. Brennan during his 49 years service included Escorting Officer at the V.G.T.B., and Entertainments Officer at Mt. Buffalo Chalet.

Apprentice re-union

It is planned to hold a re-union of 1927 apprentices at the Princes Gate Restaurant on May 2 at 6.30 p.m. Further information can be obtained from Messrs. J. Whitelaw (tel. 5 3078) or P. Hunt (391 3876).

Signal engineers meet

The annual general meeting of the Institution of Railway Signal Engineers (Aust.) was held in Melbourne last month, and attended by representatives of Queensland, N.S.W., and Victorian Railways, and private industry. After an inspection of the Signal and communication equipment in the new Melbourne Yard, the visitors were welcomed by Mr. L. A.

Reynolds, Commissioner, at a buffet lunch. The engineers also inspected signal construction facilities at Caulfield. At the general meeting, papers on power signalling construction practices were presented by Messrs. B. W. Graham (Victoria), E. I. Goodman (N.S.W.), and K. W. Thomas (Queensland). In addition a paper on the same topic, received from Mr. D. C. Curtis (W.A.), was circulated.

12 years at South Yarra



"I've never regretted joining the railways . . . among other advantages, the steady work enabled me to buy my own home," said Mrs. Myrtle Mathers who recently retired after 25 years in the Department, 12 of them as a station assistant at South Yarra. Surrounded by some of her colleagues, Mrs. Mathers is trying out the lounge that was among the gifts the staff presented to her.

Moomba finalist



Miss Lorraine Zimsen of the Statistical Division of the Accountancy Branch, was one of the seven finalists selected from over 300 entrants in this year's quest for *The Sun Moomba Queen*. Lorraine together with her parents, came to Australia from Ceylon, eight years ago. Her hobbies are reading, painting, and water skiing.

Oysters at Merimbula

Among the presents received by Mr. M. D. P. McIntosh when he retired last month as a signalman at Caulfield, was a book on house repairs that was autographed by his colleagues at the station. It looks as though there might be some hammering and sawing at the McIntosh residence in the near future—that is, after he returns from a trip to Merimbula. "I'm really looking forward to those oysters at Merimbula", Mr. McIntosh told *News Letter*.

Another signalman who retired last month is Mr. Len Yemm who had worked in E Box at Jolimont for the past 19 years. A footballer of note in his younger days, Mr. Yemm played for Hawthorn and Oakleigh and also for the V.R.I. against teams from the police, fire brigades, Yellow Cabs, etc. He intends to live in Queensland.

Old issues wanted

Mr. Ian Weickhardt has *News Letters* going back to 1951, but some copies are missing. He would like to obtain the missing copies to complete his collection, and would be glad to offer in exchange other copies or items of interest to railfans. Incidentally, Mr. Weickhardt is not a railwayman—he is a Technical Instructor with the P.M.G.—but, as well as being a keen *News Letter* reader, he has another link with the V.R., as, each year, his classes include several railway telecommunication apprentices.

The issues wanted are February, August, September, and December 1959, and January, February, March,

Paints in oils



Mr. Foster and one of his paintings. It is a Hong Kong scene done from a black and white sketch.

While away from work as a result of an accident, Mr. J. Foster, a boiler-maker-welder at Newport Workshops took up painting in oils. Although his only previous experience in that line had been confined to modelling, Mr. Foster found his new hobby both interesting and rewarding.

"Painting teaches you to closely observe the world around," he points out. He paints mostly landscapes, and has also done a few racing cars.

Mr. Foster was recruited in England for the V.R., in 1950. There, he was working in shipyards, and living near Birkenhead.

Before arriving at Newport Workshops, he worked at the Departmental motor garage for about five years.

Mr. Foster's eldest son, Timothy, on national service in Vietnam, is also a boiler-maker-welder—at Spotswood Workshops.

At Nathalia



Passing through Nathalia (on the Numurkah-Picola line) a *News Letter* photographer found Station Assistant R. K. Sloper (above) unloading inwards goods, and (at left) Stationmaster G. Nicholls exchanging staff with Firemen G. McKerral; in cab is Driver G. Simmons.

April, August, September, November, and December 1960. His address is

4 Judy Court, Vermont, 3133; telephone 874 4510.

RETIREMENTS

ROLLING STOCK BRANCH

Battiato, S., Jolimont, 14/5
 Chandler, J., Ararat, 10/5
 Chown, J., Newport, 18/5
 Cobbin, S. W., Newport, 24/5
 Davies, T. H., Jolimont, 10/2
 Emanuel, J. F., Newport, 10/2
 Gatehouse, A. R. C., Newport, 13/5
 Groves, A. D. W., South Dynon, 14/1
 Grigula, P., Geelong, 24/1
 Jacobsen, F. C., Head Office, 29/1
 Jenkins, T., Bendigo, 28/3
 Kotsanis, V., Newport, 9/5
 McCallum, H. L., South Dynon, 1/5
 McGrath, T. R., Maryborough, 15/1
 Poole, S. G., Shelter Shed, 1/5
 Rankin, K. J., Motor Garage, 8/5
 Sneddon, J., Ballarat Loco., 28/5
 Supple, W. J., Geelong, 14/3
 Townsend, J. F. B., Ballarat North, 24/1
 Walsh, M. J., Jolimont, 29/3
 Wignall, T. F., Newport, 20/5

TRAFFIC BRANCH

Biddington, P., Spencer Street, 30/4
 Herlihy, T. W., Dimboola, 14/5
 Llewellyn, A. W., Bendigo, 10/2
 Milch, G. C., Melbourne Goods, 3/5
 McDonald, D. H., Murtoa, 12/5
 Rickard, J. E., Spencer Street, 18/5
 Smarrelli, L., Melbourne Goods, 19/2
 Weiland, L., Melbourne Goods 23/5

WAY AND WORKS BRANCH

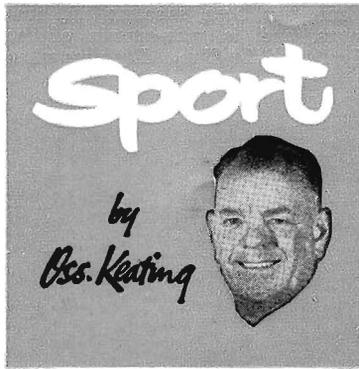
Dal Santo, M., Laurens Street, 24/5
 Finn, J. A., Anstey, 1/5
 Harrop, J. J., Dunolly, 29/3
 Jones, R. C., Springvale, 11/5
 La Porta, M., Laurens Street, 25/11
 O'Connor, K. P., Ballan, 15/1
 O'Halloran, J. J., Ironworks Divn., 21/2
 Peckett, E. W., P.W.M.D. S'wood 10/5
 Satchell, W. W., C/o. T & T Supvr., 16/5
 Walls, A. W., Bendigo, 13/5
 Woolnough, W., Echuca, 2/5
 Wyatt, R. F., Spotswood, 13/5

STORES BRANCH

Musicka, J. H., Clothing Depot, 26/5
 Slatter, E. J., Perm. Way Depot, 9/5
 Timmons, J. P., Head Office, 2/5

ACCOUNTANCY BRANCH

Cadan, F. D., Head Office, 11/4
 Cooke, (Mrs.) S., Head Office, 12/2



V.R.I. wins Triggs Shield

For the first time in seven years, a V.R.I. team won the Triggs rifle shooting competition for members of Railway Institutes.

Held at the Williamstown range on March 8, it was conducted concurrently with the Composite Teams event in this year's Queen's Prize. The Victorian team, with an aggregate of 189 points out of a possible 200, also finished fourth out of all teams in the Queen's tournament. As an indication of our team's good shooting it is interesting to note that the highest score in the Composite Teams event was 191 out of 200; if the V.R.I. team had scored one more point they would have finished second instead of fourth in that event.

The Triggs Shield was donated in 1914 by a Mr. A. B. Triggs (a wealthy N.S.W. grazier), for perpetual competition among the railway riflemen of Australia. The shield was presented by the donor to the N.S.W. Railways and Tramways Rifle Union, and it has been completed for since 1914 by, initially, the N.S.W. and Queensland Railways Institutes. About 1925, the V.R.I. came into the picture. Over the past 10 years, Victoria, N.S.W. and Queensland have competed for the shield, but it is hoped to arouse the interest of other Railway Institutes in this competition.

The Triggs event is held concurrently with the Queen's in the host State. This year, only N.S.W. and Victoria were able to compete, but N.S.W. has won the shield on most occasions. In this year's event N.S.W. shot 185 out of 200, thus losing to Victoria by 4 points. However, to gauge the quality of the opposition it might be mentioned that the non-shooting captain and coach of the N.S.W. Triggs team is a triple Queen's winner, and the team also included one shooter who has been runner-up to Queen's winners on many occasions, and has won the Syme Aggregate trophy more than once.

The Victorian team who won the Triggs this year (previous wins were in 1925, 1926, and 1962) comprised Messrs. J. C. Chamney (Coach), P. W. Allen, W. E. Elmslie, J. W. Mitchell, and D. H. E. Schulze, who have all gained distinction in their own right in big bore shooting.

Jack Chamney, a foreman copper-smith at Newport Workshops, who has been shooting for 48 years, has competed with some success in many Queen's events. He is a Councillor of the Victorian Rifle Association, and this year was, for the fourth time, the Chief Range Officer, a very important and responsible position. Jack was also the first winner of the Jim Kirkwood trophy for the Institute shooter in the Triggs event with the highest aggregate score.

Percy Allen, fitter's assistant at Ballarat Loco, who has been shooting competitively for 38 years, has been captain of the Ballarat East Rifle Club for the past 15 years, five times club champion, and twice winner of the Ballarat District Union Rifle Championship trophy. Perc. is no stranger to the Queen's as he has won three badges, finishing third, fifth, and 28th in those events. (The first 30 shooters in each Queen's receive a badge).

Bill Elmslie, an E. T. driver, who also won this year's Jim Kirkwood trophy with a score of 49 out of a possible 50, is a member of the Western Suburbs Rifle Club and has been shooting for about 10 years. He was a member of the Australian team competing for the Dewars trophy against England in 1963 and 1964, and the Australian team in the matches against U.S.A. in 1962 and 1966; he is current Victorian three-position (standing, kneeling, and prone) 50-metre champion (small bore). In this year's Queen's he finished just outside the top 30 shooters, and thus missed a badge, but a bad wind change on the last shoot ousted him from a very favourable position in the top shooters' list.

John Mitchell, a pupil architect at Head Office, is the present club captain of the Melbourne University Rifle Club (he is doing architecture at the University under a pupilship from the Department). He was club champion in 1966 and 1967, and runner-up in 1968. In the four years he has been shooting, he has been in the team representing the Melbourne University in *Intervarsity* rifle shooting each year, and was a member of the Australian Universities' rifle team competing at Adelaide in 1967. He has gained a University Blue for rifle shooting, and also an Imperials Blue. The latter is awarded to members of the winning team in a shooting competition between all universities in the British Com-

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH

Browning, A. H., Ballarat Nth. 10/2
 Fowle, T. A., Jolimont, 25/2
 Regan, A. K., South Dynon, 15/2
 Stevens, P. L., Benalla, 26/2

TRAFFIC BRANCH

Nottle, R. H., Fairfield, 21/2

WAY AND WORKS BRANCH

Jackson, L. H., Ballarat, 11/2
 Malm, V., Special Works, 28/1

monwealth, the competition being conducted through the mail.

Dick Schulze, a driver at Geelong

Loco, has been shooting for nine years, and is vice-captain of the Geelong Rifle Club. Dick has taken

part in two Queen's events and reckons that next year will be his year.

Interest in rifle shooting as a sport is increasing, particularly among the younger railway staff, and when the next Triggs is held (at Brisbane in 1970) we may be able to repeat this year's performance.



Rifle shooting: members and officials of the V.R.I. team that competed for the Triggs Shield—(from left) Messrs. F. C. Mitchell (Gen. Sec., V.R.I.), P. W. Allen, D. H. E. Schulze, W. E. Elmslie, J. W. Mitchell, and M. L. G. McKenzie (President, V.R.I.).



Chief Range Officer Jack Chamney calls for the teams to start shooting in the match for the Triggs Shield.

COUNTRY BOWLS WEEK

This year's fixture attracted one of the largest entries we have had for many years, and the closeness of the scores in most matches helped to make the week a most interesting one. The week opened when Mr. H. Jones, Hon. Secretary, Albert Park-V.R.I. Bowling Club offered the green and club facilities to our General President, Mr. M. L. G. McKenzie, who thanked the club and welcomed all participants on behalf of the Institute. The Chairman of Commissioners, Mr. G. F. Brown, then declared the week open, and the first event, the championship fours, immediately got under way.

By the time the quarter-final stage had been reached, it became quite obvious that the domination of this section by Ballarat and Bendigo was over, as not one Ballarat four had qualified and only one Bendigo rink made the final eight. In the final, some great bowling was witnessed, but Geelong 1 kept sufficient pressure on their opponents to enable them to take out the title, beating Composite 15 to 12. The winning rink comprised R. Robinson, B. Umlers, B. Wilkinson, and A. Scott. It was good to see the cup go to Geelong after so many attempts. Congratulations to their opponents—the Composite rink made up of K. Hatley (Donald), J. Baird (Korumburra), and B. Jameson and B. Gazzard, (both Colac). It should be borne in mind that the Donald and Korumburra players were complete strangers to one another and to the Colac pair, and had certainly never played together until this event, so it's a real feather in

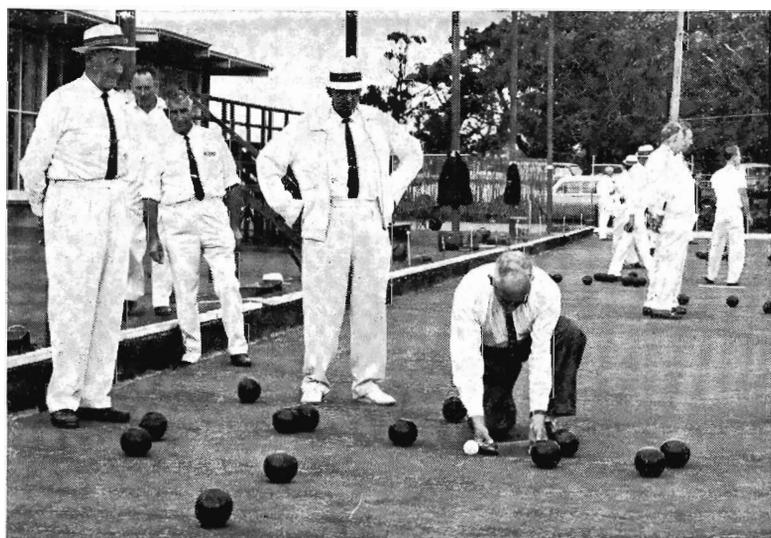
their caps that they made the final.

The pairs title went to what could also be called a composite team, when Bill McPherson of Bairnsdale and Clarrie Connor of Traralgon beat Vin Kelly and Reg Sawers of Bendigo, 18 to 16.

The other major title, the singles championship, also went down the line, when Roly Hughes from Hastings beat Clive Beanland 26 to 23. This game was a beauty, with the lead fluctuating after each end, and the result in doubt right up to the last bowl.

As an anticlimax, the consolation fours, which was almost an all Ballarat affair, saw Ballarat 5 beat their No. 1 rink, 22 to 7.

My sincere thanks go to Des O'Donnell and Bob Grace, (Institute councillors), and Harry Watts, (Hon. Secretary of the V.R.I. Social Bowling Club), who gave up a week of their annual leave to assist in the running of this tournament. And the co-operation of branch heads and staff clerks in arranging to free country staff for this event, is much appreciated.



Country Bowls Week: Harry Watts (Hon. Secretary of the V.R.I. Social Bowling Club) measures the heads during the pairs championship at St. Kilda Bowling Club.

VICTORIAN RAILWAYS

NEWS LETTER

MAY

VR

1969

BUFFET CAR



V.R. CHAIRMAN COMMENTS ON

UNEQUAL TREATMENT

A recent letter in the suburban press, from the Publicity Officer of the Ringwood Living Standards Committee, drew public attention to some little known anomalies in the allocation of loan money for Melbourne's public transport.

The anomalies are:

- road improvements are financed from *non-interest-bearing* revenue funds, but only *interest-bearing* loan funds are made available for railway improvements, and
- ratepayers, through the metropolitan improvement rate, are obliged to contribute towards the cost of freeways, but none of this money is made available for improve-

ment to public transport services.

It is difficult to see any justice in this discrimination, particularly as about six out of every 10 central city area workers travel by fixed-rail public transport.

In the letter referred to above, the writer added, *inter alia*, that "... The Commonwealth should provide money for railway improvements as they do for roads. This would go a long way towards providing the standard of public transport to which the public is entitled. Cheaper public transport is also desirable to draw cars off the roads. This lesson is one the Americans are learning almost too late".

Car park survey

A commuter car parking survey is being undertaken by the Department, to ascertain where more metropolitan station car parks can be provided and existing areas extended. The result of the survey should be known about the middle of this year.

Since 1954, when the Railways instituted free commuter car parks at metropolitan stations, the number of car spaces made available on railway property had risen from 590 at 44 locations to 6,276 at 98 locations in 1966, and to 7,504 spaces at 109 stations in 1968.

The Railways have thereby provided the equivalent of 47 acres of car parks at metropolitan stations for commuter use, equal to 28½ miles of kerbside parking, at no cost to users, municipalities or other bodies.

Previous surveys have shown that about three-quarters of the commuter car parkers live within the municipality in which the railway station lies. This means that the Railways have provided 35 acres of off-street car parks for the benefit of council ratepayers, allowing municipalities to concentrate their resources to provide car parks for shopping centres, etc.

As most commuter car parkers

travel during the peak period, Railway car parks have relieved authorities from the obligation to provide road space and city parking facilities for 7,500 vehicles.

The practical capacity of a single lane highway is 1,500 vehicles an hour travelling at 35 m.p.h. Therefore, it can be argued that commuter car parks already provided by the Railways are equal to the capacity in peak hours of five lanes of roadway in each direction, if cars were used instead of trains for completion of journeys.

Based on the average rail commuter journey of 8½ miles, road authorities would have to find the equivalent of about 42 miles of dual-lane highway to cater for train travellers now parking their vehicles at metropolitan stations.

The cost of providing multi-storey car parks in inner urban areas is about \$1,150 per vehicle, and that excludes land cost. To house 7,500 vehicles a capital expenditure of up to \$9 million, excluding land, would have to be found.

As others see us

"Most Australian railway enthusiasts have a tendency to regard U.S. railroad practices and equipment as being markedly superior

to our own—from both an administrative and technological viewpoint. So it was not without surprise that some of us listened to the comments of a party of 60 members of the Pacific Coast Chapter, American Railway and Locomotive Historical Society, who visited Victoria and New South Wales recently.

"Their remarks mainly centred around our passenger rolling stock and facilities, and it soon became obvious that the visitors were extremely impressed by such equipment as Melbourne's new "walk-through" Harris carriages, Sydney's double-deckers and high-speed inter-urban units, and the comfort and appointments of the VR's air-conditioned dining and sitting cars. Indeed, it would seem that these items (and similar equipment from other Australian systems) are better designed and maintained than contemporary American vehicles.

"The Americans were also undoubtedly impressed by the steam power provided on special trains run for their benefit in both States, and were openly amazed at the co-operation and assistance rendered to enthusiast groups by the railway administrations with preservation projects and operation of *railfan* tours—things which we often tend to take very much for granted . . ."

—*Australian Railway Enthusiast*, December 1968

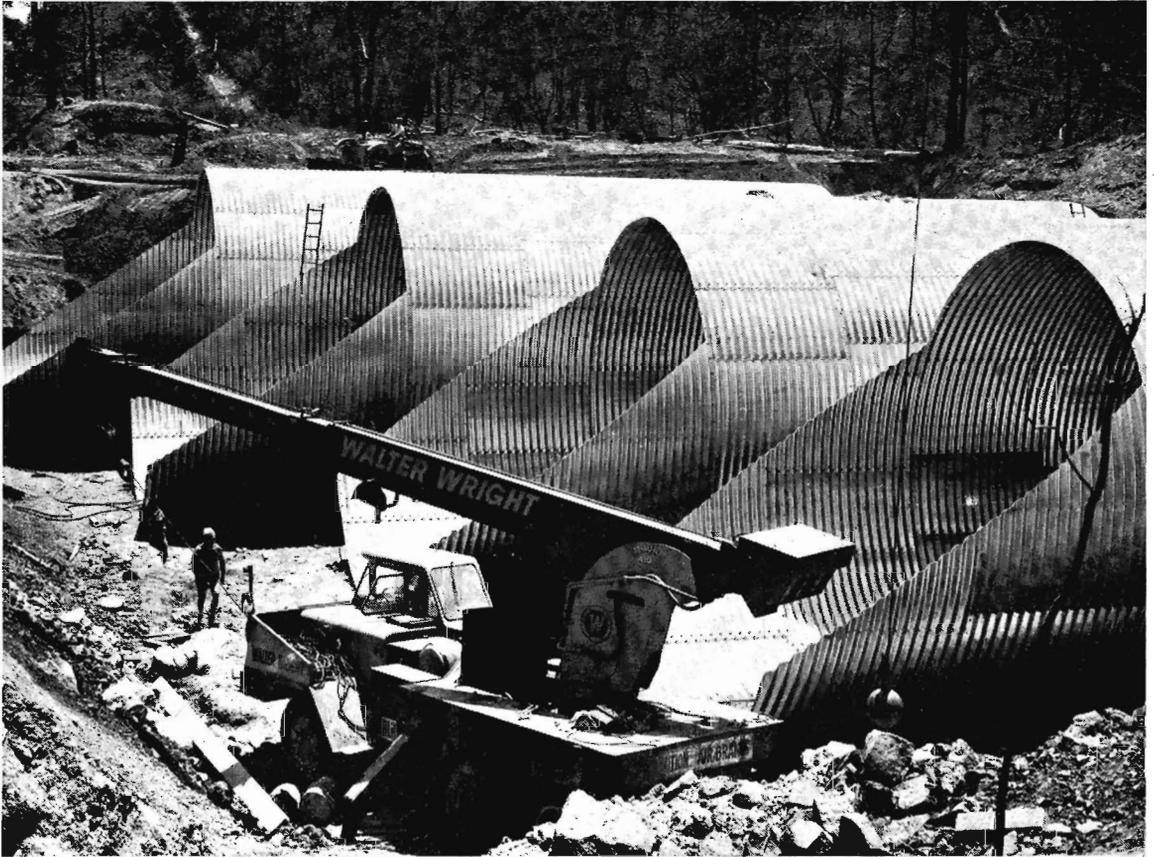
(The U.S. railfans' visit was covered in December 1968 *News Letter*—Ed.)

FRONT COVER

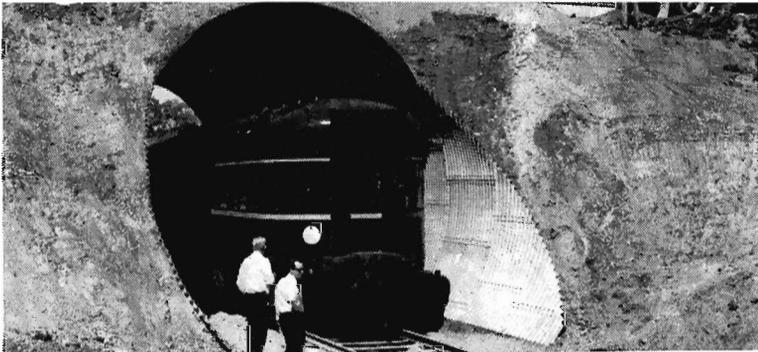
Messrs. E. P. Rogan, Deputy Chairman of Commissioners (*left*), and F. P. Kennedy, Superintendent of Refreshment Services, preview the new uniform for buffet car staff on standard gauge trains. In lime green material with navy blue *gobette* cap, it is being worn by Pat Jennings, a buffet car crew member. The uniform was originally issued to the Mt. Buffalo Chalet staff, but was so well received, it is being adopted for the two Melbourne-Sydney trains.

ANOTHER V.R. "FIRST"

The Victorian Railways and the Railway Construction Board have made history in Australia with the installation of galvanized corrugated steel pipes to replace bridges in one area, and in another, to form a short tunnel, through which the train will pass, in lieu of a conventional road bridge over the railway.



Steel pipes are in position for one of the bridges on the Hurstbridge line.

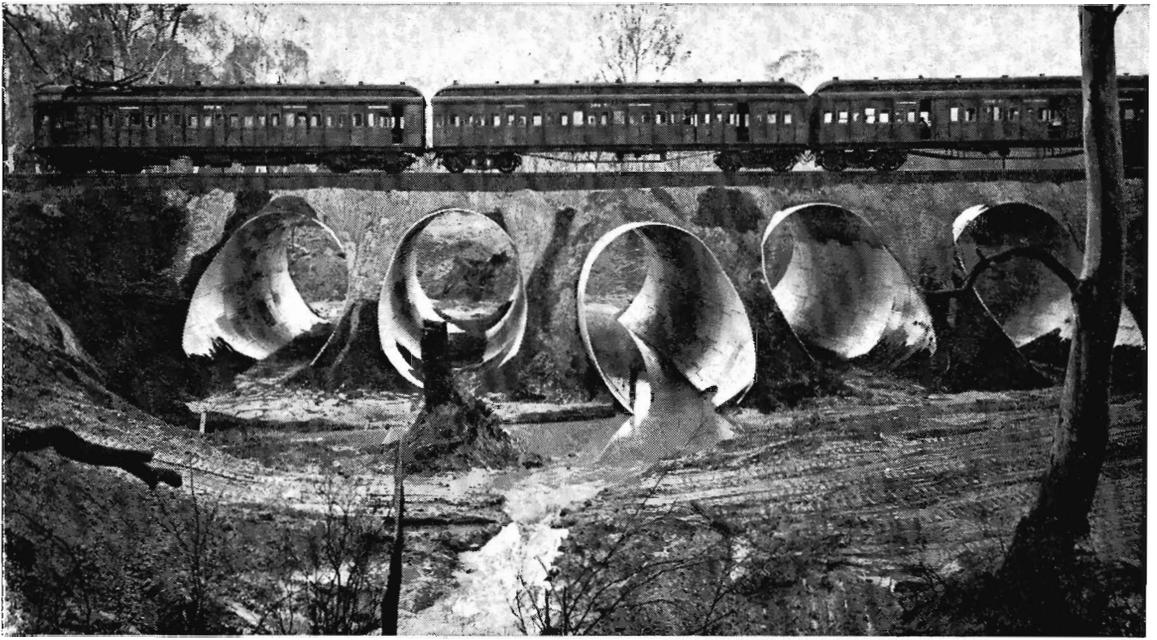


Inspection train passing through the pipe tunnel on the new track to Long Island Point—the first use in Australia of these pipes for a road-over rail bridge.

The pipes were manufactured by Armco (Australia) Pty. Ltd. at the company's Sutherland, N.S.W., plant, and the installations were inspected by railway and highway engineers from four states.

The tunnel is on a new track to Long Island Point, the site of major industrial development, including the Esso-BHP crude oil tank farm. In 1968 the Victorian Parliament authorized construction of a track connecting the area with the main line from Melbourne to Crib Point.

The grading of the new spur line in a cutting through undulating terrain required the design and con-



Train passes over one of the bridges on the Hurstbridge line—the largest diameter multi-cell installation in Australia.

struction of a road-over-rail grade separation.

The aim was to economically cater for both the present heavy construction traffic, and to allow future road width extension as the volume increased with development at the Point.

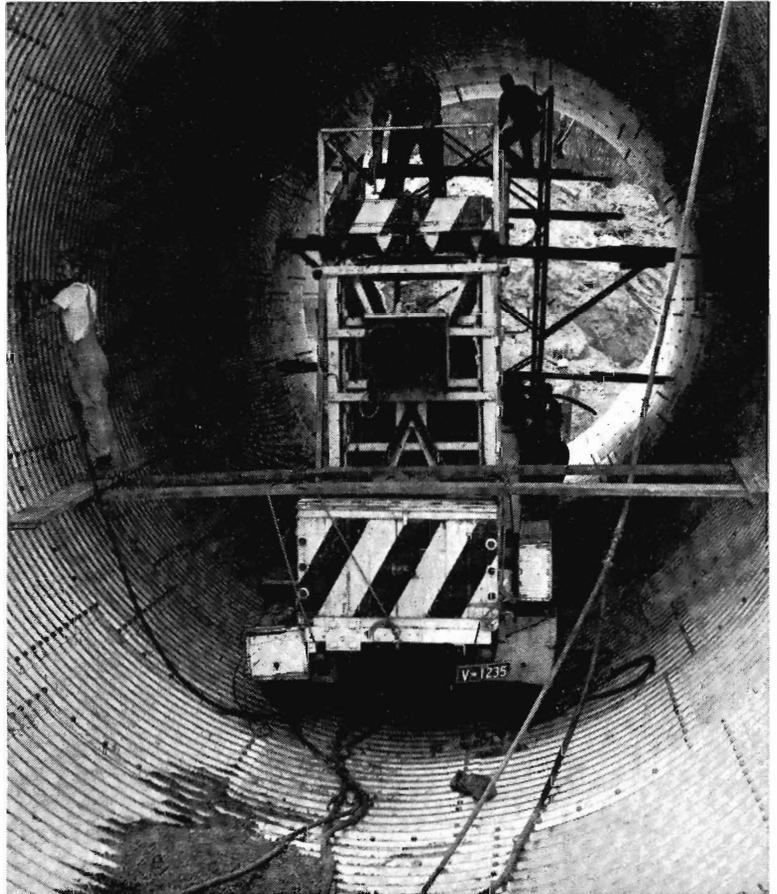
The Railway Construction Board studied all designs and materials before deciding to use the Armco Multi-Plate pipe—for the first time in Australia, but a form of installation common in the U.S.A. where Armco has its headquarters. One of the determining factors was that road widening could be done simply by installing rings of Multi-Plate steel pipe.

The installation was carried out by a 6-man work force, and was essentially a task of bolting the plates together. Main equipment was a crane to place the plates in position, and a scaffold mounted in the bucket of a front end loader. The scaffold enabled bolts to be placed and nuts to be tightened by pneumatically operated wrenches, from within the pipe.

Replaced bridges

The other use of the pipes—as replacements for bridges—has taken place in the Eltham–Diamond Creek area, 18½ miles north-east of Melbourne.

On January 8, four timber railway bridges were destroyed in bush fires. They were on the electrified suburban branch line to Hurstbridge, and carried the track over the Diamond Creek, crossing it four



Working from a scaffold mounted on a mobile truck, workmen are assembling pipes and tightening nuts with pneumatically operated wrenches.

times in approximately 30 chains. (See February *News Letter*, page 19)

Two bridges were on straight alignment, and the Department was able to replace those with bridging which had already been manufactured for other areas. But as the other two bridges were on a curved alignment, a ballasted deck would have been required, and there being no suitable super-structure in stock, it was decided to use the Armco pipes.

The order for ten 20-ft. diameter pipes to replace both bridges was placed with Armco on Friday, January 10—two days after the fire. The factory at Sutherland worked through the week-end, and loaded the plates for four of the pipes on standard gauge wagons at Cooks River on the following Monday.

These were railed to Melbourne, bogie exchanged at Dynon, and the wagons arrived at the Eltham site on the Wednesday.

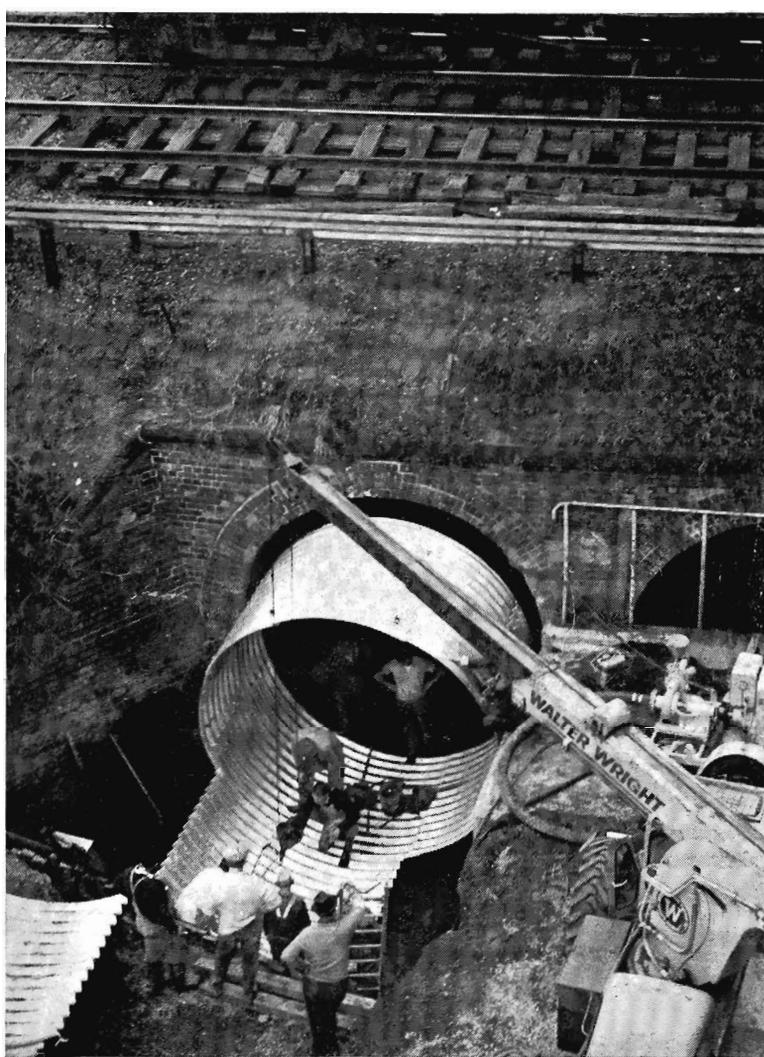
Assembly began the following day. The assembly of five pipes to replace one bridge was completed one week later, and assembly of five pipes to replace the second bridge was started on January 23 and completed on January 31.

Work on the new bridges and pipes proceeded concurrently, and the first plant train passed over the restored line on Sunday, February 9. Normal passenger service was resumed on February 10.

The total cost of restoration, including trackworks, overhead structure and wiring, and bridge construction, and the finishing work which is now under way, will be approximately \$300,000.

Culvert strengthened

An Armco 11ft. 6in. diameter pipe has been used to strengthen the Hanna Street culvert under the Port Melbourne and St. Kilda lines.



A steel pipe is being used to strengthen a culvert under the St. Kilda and Port Melbourne lines.

RICE CROP MOVING

The paddy rice crop, expected to be about 62,000 tons, is pouring in from N.S.W. rice growing areas to the Echuca rice mill, and will continue to the end of this month. On stand-by in the rice-growing areas are 46 hopper grain wagons which are used for a shuttle service between Caldwell, Burraboi, Deniliquin and the Echuca rice mill.

Milling has been going on since the rice harvest began last month; rice is leaving the Echuca mill in train loads. For export alone, nearly 10,000 tons are being railed right to ship-side in Melbourne. Although

the rice mill is in Victoria, the entire rice crop is grown in New South Wales. Echuca was chosen for the mill site, in 1954, because it was the junction station for the Deliquin and Balranald railway lines.

Bulk rice storages have been established at Burraboi (20,000 tons), Deniliquin (11,000 tons), and Caldwell (15,000 tons). Storages, each of 8,000 tons capacity, have almost been completed at Deniliquin and Moulamein. In addition, the Echuca mill can store 8,000 tons of paddy rice.

Experts forecast that the rice crop milled at Echuca could reach 100,000 tons in about eight years.

Rice growing became an established

Australian primary industry in 1925 when a few settlers in the Murrumbidgee irrigation area produced a 222-ton crop and proved the economic possibilities of rice.

The seed is usually sown, into dry ground, about late September. For this season's crop almost half was aerial sown. The plots are lightly irrigated early in October when the seed germinates. As the young plants emerge, a second irrigation is applied.

When the rice plants increase in height, water level is built up in each plot to about eight inches. The crop is then sufficiently matured by mid-March for the water to be drained off ready for harvesting to start in April. Within about six weeks the entire crop is stripped.

V.R.I. PRIZES PRESENTED

Prizes awarded in the 1968 Annual examinations of the Victorian Railways Institute were recently presented to the winners by Messrs. G. F. Brown, Chairman of Commissioners, and M. L. G. McKenzie, V.R.I. President.

Railway staff who received prizes were:

PETER ALEXANDER MEMORIAL PRIZES: Engine Working—Senior grade, K. Wallen (fireman, Numurkah); Junior grade, A. D. Murtagh (trainee engineman, Wodonga). Westinghouse Brake—Senior grade, G. Reiffel (fireman, South Dynon); Junior grade, F. W. Gill (fireman, Yarrowonga).

R. E. PURVES COMMEMORATION PRIZES: Diesel Operation—Senior grade, S. P. Bolwell (fireman, Horsham) first, J. Knight (driver, Geelong) second. Junior grade, R. T. King (fireman, Dimboola). Diesel Maintenance—K. J. Williams (fireman, Korumburra).

W. R. BROWN MEMORIAL PRIZES: Station Accounts and Management—Senior grade, F. Donnelly, (clerical asst., Mentone). Junior grade, R. J. Edlick, (A.S.M., Willaura). Ticket Checkers—S. E. Ibbotson, (block recorder, Caulfield.).

A. E. HYLAND PRIZES: Shorthand—Advanced Theory, J. F. Dowling (Head Office). Type-

writing—Miss M. Ould, (typist, Melbourne Goods).

V.R.I. COUNCIL PRIZE: Shorthand—Theory, Miss Y. Farrugia (typist, Head Office).

RETIRED RAILWAYMEN'S ASSOCIATION PRIZES: Engine Working—K. Wallen (fireman, Numurkah), first, A. D. Murtagh (fireman, Wodonga), second. Westinghouse Brake—G. Reiffel (fireman, South Dynon), first, T. K. Greensill (fireman, South Dynon), second.

BROTHERHOOD OF RESONANS PRIZES: Engine Working and Westinghouse Brake—Senior grade, W. T. Webb (fireman, Cohuna) first, K. Wallen (fireman, Numurkah), second, K. Greensill, (fireman, South Dynon), third. Junior grade, F. W. Gill (fireman, Yarrowonga), first, R. C. Rasmussen (fireman, South Dynon), second, R. T. King and W. Barron (both firemen from Dimboola) equal third. Station Accounts and Management—Senior grade, (Oral), J. J. Boundy (clerical asst., Croydon); (Correspondence) G. C. Polinelli (A.S.M., Dunolly). Junior grade (Oral). K. P. Fogarty (signal Asst.); (Correspondence) A. Caramia (A/A.S.M., Corio). Most Diligent Student, Senior grade, C. Battensby (A.S.M., Boort); Junior grade, M. F. Kick (station asst., North Port). Storemen's Duties—Storeman-in-Charge, P. Moore (Bendigo North). Storeman, Class 2, N. Torney, (Head



Mr. C. Battensby, A.S.M. Boort, (left) receives his prize from Mr. Brown.



After receiving her prize, Miss Y. Farrugia (typist, Head Office), is congratulated by Mr. McKenzie.

Office), first, J. Innes, (Caulfield), second.

TRANSPORT INFORMATION CENTRE



The new Transport Information Centre at the Victorian Government Tourist Bureau, Collins Street, opened on April 21. It provides co-ordinated information on bus, tram, and train passenger services anywhere in the State.

Inquiries may be made personally, or by telephone—63 0141 for metropolitan services and 63 0202 for country services.

It is planned to issue easily readable time-tables with strip maps showing linking services and other useful information, as well as maps showing all transport routes.

At left, Miss Ariella Suban of the Victorian Government Tourist Bureau locates a metropolitan transport route to assist Mr. Bradley Wilson in answering a telephone inquiry.

WALKIE-TALKIES HELPED SUCCESS OF RAIL TRANSPORT IN BILLY GRAHAM CRUSADE

All concerned with planning for the *Billy Graham Crusade* agreed that trains were the best way of moving the large numbers of people who would be using public transport to attend the Crusade meetings. And events proved them right. Trains had the great advantage of eliminating road congestion and parking problems. And special concession fares were offered. A Commercial Agent and the Traffic Liaison Officer visited country centres and successfully canvassed for the traffic.

Specials were run each day from March 14, building up to a maximum movement on the final day, Sunday, March 23. Melbourne people were easily accommodated by the suburban service, but for the country, a total of 25 specials were run during the period of the Crusade. For the concluding day, 12 specials came from the Warrnambool, Kaniva, Swan Hill, Tocumwal, Albury, Bairnsdale, and Yarram lines.

All the meetings except the concluding one were held at the Myer Music Bowl; for these, country trains terminated at Flinders Street and were stabled in the Flinders Street Yard. With the final meeting at the Melbourne Cricket Ground, 10 country trains were handled at Richmond, and two at Jolimont. While Flinders Street and Spencer Street each stabled two of these trains, it was necessary to stable and service the remainder at suburban stations—Caulfield (3), Box Hill (2), and one each at Sandringham, Heidelberg, and Fairfield.

Walkie-talkies

Good planning by Traffic Branch officers enabled country people returning after the final meeting, to board their trains at Richmond and Jolimont, in orderly comfort.

On the forward journey, passengers had each been given a leaflet showing the area outside the M.C.G. where they were to assemble. On arrival at the M.C.G., they were also shown the areas, which were marked by signboards.

After the meeting, as they assembled in the areas designated for each train, Traffic Branch officers used walkie-talkie radio to keep in touch with staff at the Richmond and Jolimont stations. This is the first time that walkie-talkies have been used on the V.R. for this purpose. It ensured that

country passengers did not arrive at the stations until their trains were docked at the platform, and also obviated passengers joining the wrong train.

That this well-planned service given by the Railways was appreciated can be seen from the following letters.

"This note is to pass on to you our commendation of the courteous and efficient way your officers at Richmond railway station handled the crowds attending the final rally of the *Billy Graham Crusade*. This helped to make the day a most enjoyable and memorable one.

We are also most appreciative of the generous concessions made for country travelling. This was a tremendous help, and also a significant factor affecting the numbers who were able to travel from the country . . ."

—John Chalker, Albury North High School

"On behalf of the local *Billy Graham Crusade* Committee, I wish to express our thanks to you and the Traffic Branch for the wonderful service provided by the Railways on both Sundays, March 16 and 23.

"Those who travelled from here on the 16th were loud in their praise of the services rendered and I can support that from my own experience on the 23rd. A colossal job was well and truly done and we are very appreciative . . ."

—(Rev.) F. S. Belcher, Yallourn, writing to Mr. P. H. Asker (Commercial Agent)

"I would like to express my appreciation of the courteous, helpful manner of the railwayman with the walkie-talkie on Platforms 9 and 10 when the trains were departing after the *Billy Graham Crusade* service . . ."

—(Miss) Daphne York, Royal Crescent, Camberwell, writing to the S.M. at Richmond

"I, along with many others, would like to say how much we appreciate the way things were organized and carried out for the *Billy Graham Crusade* . . . Whoever was in charge of the train running on Platforms 4 and 5 did the job extremely well . . ."

—(Mrs.) E. Cracknell, writing to the Secretary

" . . . Far too often we hear only criticism of Government facilities and public servants; but in this letter I wish to speak on a note of gratitude.

"It is in connexion with the two special trains from Bairnsdale to Melbourne to meetings of the *Billy Graham Crusade*.

"Not many people will know that the initial move for a special train came from the Railways. A representative was sent from Melbourne to talk with the local committee, and make suggestions and offers for transport to the Crusade.

"In all the planning and details that followed, we received the utmost co-operation and assistance from the Railways; as well as nothing but courtesy and helpfulness from the Bairnsdale staff . . ."

—(Rev.) E. R. Gronow (extracts from letter in Bairnsdale "Advertiser", 27.3.69)

NUTS FROM BRIGHT

From the middle of last month, the new season's nuts from the Ovens Valley have been moving by train to Melbourne and Sydney, and the traffic will continue during the next four weeks. About 35,000 lb. of nuts are being railed twice a week from Bright and Porepunkah this season, in consignments of 60-lb. bags. They are mostly chestnuts with smaller quantities of walnuts and hazelnuts.

Bright, Porepunkah, and Wandiligong districts produce about three-quarters of Victoria's nut supply. Some of the area's numerous nut trees are up to 70 years old. Chestnut trees take up to seven years to bear, then each tree produces a crop of about 8 to 10 bags a year. Generally, nuts are not picked, but allowed to drop to the ground and then gathered. Chestnuts are raked to remove the burs.

The biggest consignments are railed on Monday mornings following the week-end collection. Smaller quantities move on Wednesdays.

It is thought that many of the

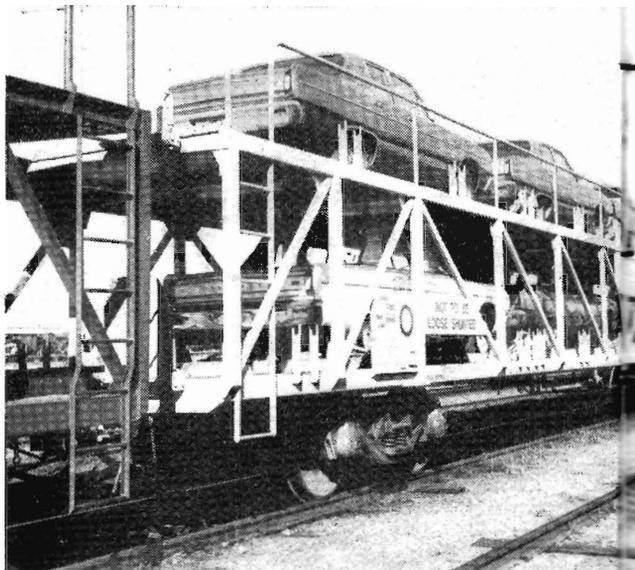
chestnut trees were planted in the early days by English settlers who tried to make this part of the Ovens Valley look like their homeland.



On the tree, the chestnut's prickles look uninviting, but, fortunately, they are easily removed.

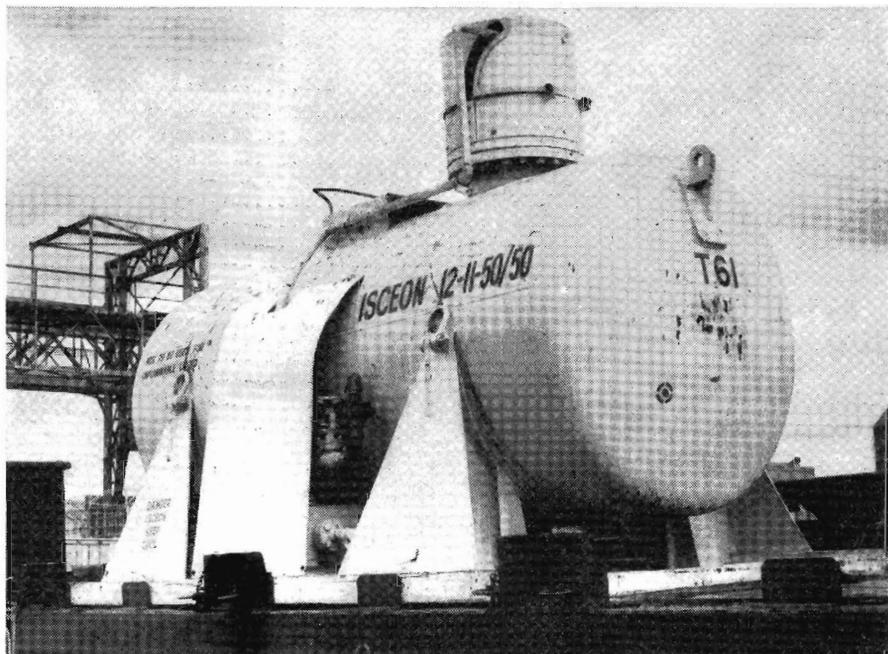
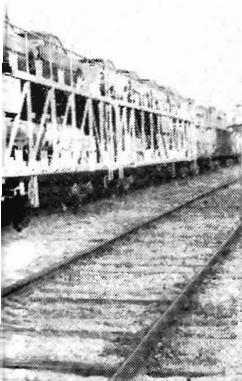
AROUND THE SYSTEM

TO THE WEST: A pointer to the growing importance of rail links between east and west, is this W.A.G.R. car transport wagon shown at Dynon shortly before leaving for Perth. It will be bogie exchanged at Port Pirie.

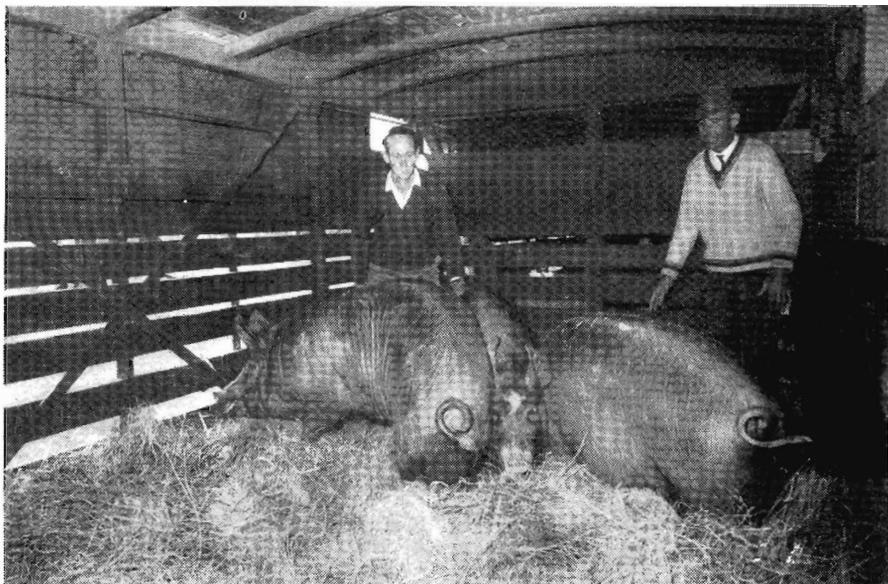


CONTAINERS: A train load of containerized tinned fruit for Victoria's first container ship – *Encounter* – is en route from Shepparton to Swanson Dock.





FOR AEROSOLS: An example of the specialized container is this tank holding liquefied Isceon. This is a gas used as a propellant in many aerosols such as fly spray, shaving cream, etc. It is carried from Sydney to the Melbourne factory of Monsanto Chemicals (Aust.) Ltd. at West Footscray. Each container holds enough to fill about 100,000 normal size aerosol cans.



PORCINE PLEASURE: Grunting with approval as they enter their straw-bedded van, these pigs were among the valuable livestock railed from Melbourne for Sydney's Royal Easter Show. They were some of the 19 pigs entered by Mr. J. McCrum of the Western District. Fourteen of the pigs won awards at the Show, including three championships and three firsts.

FOR GOURMETS

Among the most mouth-watering freight carried on the V.R. are the consignments of Murray cod and Murray perch sent from Mildura to Melbourne. About seven or eight boxes, each weighing 1 cwt., are raised twice weekly.

These expensive delicacies travel in special iced vans attached to the *Fruit Flier*. Netted in the River Murray by professional fishermen, they are collected at the river bank by Mildura road carrier, Mr. W. B. Jory. Twice a week, he makes the 126-mile trip from Mildura to Renmark (South Australia) for the fish, and arranges packing into boxes and icing.

The fish leave Mildura about 4.30 in the afternoon, travel the 350 miles by 3.45 next morning, and sell at Melbourne market that day.

"It's a terrific service, so far as I am concerned", says Mr. Jory. "The fish reach Melbourne in the same condition as they leave the river".

This co-ordinated rail-road fish service runs throughout the year, except during the closed season from September to the end of November.

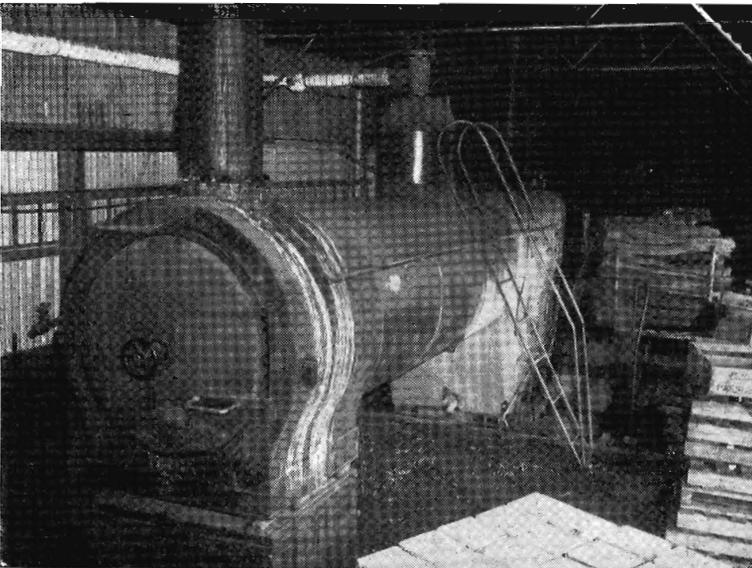
Generally there are more perch than Murray cod, because the

minimum size for cod is 21 in., but a perch needs to be only 13 in.



In the Mildura station yard, Mr. Jory packs Murray cod and perch into iced boxes before loading on to the *Fruit Flier*.

Still working



After the end of its economic life on the V. R., this old D3 boiler now plays a part in slaking the thirsts of young Australians. It has been adapted as a boiler at the M.O.N. cordial factory, Echuca.

THE ROSSTOWN JUNCTION RAILWAY

A 5-mile private railway between Elsternwick and Oakleigh, on which only one train ever ran, came to an abrupt ending 75 years ago this month, in 1894. Much of the old right-of-way is now parkland.

The Rosstown Junction railway was planned to develop a beet-sugar industry at Oakleigh. Beets were grown extensively in Gippsland, and William Murray Ross intended to connect the Gippsland and the Brighton railway lines.

In 1878, the Victorian Government gave authority for a single line to be built, provided it was completed within five years. Financial difficulties caused the time limit to be extended for a further five years and the Victorian Railways helped by supplying \$4,000 worth of materials for the line.

Later, Ross wanted to extend his railway, but this was rejected by Parliament in 1885. His offer of the unfinished line to the Government for \$40,000 was also declined.

In 1888, the venture was converted to a joint stock syndicate. On November 14, 1888, the line was "legally" opened to conform with the time limit conditions of the authorization act, which expired on that date. It was on this day that the first and only train, hired from the Department, ran. It carried guests from Elsternwick to Oakleigh. The line then remained idle till its dismantlement.

From 1883, when Ross refused to pay \$600 for materials supplied by the Victorian Railways, there were many disputes between Ross and the V.R. Friction arose in 1884 when the Commissioners of the day refused to allow the Rosstown railway to cross the Mordialloc line on the level. Agreement was reached two years later to carry the private line through a subway. Two-thirds of the cost was to be paid by the V.R. But, on completion of the subway, there was further argument following adjustment of Ross's earlier debts.

Between 1888 and 1891, the Victorian Railways supplied more materials for which no payments were made. Efforts to secure settlement of money owing by Ross were unsuccessful. Because of this, the Victorian Railways, in 1894, disconnected the Rosstown railway from their system at both Elsternwick and Oakleigh. The company ultimately went into voluntary liquidation.

RAIL USERS SAY...

Case recovered

Last week I inadvertently took another man's brief case from the rack in the train in which I was travelling to Surrey Hills.

In the ensuing efforts to trace the owner of the case, and to recover my own case, I spoke to station staffs at Ringwood, Lilydale and Surrey Hills. Through their efforts I recovered my case and was able to return the other case to its owner.

I would like to record my appreciation of their efficiency, but more than anything else I wish to express appreciation of the wonderful courtesy and co-operation shown by everybody I spoke to at these stations. So impressed I was with this courtesy that I would have written this letter whether or not I had recovered my case . . .

—E. C. Curtain, Russell Street, Surrey Hills, writing to the Commissioners

Special to Mildura

A letter has been received from the secretary of the Bairnsdale Urban Fire Brigade expressing appreciation for the comfortable travel provided on the special train from Melbourne to Mildura for the Urban Fire Brigades' annual demonstration.

The Overland

Writing to the Commissioners, Mr. Eric H. Biggs, Sydney Street, Nambour, Queensland, thanks the staff of *The Overland* for the service he and his family received during a recent interstate trip.

. . . Solely due to Conductor Farr's very willing help and sympathetic manner, I was saved much worry and distress on a recent trip to Melbourne when my travelling companion, who held the tickets and money, failed to board the express.

—(Mrs.) G. Scragg, Blair Athol, S.A., writing to the Commissioners

School sports

On behalf of the Werribee High School, I would like to express our sincere thanks for the courtesy shown to us in the arrangement of trains for our swimming sports. Mr. Martin, under very trying circumstances, provided us with an understanding service which we greatly appreciate.

—E. J. Daniels, Principal, writing to the Supt. of Train Services.

Parcel to Warragul

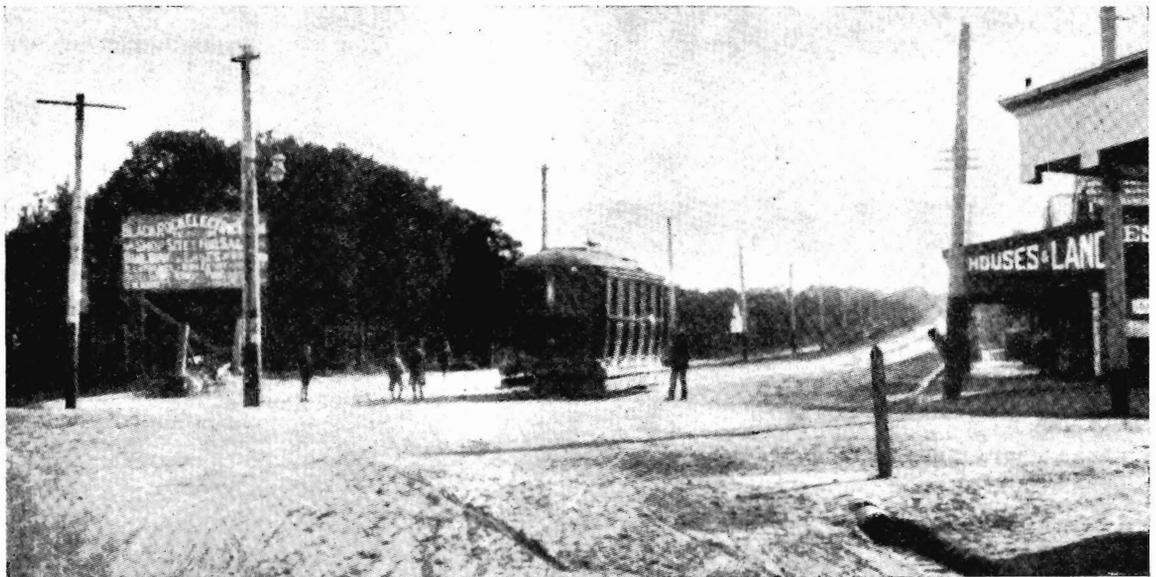
I wish to express my thanks for the excellent service provided in delivering a parcel containing a living culture of pond organisms to this school. It was dispatched by air-freight from Sydney on Monday 17.3.69 and delivered to this school at 11.40 a.m. next day. The parcel was marked for immediate delivery and was obviously given that treatment and must have been dispatched to Warragul on the first available train, and then very quickly picked up at Warragul by the local carrier. I had not thought that such quick delivery would have been obtained and wish you to know how much we appreciate your excellent service on your part of our parcel's journey.

—Colin Ramsay, Biology teacher, Warragul High School

First railway mail

It is believed that the first recorded railway movement of mail occurred in America in 1831, when several bags were carried by the South Carolina Canal and Rail Road Co. The newly completed rail line hauled the mail 12 miles west from Charleston where the bags were transferred to stagecoach, and taken the remaining 100 miles to Columbia.

E.S.R. Terminus



This picture, probably taken about 1920, shows the terminus of the V.R. tram service at the junction of Beach, Bluff, and Balcombe Roads. This tram service—officially termed an electric street railway—ran from Sandringham to Black Rock from 1920–56, and was operated by the Department. From 1926–31, the service extended to Beaumaris. In 1920, blocks of land not very far from the corner shown, could be bought for about \$100. Today, the traffic at the junction is somewhat heavier, and the land prices somewhat higher—say about 100 times.

LINES FROM OTHER LINES

Windows sucked out

The Pennsylvania Central Railroad (U.S.A.) has issued orders that only trains with aluminium window sashes may operate on tracks adjoining its high-speed *Metroliners*. The reason is that the *Metroliners* have been sucking the windows right out of the older trains.

Not long after they went into service (on January 16 last) seven wooden-sashed windows were popped from their frames in two incidents at Chester. No injuries were reported. The *Metroliners* travel at speeds up to 110 m.p.h. (*Labor*)

N.S.W. double-deck trains

Few passengers using them have spared much thought for the exceptional engineering skill that went into the design and construction of the four motorized double-deck electric cars now undergoing evaluation trials in the Sydney metropolitan area.

Perhaps this is because computers of the harbour-side city have long since been accustomed to the size, shape and operational efficiency of the 120 double-deck trailer cars already in service.

A double-deck motorized car, however, was an iron horse of a different colour.

Double-deck trailer cars were not unknown on the U.S., French, German, and Japanese railways, but none had yet produced a double-deck motorized version that was entirely satisfactory. Although the Japanese have articulated double-deck units, they surrender much of their space to motors and equipment.

The N.S.W. double-deck power car is a surprising $6\frac{1}{2}$ tons lighter than the existing single-deck power car and carries 53 more seated passengers.

Encouraged by the success of the double-deck train experiments, the N.S.W.G.R. have let a contract for the supply of 16 such units for the Sydney-Gosford (56 miles) run.

Although of similar basic design to the units now in service around Sydney, these cars will be of stainless steel and fluted along the sides, as in the case of the *Southern Aurora*.

Of the 16 cars on order, eight will be motorized and eight trailer. Four of the trailers will be fitted with

controls to enable 4-car sets to operate as required.

First deliveries are expected early next year.

Little labels tell big story

American railways are spending upwards of \$US 20 million just to label their items of rolling stock.

In less than a year, a tiny patch of plastic strips will be telling a big story about the railway equipment to which it is attached. In a space about 20 in. high by 6 in. wide, 13 strips of a brightly coloured plastic material will contain an incredible amount of information, such as whether the rolling stock is a freight wagon, a piggyback trailer, a container, a locomotive, a passenger carriage, a brake van, or a piece of railway service equipment. The strips will also identify the owner of the rolling stock as well as its number.

The strips of plastic are part of a nation-wide automatic car identification programme, known by its initials as ACI. By January 1, 1970, every item of rolling stock used in interchange service must carry an ACI label. Privately owned equipment which does not have a label by that date will carry a temporary label.

The ACI system will require substantial investment. The nearly two million items of rolling stock in the national fleet must be labelled, at a cost of about \$US 10 per item.

The other part of the ACI system is a trackside scanner, which can read labels on equipment going by at speeds of up to 80 m.p.h. Working day and night through fog, rain, or snow, the scanners can read ACI labels and relay the information to local yard offices or to computers at headquarters. As far as the Illinois Central railway is concerned, plans provide for the leasing of about 60 scanners. Some will be installed at interchange tracks for automatic recording of rolling stock received and delivered. Other scanners will be placed at strategic points in yards and at repair shops.

—(*Illinois Central Magazine*)

Perth Terminal

The western passenger terminal of the trans-continental standard gauge railway is under construction at East Perth, less than a mile from the centre of Perth.

It will be completed in time for the standard gauge passenger services from Port Pirie.

The new interstate passenger terminal will then be named Perth Terminal, and the Perth railway station in Wellington Street will be renamed City.

Waterways threat to railways

In Europe a massive programme of waterway construction or improvement threatens the life blood of many railways—their bulk traffics. The great trunk routes of the German Federal Railway and others may suffer serious diversions of traffic if many more of the waterways are built, reports the *International Railway Journal*.

Among the canal works being done is a project in the German Federal Republic where the Rhine will be linked with the Danube. The project covers a route length of 425 miles and will create a single waterway of more than 2,000 miles—stretching from Rotterdam to the Black Sea—and suitable for barges and even ships up to 1,500 tons. It has been estimated to cost \$600 million.

European railways are under a great disadvantage when competing with canal operators as the railways carry all their overhead costs but almost all canal and river costs are borne by the community. (Railways in Australia are, of course, similarly handicapped when competing against road and air.—Ed.)

Microwave radio in N.S.W.

The N.S.W. Railways have let a contract for the installation of their first microwave radio link system. Providing 24 channels between Sydney and Orange, the system is part of a communications improvement programme for the Sydney-Perth standard gauge service.

The microwave radio link can be extended to railway subscribers throughout interconnected railway systems on the Australian mainland.

The radio equipment can be used to provide bearer circuits for data transmission and centralized train control systems.

The main terminal's radio antenna will rise above the 200 ft. high clock tower at Sydney Central Station.

The microwave system is scheduled for completion by April 1970.

STANDARD GAUGE SUCCESS

The benefits to the Victorian Railways, and Victoria, from the introduction, on January 3, 1962, of the 197-mile Albury-Melbourne standard gauge line, have been considerable.

The through service has brought the transit times by road and by rail, between Melbourne and Sydney, to almost the same (with the railways now holding a slight advantage), and has enabled the railways to provide for consignors, a highly attractive service that previously was not possible.

Forwarding Agents have put on rail a considerable amount of traffic that would have gone by road had not the standard gauge line come into being.

Before the standard gauge line was built, all freight from Victoria to New South Wales and Queensland (and vice versa) had, of course, to be manually transhipped, thereby increasing the possibility of damage, at one of the three break-of-gauge border stations. Of these stations (Albury, Oaklands and Tocumwal), Albury handled by far the greatest volume of traffic.

A considerable work force was employed at Albury solely to transfer goods. The consequent delay, due to the manual transhipping of the goods, mitigated against gaining new traffic, particularly that of a perishable or urgent nature.

Since standard gauge, Albury is now mainly a locomotive changing point and a remarshalling station for the through trains. Some freight trains are there for as little as 15 minutes.

Transhipping is still done at Albury, but principally of goods for North-East Victorian towns not served by the standard gauge line.

Freight train running times between Melbourne and Sydney are now around 16½ hours. Before standard gauge, transit times were 36 hours and more (including transhipping) for the near 600 mile journey.

The Victorian Railways now offer an overnight service to Sydney. Goods loaded at the Dynon Terminal (Melbourne) by noon can be available at Darling Harbour (Sydney) the next morning.

Rapid and reliable transport reduces the need of industry for warehousing and stockpiling goods, and much of the traffic gained by the standard gauge line could be the result of this transport service.

In 1962, only two freight trains each way per day were running. Today, an average of seven trains to Sydney, and nine trains from Sydney and/or Port Kembla run each day.

Big traffic increase

An indication of the success of the standard gauge line for gaining freight traffic can be gauged from the following table of traffic through Albury:

Period	Tons net
1.7.60-30.6.61 (before standard gauge)	800,000 approx.
3.1.62-5.1.63 (first year of standard gauge)	945,400
1.7.64-30.6.65	1,710,776
1.7.67-30.6.68	2,250,258

Without the standard gauge line, it is doubtful if rail tonnages would have increased much over the 1960-61 figure. The resultant 1.4 million ton (net) increase would have probably been carried by road—mostly over the Hume Highway causing inevitable congestion and damage to that road.

For passengers, the standard gauge ended the 80-year-old nuisance of changing trains at Albury. It enabled travelling time to be reduced by as much as 3 hours and 55 minutes and the new luxurious *Southern Aurora* to join the already famous *Spirit of Progress* and *Intercapital Daylight* (previously *The Daylight*).

Passenger travel on the standard gauge line for a normal month is now approximately 30,000.

As a direct result of the standard gauge line, a developing industrial complex has been established at Somerton, 14 miles from Melbourne. Each of the industries there at present has connexions to standard and broad gauge lines.

Bogie exchange too, has played its part. Goods can be consigned

from any station on the Victorian system direct to Sydney or Brisbane without transhipping. With the Port Pirie bogie exchange, and completion of the Western Australian portion of the national standardization project, direct loading is possible between Perth, some Western Australian destinations, any station in South Australia served by broad gauge lines, Victoria, New South Wales and Brisbane.

In the five years since the introduction of bogie exchange at Dynon, 103,000 rail wagons have used the system, including 30,000 in the 1967-68 financial year. A further 3,000 were handled at the Wodonga depot last financial year.

Forwarding Agents

The growth of Forwarding Agents is perhaps the most significant feature of the success of the standard gauge line in Victoria.

Forwarding Agents combine the best features of rail and road—the economies of rail on long haul, high density routes, with road providing the convenient door-to-door service at each end of the haul.

This form of co-operation has broken down the former war between the two forms of transport to the benefit of the public. It has led to the greater development of containerized handling (including Flexi-Vans) and other forms of road-rail co-operation and co-ordination, both here and overseas. It has given rise to the now large numbers of specialized rail wagons designed for only one commodity.

At the end of its days



Old V. R. rolling stock can end up in unusual places. Here, brake-van Z302, bought about 14 years ago from Bendigo North Workshops for use as a children's playroom is now a storage for soft drink bottles at the Koyuga store, where it rests alongside the Echuca-Toolamba railway line.

Rescue from river

Two railwaymen who recently saved a man from drowning in the River Yarra are Electrical Mechanic K. M. Paul and Assistant High Tension Linesman J. Skec. Arriving at Batman Avenue Electrical Centre early in the morning, Mr. Paul saw a car in the river and a man floating near it. He swam out to the man and began to tow him to the bank.



Mr. Paul



Mr. Skec is near that part of the river where the rescue was made.

When part of the way there, he was joined by Mr. Skec, who had swum out to help. Together they brought the man to the bank.

Mr. Paul works at the Footscray depot of the Electrical Engineering Branch and Mr. Skec's headquarters are at Batman Avenue.

Lost and found

When Station Assistant K. P. Marrinan, of Kyneton, was renovating his house recently, he found two railway tickets behind the wall lining. They were second returns from Melbourne to Braybrook Junction (now Footscray), and were dated February 26, 1892.

Dahlia grower

One of the best dahlia gardens in the Bendigo district belongs to Mr.

A. W. Llewellyn, who was farewelled last month after 34 years as a signalman at Bendigo. He was 46 years in the Department. Horticulture has been Mr. Llewellyn's lifelong hobby; he has raised quite a few new varieties of dahlias, and regularly corresponds with growers overseas.

50½ years

Mr. A. E. Hosking who recently retired as a Sub-foreman Train Examiner at Spencer Street had two notable achievements—his Departmental service totalled 50 years three months and two days, and he proudly recalls that he played with what he describes as "one of the best cricket teams ever"—the Wonthaggi team. Mr. Hosking was at Wonthaggi for 12 years, and, while there, he also played in the local football team.

Apprentice artist



Apprentice Peter Tzortzatos and one of his landscapes, *During the Drought*.

The hobby of Apprentice Fitter and Turner Peter Tzortzatos of Newport Workshops, is painting in oils. He has been sketching since 12, and, three years ago, began with oils, doing mostly landscapes and an occasional "still life". His method of working is to make pencil sketches and colour notes at the location, and complete the picture at home. Until recently, he painted on textured paper, but now uses wallboard. Peter is in his second year apprenticeship, and works in the Steel Construction Shop.

RETIREMENTS

ROLLING STOCK BRANCH

Bruce, W. M., Newport, 27/6
 Buxton, W. J., Jolimont, 2/4
 Clarke, S. W. R., Nth. Melbourne, 5/2
 Davis, G. A., Bendigo North, 24/2
 Duffin, H. W. A., Newport, 29/6
 Foon, F. J., Ballarat North, 13/6
 Fuller, A. R., Mt. Gambier, 18/3
 Hamilton, C., Motor Garage, 18/4
 Harrison, C. H., Bendigo North, 12/6
 Hawke, W., Maryborough, 12/4
 Jenkins, T., Bendigo Loco, 28/3

RETIREMENTS (continued)

Kervarec, L., Ballarat North, 22/6
Kowalczyk, W., Newport, 27/3
Marr, C. H., Dimboola, 4/3
Morrison, D. H., Donald, 11/3
O'Brien, F. L., Newport, 11/6
Payet, H. F., Newport, 14/3
Smith, J., Bendigo North, 22/6
Strahan, R. H., Wodonga, 22/3
Strefford, T. N., Newport, 27/6
Trayes, A. W., Bendigo North, 25/6
Tsesmelis, A. T., Newport, 12/3
Walker, K. J., Ararat, 6/3
Walmsley, J. H., Head Office, 22/6
Willoughby, T. A., Wodonga, 27/2

TRAFFIC BRANCH

Barbour, W. T., Melbourne Goods, 5/3
Brooks, R. R., Ballarat, 17/3
Capper, A. J., Box Hill, 19/6
Corben, A. S., Spencer Street, 20/6
Dwyer, E. N., Dynon, 13/6
Hartigan, M. M., Benalla, 27/6
Hosking, C. L., Head Office, 22/3
Lofts, Mrs. B., Flinders Street, 12/3
McCloy, C., Tooronga, 24/3
Mahoney, J. J., Spencer Street, 2/6
Perkins, T. A., Flinders Street, 31/3
Wicks, W. T. J., Dandenong, 27/6

WAY AND WORKS BRANCH

Bubeck, J. R., Spion Kop, 28/6
Egan, M. J., Special Works, 17/6
Eschholz, H. W., Wangaratta, 16/3
Fry, S. V. A., Elmore, 24/6
Hall, C. H., S & T Seymour, 28/3
Harlock, A. L., Warrnambool, 7/6
Hassell, S. F., Benalla, 8/6
Hepburn, J., Spotswood, 11/6
James, L. R., Seaford, 21/6
Lambe, A. J., Spotswood, 27/6
McCull, S., Cobden, 6/2
Meagher, J. J., Carisbrook, 25/6
Murray, D. M., Yarrowonga, 18/2
Petty, W., Benalla, 24/6
Proctor, K. W., C/o. S & T Engineer,
18/6
Scutella, A., Spotswood, 4/6
Shaw, T. F. G., Special Works, 15/6
Tanti, A., Spotswood, 19/6
Watts, H. G., C/o. Bonding Supervisor,
7/6
Williams, R. S., Maryborough, 19/3
Woolnough, W., Echuca, 18/3
Wyszynski, C., Geelong, 12/3

REFRESHMENT SERVICES BRANCH

Ryan, Miss J., Flinders Street, 26/7

ELECTRICAL ENGINEERING BRANCH

Bilston, A. E., Warragul, 12/6

ACCOUNTANCY BRANCH

Clausen, C. L., Flinders Street, 11/4

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH

Eeles, E. A., Newport, 12/3
Foletti, J. R., Donald, 22/4
Rivett, F. F. G., Newport, 12/3

TRAFFIC BRANCH

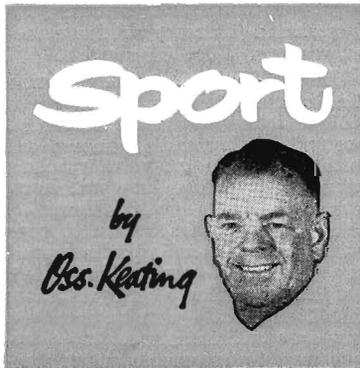
Bilson, J. G., Flinders Street
Yard, 30/3
Bravo, W. J., North Geelong, 9/4
Egan, W. J., Melbourne Goods, 11/4
Hartley-Tarbutt, S., C/o. Metro.
Sup't, 24/3
Salter, T. H., Flinders Street, 11/4

WAY AND WORKS BRANCH

Hemphill, W. C., Seymour, 23/3
Sheppard, D., Bandiana, 19/3

ELECTRICAL ENGINEERING BRANCH

Fisher, W. G. R., Overhead
Division, 2/4



Country Tennis Week

This event attracted the best entry we have had for many years. Seven centres were represented—Ballarat, Benalla, Geelong, Maryborough, Ouyen, Stawell and Sunshine. The standard of play in the team matches and the individual championships was of a very high quality, and the players themselves were evenly matched.

Mr. W. Walker, Secretary for Railways, representing the Commissioners, opened the proceedings at lunch on the Monday, while Mr. M. L. G. McKenzie, General President V.R.I., welcomed the players on behalf of the Institute.

Unfortunately, rain washed out play on the opening day, and this time lost caused quite a few bottle-necks in the programme as the week progressed. However, Ballarat No. 1 took out the coveted teams title, and the Donald McIntosh cup and the F. M. Mitchell trophy are now residing in the showcase of the local V.R.I. This was a popular win as these young blokes, some of whom are still apprentices, first entered Country Tennis Week about three years ago, and at that stage could only be regarded as battlers, but their improvement has been so marked that they were able to complete this year's round of matches without a loss. This, I might add, is only the second time that Ballarat has won the teams event, the first being in 1933 when the Donald McIntosh Cup was first competed for. I think after a gap of 36 years, nobody would begrudge them this year's victory. Runners-up were Geelong, who suffered one defeat (by Ballarat), then followed Benalla, Ouyen, Maryborough, Stawell and Sunshine equal, and Ballarat 2.

The Railways Singles Championship was again won by Russell Booley who gave a fine display of consistent tennis to beat Jim Forrest of Ballarat. The Open Singles final was fought out by a couple of our "old" stagers—Les Cook of Benalla and Jack Math-

eson of Ouyen. I feel a little sorry that this game could not end in a tie, as both Les and Jack have been two of our staunchest supporters of Country Tennis Weeks. They have competed for many years but neither of them had won an individual title until this year. However, there had to be a winner, and 1969 was Les's year.

In the doubles, Russell Booley and Wayne Schulz of Geelong gave a great exhibition of power tennis to beat their Ballarat opponents, Barry Matheson and Geoff Forrest.

So ended country tennis week. At the presentation ceremony, Mr. E. P. Rogan, Deputy Chairman of Commissioners, assisted by Mr. McKenzie, presented the trophies. Before concluding, mention should be made of how pleasant it is to see Maryborough once again represented, and we thank Peter Ivey for his efforts in this direction.

Country Cricket Week

What an interesting and keen week of cricket the 1969 fixture turned out to be. The return of Benalla (which I might add was due mainly to our old mate Doug Walker) eliminated the bye in the draw and so we had six teams competing—last year's winners (Korumburra), Traralgon, Sunshine, Dimboola, Ballarat and Benalla.

The players were welcomed by Mr. L. A. Reynolds, Commissioner, and Mr. McKenzie, and the week began with a great win by Ballarat over the 'Burra. Traralgon started well with a comfortable win over Sunshine, and Dimboola beat Benalla.

On Tuesday, Ballarat beat Sunshine outright, Korumburra defeated Dimboola, and Traralgon won from Benalla. Wednesday saw a great effort by Sunshine, who had Korumburra really struggling, but lack of experience let last year's champions off the hook, and they managed to get the points. Traralgon continued on their winning way, over Dimboola, and Ballarat beat Benalla.

At this stage, the result was wide open, Ballarat leading with 22 points, Traralgon had 18, and Korumburra 13 points. Thursday and Friday saw a dramatic collapse by Ballarat, who were beaten by Traralgon and Dimboola in turn and, in effect, dropped out of the race. Korumburra beat Benalla outright, and immediately came into calculations, while Sunshine, by defeating Dimboola gave notice that they could not be taken lightly. Finally, on Friday, the Korumburra-Traralgon match became what was virtually a final. A great effort by the bowlers in the morning, gave Traralgon the initiative and a good effort by their batsmen brought the D.S.J. Shield

and the F.M. Mitchell trophy to Traralgon for the first time ever.

What a great thrill for Max Lethlean, who has been bringing a team up to country week for more years than I care to remember. Korumburra were great in defeat. Early in the week they looked *no hoppers*, but in typical fighting spirit, they made the winners earn their victory. I think the main feature of the week was the evenness of all teams, and it now requires a really good side to win this title. The D.S.J. Shield and the F. M. Mitchell trophy were presented by Mr. Reynolds to Traralgon; and the batting and bowling trophies were presented by Mr. McKenzie to Alan Anton, captain of Traralgon, and to Peter McGregor of Ballarat, respectively.

Interstate Billiards and Snooker

It was our turn to be hosts to our friends from across the border, and it was mighty pleasant to be able to welcome our old mate Joe Lyons and his boys from South Australia. The teams matches were very close, but, possibly with the home ground advantage, we were just able to hold off our challengers and so retain the Dunkling Challenge Bowl. Believe you me, it was very close.

The billiards championship was won by Tom McLoughlin of South Australia, who beat Jim McKain of Victoria. In the snooker championship, the tables were turned when Jim beat Tom to win this event. The highest break in the billiards matches was made by Tom McLoughlin, and in the snooker matches by Frank Baldock, also of South Australia.

The presentation of trophies was made by Mr. Reynolds, assisted by Mr. McKenzie, at a delightful buffet dinner, held in the Princes Gate Cafe. I have made mention of this before but I feel it is worth repeating, that when we railwaymen celebrate we should make more use of our excellent Refreshment Services.

Intersystem Cricket Carnival

Space perhaps will not permit me to do justice to the team that represented us at the recent intersystem cricket carnival in Perth. Only 13 players were available (and this number included our Manager, John Cooney, and Property Steward, John McAllister) but with an ounce of luck we would have brought back the Commissioners' Shield to Victoria.

Ably led on the field by the captain, Stan Wallis, (Ballarat), the players turned in performances that would have done credit to any representative side of recent years. Actually, only 25 runs were the difference between winning the carnival and finishing

third, our two defeats being by Queensland (10 runs) and the eventual winners, W.A. (15 runs).

Fellows like Ted Barnes, who won the carnival batting average, Les Hill, who bowled as well as ever he has and Lou Balcombe, who excelled with the bat and proved an excellent wicketkeeper when Stan was hurt, were selected in the *All Australian* side. These blokes were ably supported by Ron Jenkins, who turned in one good performance after another as opening bowler with Les; Bob Coyne, who has now arrived as a first-class cricketer; Harold Wosley, who was always reliable under pressure; Kevin Schickerling, slow bowler; and Brian Brister and Tony Evans who both improved tremendously during the carnival. Even our manager and property steward—who, as mentioned earlier, were forced to take their place in the team on some occasions—acquitted themselves very well. I think we should be very proud that we were represented by 13 mighty good triers.

Stan Wallis was an inspiring captain and a fine example to his team both on and off the field. And my old friend, Bill Crowe, was a tower of strength behind the scenes, and—to use Stan's words—"always encouraging, giving sound advice and wonderful help to our manager".

Before concluding, might we offer our personal congratulations to W.A. on their victory. It must have been a great personal thrill to Les Havercroft, the carnival chairman, and Peter Joyce, the carnival secretary. And I might also thank these two

and their committee for the way they looked after our fellows.

Intersystem Bowls Carnival

An invitation has been received from the Tasmanian Railways Institute to compete in the 1970 intersystem bowling carnival to be held in Launceston, Tasmania, from Sunday, February 22 to Friday March 6 inclusive. The capitation fees for the carnival will be \$18 (men) and \$10 (women). Bowlers who are interested in making this trip should watch this page and the Weekly Notice for further details.

SOME V.R. MILEPOSTS

1858

Contracts awarded for construction of trunk railways; work began, Melbourne-Bendigo, June 7, Geelong-Ballarat, August 26.

1859

Spencer Street (or Batman's Hill) station opened and the first Government lines inaugurated: Melbourne to Williamstown and to Sunbury, January 13. Public traffic opened to Williamstown, January 17; to Sunbury, February 10.

Private railways opened: Princes Bridge to Richmond, February 8, and Cremorne (near River Yarra), December 12, by Melbourne and Suburban Railway Co.; St. Kilda to Windsor and North Brighton, December 19, by St. Kilda and Brighton Railway Co.

Country bowls champions



Members of the Geelong bowling team that won the V.R.I. Country Fours Championship: (from left) Albert Scott (captain), Brian Wilkinson, Bill Umbers, and Ron Robinson. They each belong to a different club in Geelong, and in the V.R.I. competition were playing together for the first time. And it was the first time that Geelong had won that event.

VICTORIAN RAILWAYS

NEWS LETTER

JUNE

VR

1969



V.R. CHAIRMAN COMMENTS

WILL SYDNEY-PERTH LINK AFFECT V.R.?

The suggestion is occasionally made that the introduction of a direct standard gauge link between Sydney and Perth will adversely affect the Victorian Railways.

Those who make this suggestion have evidently failed to examine the new link in its correct perspective, and have ignored the use of bogie exchange to give direct freighting over different gauges without transhipment.

Without disputing the value of the Sydney-Perth direct standard gauge link, the vital fact is that most of the interstate rail traffic in Victoria is generated within the State and is not the result of traffic passing through from other States. In the six months ended December 31, 1968, only 19 per cent of our total interstate tonnage comprised through traffic, and there is no certainty that this will be lost because of the new rail link.

Victoria has highly developed secondary industries that demand efficient rail transport to all major trading centres in the Commonwealth. This access is already available and will not be reduced by the new Sydney-Perth link.

Incidentally, most rail freight traffic from the eastern states to the west originates in Melbourne.

As railwaymen know, bogie exchange, pioneered in Victoria, allows rail freight in vehicles owned by W.A., the Commonwealth, S.A., Victoria, and N.S.W. to travel undisturbed between Perth and Brisbane.

So far as passenger services are concerned, Sydney-Melbourne and Melbourne - Adelaide - Perth passenger traffic is far greater than the direct Sydney-Perth traffic that might be lost. However, even with the latter there is still the tourist traffic potential of people visiting Melbourne on a Sydney-Perth trip. All systems concerned have agreed on the important principle that a Sydney-Perth passenger, for the same rail fare, may choose the alternative routes via Melbourne or via Broken Hill in either direction.

The Overland—Intercapital Daylight connexions will be retained for those who want to make a direct trip between Adelaide and Sydney and see the Melbourne-Sydney countryside in daylight.

Railway on Freeway

As *News Letter* went to press it was announced by the Minister for Local Government, Mr. R. J. Hamer, that work on Melbourne's \$24 million Eastern Freeway will start next year. It will be 5½ miles long and, when completed, will have four lanes each side of a new double-track railway.

It will go from Alexandra Parade, Collingwood, at Hoddle Street, skirt the Yarra Bend National Park to the intersection of the Chandler Highway and Princess Street, Kew, and then along the fringe of the Yarra Valley to Burke Road, and finally to Bulleen Road, Bulleen.

The railway will connect with the Clifton Hill line and run to East Doncaster. No date for a start on the line has been fixed.

The Chairman of the Melbourne and Metropolitan Board of Works, Mr. A. H. Croxford, said there was no other project like it in Australia, combining road and rail transport.

Graphite for points

In addition to its use on motor-operated points, graphite is now used as a lubricant for all newly installed or renewed points. The use of oil on points on which graphite has been applied is prohibited.

Progressively, graphite will be applied to all points, and the use of oil discontinued.

Further instructions were published in Weekly Notice No. 13 of April 1, 1969.

Bees by train

Honey Week, held last month, brings to mind how the Railways move bees, honey, and apiarists' supplies.

Queen bees, railed in *queen banks* that have 100 small cages, are bred by beekeepers who specialize in breeding rather than concentrating on honey production. During the rail trip, the queens are fed by a number of nurse bees that are included with each consignment.

Many of the queens are railed to Reservoir for Mr. R. J. Gell. He replaces queen bees after two years in a hive because they are then considered to be no longer able to lay sufficient eggs.

This year, a smaller number than usual of 60-lb. tins of honey have been railed from Victorian country apiarists, mainly at Bendigo, Wangaratta, Echuca, Orbost, Horsham, Stawell, and other stations in north-east Victoria and Gippsland.

Normally, Victoria's 1,100 beekeepers produce more than 7 million lb. of honey a year and about 88,000 lb. of wax.

According to Mr. A. P. Wills, Secretary, Victorian Apiarists Association, this year's honey production could be down by 50 per cent because of the aftermath of the 1967-68 drought.

Although there were good winter rains, there was very light tree budding and less flowers, because it takes up to 12 months for moisture to reach flowers from the ground, added Mr. Wills.

City dweller: Now that you've moved out of the city, what do you miss the most?

Outer suburbanite: The morning train!
—(L & N Magazine)

FRONT COVER

A train, a tree, and a station combine to make this pleasant picture of the up Albury passenger train staff being exchanged at Longwood by Station Assistant Eric McKay.

APPRENTICESHIP WEEK DISPLAYS



The Department's contribution to Apprenticeship Week (May 25-31) included a 3-day display and public inspection of the Technical College and Manual Training Centres at Newport Workshops.

Nearly 4,000 visitors saw the display and trade exhibits. More than a third of the visitors were students from metropolitan schools; others included parents and friends of railway apprentices, and representatives of educational and industrial organizations, and trade unions.

Exhibits at the College and training centres showed the relationship between the theory and practice of each of the 29 different apprenticeship trades in the Department. And the exhibits were not only fascinating in themselves, but spoke volumes for the skills imparted and thorough training given to railway apprentices. Visitors also saw apprentices at work on their normal trade training exercises.

Other railway activities during Apprenticeship Week were special displays, having the theme *Trained Tradesmen*, in the Lower Melbourne Town Hall, Victorian Government Tourist Bureau, Collins Street, Melbourne, and in shopping centres at Bendigo, Ballarat, and Geelong.

(Top, from left) Apprentice Telephone Technicians I. Ralph, J. Whittington, and P. Bourke show how teleprinter distortion is measured.

(Left) The functioning of a point operating mechanism is demonstrated by (from left) Apprentice Electrical Fitters (Signal) M. Hall, W. Gear, K. Maher, P. Walton, and J. Broughton.



PRIZE WINNERS



These V.R. apprentices were presented, last month, with prizes totalling \$900, by Mr. C. S. Morris, Chairman of the Staff Board. With the apprentices are (in front row, centre) Messrs. J. B. Kain (Principal, V.R. Technical College), C.S. Morris, and R. W. Curtis (Supervisor of Apprentices).

INTER-SYSTEM GOLF CARNIVAL

BY OSS KEATING

The recent Inter-system Golf Carnival was a milestone in railway golf as, for the first time, the New Zealand and Tasmanian railway systems each sent a team to compete. Victoria was the host State—for the first time since 1959.

Although the carnival was officially scheduled to begin on Sunday April 27, it virtually started on the Saturday, when the organizing committee met the incoming New Zealanders at Essendon airport. This group (12 men and two women) was one of the most popular at the carnival, and proved fine ambassadors of their country.

Sunday, of course, saw things really get under way, and what with the arrival of the West Australian, Commonwealth and South Australian groups by *The Overland*, and the N.S.W. party by *Southern Aurora*, Spencer Street station looked like the Victoria market on Saturday morning. However, the detailed time-tabling of the buses, luggage vans etc. paid off, and the parties were conveyed to the Hotel Federal, together with their luggage and golfing equipment, without a hitch.

Arrival of the Queenslanders on *Spirit of Progress*, and the Tasmanians from the airport meant that all participants were comfortably accommodated by 1.30 p.m. A managers' conference and party photos filled in the afternoon, and at 6.30 p.m. the welcome smorgasbord dinner officially launched the carnival. Participants were warmly welcomed by the Chairman of Commissioners (Mr. G. F. Brown), the President of the Victorian Railways Institute, (Mr. M. L. G. McKenzie), and the Carnival Chairman (Mr. H. E. Fletcher). Suitable responses were made by all competing State managers. The draw for the Commissioners' Shield and Tintara Cup was made by Mr. Brown (in spite of persistent rumours—it definitely was not rigged), and the major trophies (Commissioners' Shield and Tintara Cup) were handed for safe keeping, to Mr. F. M. Mitchell (General Secretary, V.R.I.), by Mr. M. Bourke, manager of the New South Wales contingent, the holders of these trophies.

Carnival starts

Golf got off to a great start on Monday, when, at 8.30 a.m., at the lovely Victoria Golf Club, in glorious weather, the Chairman of Commissioners hit the first ball of the tournament, a sizzling drive (off the

ladies tee, I might add) straight down the fairway. The event was an 18-hole stableford and the A grade section was won by J. Paternoster (S.A.) on a count back from G. Woolmer (N.S.W.), who both returned a score of 38 points. The B grade winner was L. Stafford (W.A.) who turned in the magnificent score of 41 points to beat W. Short (Vic.)

On Tuesday, the field was divided into two groups, N.S.W., Commonwealth, and W.A. playing at Commonwealth Golf Club, and the rest of the participants playing at Kingston Heath. This was the day on which an 18-hole stroke handicap event was played, and the winner at Commonwealth was T. Hoffman (Vic.) with a net 72 who beat R. Turner (Vic.), A. Parkinson (Vic.), F. Hunt (W.A.), and R. E. Cobby (W.A.) who all turned in a net 73. At Kingston Heath, G. Malcomson (Queensland) was the winner, his score also being a net 72, while the runners-up were J. Paternoster (S.A.) and T. Kiernan (Queensland) who both had a net 75.

Wednesday was a free day, and most of our interstate visitors took the opportunity to relax.

Championship day

Thursday was championship day and this, together with the associated events, was held at Kingswood Golf Club, where 143 golfers began hitting off at 8.30 a.m. What a great day's golf this turned out to be! As the cards began coming in it looked as if the open championship was going to finish in a three way tie between Tom Wolfe (N.S.W.), Glen Cobby (W.A.) and Arthur Knight (Vic.) as each of these players had returned a score of 119 for the 27 holes. Just as we were getting hot and bothered about arranging a play-off to decide the championship, in came Col Ryan (Queensland) with a great 118, and so took the Australian and New Zealand open title. After all this excitement, the minor championship contenders decided that they should get into the act; so Jack Clarke (N.S.W.) and Howard Humphrey (Vic.) each returned an 84 for the 18 holes. A play-off could not be avoided and eventually Jack won the title at the 21st hole. Events played in conjunction with the champion-

ship resulted in the A grade handicap being won by E. Brown (Vic.) on a count back from T. Wolfe (N.S.W.), the B grade handicap by H. Humphrey (Vic.), the Veterans Trophy by J. Clarke (N.S.W.) and the Diggers Trophy by E. Brown (Vic.)

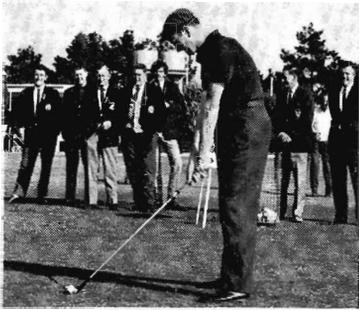
Shield and Cup

Then we came to Friday and the elimination rounds of the Commissioners' Shield and the Tintara Cup competition. These games were played at the picturesque Keysborough Golf Club, and in the first round Victoria beat Tasmania, Commonwealth was too strong for N.Z., W.A. had too many guns for S.A., and Queensland, after a mighty struggle, beat N.S.W. In the afternoon, Victoria managed to account for Commonwealth, but no decision could be reached in the Queensland versus W.A. match before failing light ended play for the day. The match committee decided that the unfinished games in this match would be completed on Monday morning at Keysborough. The rest of the participants (those not included in the various State Teams) played an 18-hole bogey event at Rosedale Golf Club. This turned out to be an all Victorian finish when A. Parkinson beat W. Donohue on a count back.

Saturday was a free day, and Sunday turned out a winner. Beautiful weather, which incidentally prevailed throughout the carnival, made the decision of the organizing committee to stage a picnic at Emerald an ideal one. Our visitors travelled to Belgrave by suburban train and thence by *Puffing Billy* to their destination. A first-class picnic lunch was boxed and provided by our Refreshment Services Branch, and this and other refreshments were served at the picnic spot. The visitors appeared to thoroughly enjoy the trip, particularly the ride on *Puffing Billy*, which was a novel experience for most of them.

The final

Monday, and back to the serious business of golf. The unfinished match was decided at Keysborough and resulted in a win by Queensland. Competitors were then taken back to the magnificent Metropolitan Golf Club where the final was due to



J. Kennedy, captain and manager of the Victorian team which won the Commissioners' Shield and Tintara Cup.



A group of competitors, officials, and visitors watch competitors hit off in the semi-final of the Commissioners' Shield and Tintara Cup competition.

begin at 1 p.m. The competing teams, Victoria and Queensland, had an early lunch and then settled down to do battle. And what a battle it turned out to be! Right throughout the match fortunes fluctuated—first Queensland seemed to have the advantage, then Victoria. At the half way mark, it was still very close, with the Banana Benders slightly the favourites. At this stage most of the Victorian spectators were biting their fingernails. Then the results began coming in—Spark (Queensland) defeated Kenny (Vic.); Climpson (Vic.) beat Helson (Queensland); Petch (Vic.) beat Heywood (Queensland); and we led 2—1. Then Whelan (Vic.) and Schafferius (Queensland) completed the 18 holes all square, and so continued for a sudden death play-off. Poulton (Queensland) beat Paterson (Vic.) to make it 2 all. Wins to Williams (Vic.) over Teague (Queensland) and R. Barlow (Queensland) over L. Barlow (Vic.) still had the teams tied to 3 all, and no result from the Whelan-Schafferius match. Jack's (Vic.) win over Trot (Queensland) was immediately offset by the news that Kennedy (Vic.) had gone down to Ryan (Queensland). The score was 4 all—with Whelan and Schafferius on the 20th.

Then the Vics. heard some great news. Manning, after being 3 down at the ninth was coming up the 18th, 1 up. He outplayed Malcomson on this last hole, and at last, victory looked possible. We had our noses in front and were leading 5—4 in the individual games. Hard on top of this came word that Knight (Vic.) was having trouble keeping up with young Kiernan (Queensland) and it was obvious that the fate of the match rested on the Whelan-Schafferius game. What a struggle they were having—but at last on this 20th hole, Schafferius faltered and Whelan clinched the game, to give us an unbeatable 6—4. Kiernan, however, not to be denied, beat Knight and so the final scores showed Victoria defeating Queensland 6 games to 5.

It was a great win to our blokes,



Sharpening up their putting are Glen Cobby (W.A.), left, who proved to be one of the biggest hitters in the tournament, and Col Ryan (Q'land) winner of the Australian and New Zealand Railways open championship.

but perhaps particular praise should go to the two "veterans" of the team—Jack Manning and Harry Whelan—who turned the tables on their opponents when both seemed on the brink of defeat. Nevertheless, all members of the team played their part in the victory. Queensland, on the other hand, must feel disappointed. On more than one occasion they seemed to have the match in their keeping, but Dame Fortune did not smile on them. Still, some wonderful golf was seen and they fought every game right out. Although beaten, they were certainly not disgraced.

On Tuesday, the final day, a 4-ball, best-ball event was played at Yarra Yarra Golf Club. There was a picnic atmosphere about the tees as the players hit off, the tension of the past few days was missing, and a relaxed game was enjoyed by all. Winners were R. Ryan (Commonwealth) and R. Spark (Queensland) who beat A Bitters (W.A.) and K. Bowe (Vic.).

Farewell dinner

The carnival officially concluded with a farewell dinner and presentation of trophies, a function which was also held at the Hotel Federal.

This dinner was chaired, of course, by our Carnival Chairman, Harry Fletcher, and was attended by many guests of the organizing committee, including Messrs. G. F. Brown, E. P. Rogan (Deputy Chairman of Commissioners), L. A. Reynolds (Commissioner), and M. L. G. McKenzie. Also present were the General Secretaries of the Commonwealth, South Australian, and Victorian Railways Institutes, representatives from a number of golf clubs on which we had played during the carnival, and from a number of business houses and sporting organizations that had donated trophies and helped us entertain our visitors during their stay in Melbourne.

Mention should be made of the interest displayed by Mr. G. F. Brown in all aspects of the carnival. He was a frequent and welcome visitor to many of the clubs, and on a number of occasions was on hand at the tee at hit-off time.

Members of the carnival committee are deeply appreciative of the help they received at all times from the Commissioners, of the assistance and advice received from the Refreshment Services Branch, the Electrical Engineering Branch, the Printing Division, and the very many Departmental officers who went out of their way at all times to help us. Perhaps a particular word of thanks should go to the Stationmaster, Spencer Street, and his staff, for the valued assistance in the handling of our visitors' luggage both on arrival and departure. Thanks are also due to the many private individuals and firms who donated trophies, and to the sporting organizations that helped us, and to the management and staff of the Hotel Federal who looked after the personal needs of our visitors. And finally thanks to all members of the Victorian party who cheerfully carried out their allotted tasks. The members of the organizing committee, particularly the Secretary (Alan Collins) and the Treasurer (Jack Baker), have every reason to be proud of what we in Victoria think was a "real beaut" carnival.

FACE LIFT FOR THE OVERLAND

In a \$1 million face-lift, *The Overland* will get a lounge-cafeteria carriage, power-generator brakevan, and modern luggage brakevan.

The new carriages, 75 ft. long, will be added to the two consists early next year.

The cafeteria section, for sitting passengers to buy their evening requirements and return to their seats, will occupy about half the carriage.

The other half will be divided into two lounges, each with eight seats,

and a 24-seat club section, with access to a bar. Detachable trays will be supplied for each chair in the lounge section. The lounge-club half will be reserved in the evening for sleeping berth passengers.

Because the sleeper passengers will continue to receive a free Continental breakfast, the whole carriage will be opened to sitting passengers in the morning for breakfast.

Three power-generator brakevans will also be built, each with two 165 K.V.A. diesel alternator sets. The 31 carriages for *The Overland* will

be converted progressively to use the A.C. power. At present all electric power needed on *The Overland*, for air-conditioning and lighting, is provided by D.C. axle driven generators under each carriage.

In addition, two modern luggage brakevans will replace the present type that have been in use for up to 60 years. The guard's compartment in each of the brakevans will be complete with toilet, desk, and fire-fighting equipment.

The vehicles will be built at the S.A.R. Islington Workshops.

THOUSANDS OF BUSHES

Winter sees thousands of rose bushes being railed from Portland. Starting with floribundas, climbers, miniatures and the bush type, they will continue until September; standards began in mid-June. In all, about 100,000 bushes will leave the district's nurseries.

Most of the roses from W. G. Treloar's Portland nursery, one of Victoria's biggest, are railed to buyers in almost every Victorian city and town, and to Sydney, Adelaide, Alice Springs, and as far away as Cairns, Queensland, and areas in Western Australia. Big supplies for sale at a Sydney department store also go by train.

About 50 to 60 orders, mostly from home gardeners, are reaching the Treloar nursery every day. Indications, so far, are that this season's top seller will be Carla, a slightly shaded salmon rose.

Buyers have about 500 varieties to choose from, with red roses predominating. The hybrid bush type are more popular than standards.

Each year buds of new roses are imported from overseas, but it is generally two years before these varieties reach the market. This season as usual, there are a number of new roses on sale for the first time. Some of the budding eyes have come from the famous Roseland Nursery, Norfolk, England.

"The rose bushes are packed in peat moss and wrapped in polythene for the rail journey," Mr. Treloar said. "The service by the Railways is amazing, particularly as some of the roses travel thousands of miles



Consignments of rose bushes are being packed at Portland's Treloar rose nursery by Mrs. Treloar (left) and Mrs. Sykes, for railing to buyers throughout Australia. At a later stage in the packing, peat moss is placed around the roots and the case wrapped in polythene.

by train before reaching their new owners."

Asked for suggestions for the type of rose needing a minimum of care and best suited for the average home gardener, Mr. Treloar gave the following list: Silver Lining—pale pink with silver base, My Choice—pink and yellow, Super Star—orange vermillion, and Carla. An ideal rose

suitable for bloom exhibition is Peter Frankenfeld—dark pink.

Mr. Treloar began his rose nursery about 10 years ago, with 100 bushes. Today, some of his roses are contract grown, but a recent acquisition of 20 acres of land at Portland will be devoted solely to the growing of rose bushes.

PUTS RAILWAYMEN IN THEIR PLACE

Folklore of the Australian Railwaymen—collected and edited by Patsy Adam Smith—Macmillan of Australia—\$4.95. Reviewed by Jim Morrissey.

By revealing the hardships endured and difficulties overcome in the early days of Australian railway building and operating (some of it not so remote from the present day) Patsy Adam Smith's latest book—*Folklore of the Australian Railwaymen*—reminds readers of the important part played by railway men—and women—in developing this country. The book comprises railway tales, traditions, legends, poems, and so on, many of which were collected by the author during an 11,500-mile rail trip all over Australia.

In these days of almost over-abundant transport, it is not easy to realize the radical change that the introduction of railways brought to this land of great distances and scattered people. In a few years, Australian transport leaped into the modern age, changing from the horse or bullock-hauled waggon—often crawling over rough tracks—to the fast and reliable iron horse.

The preface to the book puts it well. "In the minds of most Australians the railway is a country thing. It came with the beginnings of settlement, before there were large cities, while much of the land was still being explored and before much of it was settled. It was well timed: it not only linked these outposts one with the other and gave them access to the ports and an escape route from isolation, but with its regularity, well co-ordinated, complicated yet smoothly functioning activity it brought an order and unity that the new country had not yet experienced."

Patsy Adam Smith should need little introduction to *News Letter* readers. Her first book, *Hear the Train Blow*, was reviewed in July 1967. One of the very few books in which the Victorian Railways are the background, it gave a fascinating account of her life in a ganger's family. Much of it was set in the Mallee district.

Including cousins, 13 of the author's family were railway people.

Folklore of the Australian Railwaymen is the seventh of her books.

She emphasizes the unique quality of Australian railway lore. The famous U.S.A. reply to the question in a guard's exam. paper, "What do



Miss Patsy Adam Smith discusses her latest book with V.R. Conductor L. Cotter.

you do when you find a dead body?" was "Go through his pockets." The Australian version is, "Run a ring of phenyle round him to keep the bull ants off."

"The reports railwaymen are required to make have helped mould them into an unparalleled source of lore. They are all articulate, literate men with a memory. If they lack these qualities when they join the service, they must acquire them or leave because every ganger, porter, guard, and driver must write a report to his superior."

Naturally the book records a number of richly Dickensian characters, colourful types who might even deserve the immortality of a Micawber. At any rate, Patsy Adam Smith has done her part to ensure they will be remembered. Such types as Mrs. Delaney of Parattah, in central Tasmania. During the 1900's she used to board the train daily at Parattah with a basket of fruit, usually apples, which she sold to the passengers as the train went on to Launceston. Mrs. Delaney's wit was cutting, as some of the more printable stories show.

In the far north of Queensland was a ganger who carried a bible to lead his men in prayer before the day's work. As he was a powerful 16-stone man, hardly any repairer refused to join.

A chapter on railway rhymes has some interesting examples of the railway versifiers' work. A guard relates that he wrote over "a 100 poems" during his railway career. Most of them "are on top of the kitchen dresser, including one of 400 verses that followed the course of the Second World War from beginning to end".

The chapter includes *The Song of the Institute* composed for the Victorian Railways Institute, in 1910.

From the alpine railways of Tasmania to the far north of Queensland, the author has collected something—an interesting piece of railway history, or a story, often humorous, sometimes grim, but providing, in all, a rich variety.

Folklore of the Australian Railwaymen is the kind of book that you find easy to pick up and start reading—but not so easy to put down.



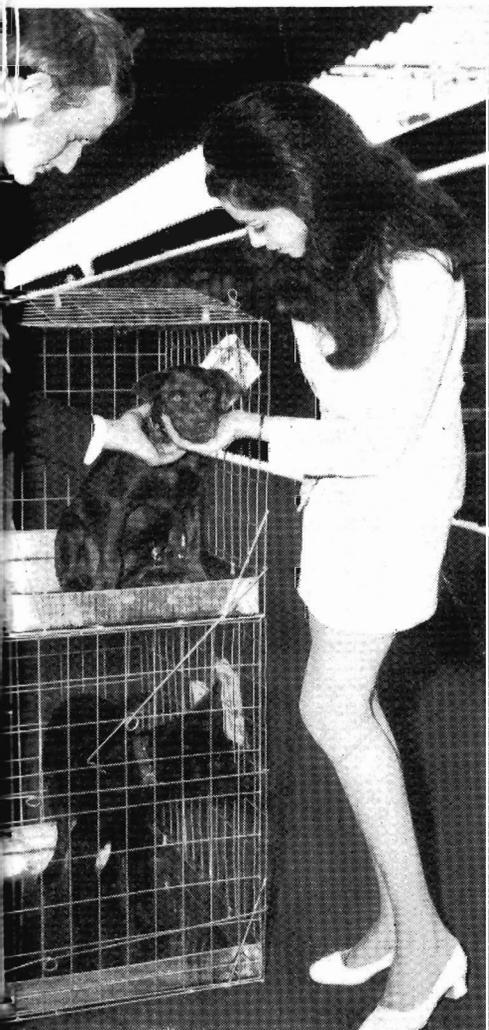
VIEWS OF NEWS

▲ **STEEL SPECIAL:** Shown near Kilmore East, this special brought a big consignment of steel from Port Kembla to Melbourne. The gross tonnage was 1157, and as some of the loading was out of gauge, speeds were restricted.

▶ **TRAINED FOR SHEEP:** Every dog has his day, and for Rocky it is a day of welcome at Spencer Street station, by Miss Lorraine Zimsen of the Accountancy Branch. Rocky and his companion Sophia (in cage below) are two trained kelpies from the Elfin Vale Kelpie Stud at Hamilton, on their way to New Zealand. At Spencer Street they were taken in the care of a representative (left) of Lap Transport Pty. Ltd. which arranged a health clearance from the Department of Agriculture and an export permit.



W.A.G.R. LOCO: On the standard gauge line, this W.A.G.R. locomotive is shown hauling a down goods train near Broadmeadows. It is the last of a batch of locomotives supplied for use on the West Australian standard gauge line.



▲ CAMERAS FOCUS ON V.R. For the business man's magazine *Fortune* (U.S.A.) interior shots of *Southern Aurora* are being taken for an article on world famous trains.

▼ After arriving by *The Overland*, this group of aboriginals are being filmed for TV. They came from the Hermannsburg Lutheran Mission, and as well as having a look at Melbourne, they sang at some concerts.



50

YEARS OF SERVICE

This is the 50th year of operation of the Refreshment Rooms by the Department; and it is nearly a half-century since the establishment of the Refreshment Services Branch.



Thirsty citizens of the 'twenties crowd the Department's first pure fruit juice drink stall at Flinders Street station. Opened in 1926, the stall sold 44,000 drinks in the first fortnight.



In 1925, this wagon was put into service for supplying refreshments through carriage windows to passengers at country stations.

Until June 1919, the Railway Refreshment Rooms had been leased to private individuals by tender. As the leases of the various rooms expired on June 30, 1919, it was decided that the Department would take over and manage the principal rooms. It was considered that this would improve both the service given to rail patrons, and also railway revenue.

Accordingly, in June 1919, the Department took over the rooms at Bendigo, Lilydale, Spencer Street, Seymour, Ararat, Ballarat, Kyneton, and Bacchus Marsh. The results were so satisfactory that the remaining rooms were taken over soon afterwards.

New Branch

In order to efficiently conduct this new business, the Refreshment Services Branch was established on March 1, 1920. The new Branch also took control of the dining car and laundry services, and the tobacco, fruit, and book stalls which were rented from the Department.

A further increase in Branch activities occurred in July 1923, when the Advertising Division was formed to

take over the letting of advertising space on railway premises. Prior to that date those advertising rights were in the hands of contractors who controlled the display of posters on suburban and some country station boards.

Changed habits

Although it is one of the smaller Branches of the Department, the Refreshment Services, by popularizing raisin bread and pure fruit juice drinks, can claim to have played a big part in changing the dietary habits of many Melbourne citizens.

A bakery was established in 1923, and among the items produced was raisin bread. It was sold on the railway stalls, and a big business was soon built up. At the start, about 150 raisin loaves a day were being baked, but by April 1924, this had risen to nearly 1,000 a day.

And, from 1925, the raisin bread was wrapped. The railway bakery thus became the first bakery in Victoria to produce fully wrapped bread.

Although private bakeries also began to bake raisin bread, there seems little doubt that railway enterprise was a major factor behind its popularity.

Children, yes, and grown-ups, too

In the 'twenties, if you bought a drink at a cafe, you were often given a dubious draught flavoured with some chemical concoction that tasted as though it had been brewed by a laboratory assistant in a fertilizer

plant. Pure fruit juice drinks were practically unknown.

This changed on November 29, 1926 when the Department's first fruit juice drink stall was opened on Flinders Street station. It was an immediate success.

"This stall is unique, there is



The Dining Section of the cafeteria at Spencer Street station is an interesting contrast with the dining room (below) at the old station.



nothing like it anywhere else in the Commonwealth", said *The Victorian Railways Magazine* of January 1927. During the first 12 days, nearly 44,000 drinks were sold. Emphasis in advertising was placed on the content of the drinks—"only pure fruit juice . . . no chemicals are added". Helping the sales was the jingle spread over the Swanston Street entrance to the station—"Children, yes, and grown-ups, too, Orange juice is good for you".

It was also good for the citrus fruit growers as well as railway revenue. The Department was then the biggest customer of the Victorian citrus grower. For the first year's operation of the fruit juice drink stall, about 30,000 cases of fruit were used.

As part of a Departmental policy to help primary producers, a V.R. stall sold dried fruit in the Government Pavilion at the 1927 Royal Show.

It was so successful that, in the following June, a stall was put in at Flinders Street station to sell raisins, sultanas, nuts, etc.

Carried over the long haul from Mildura district, citrus and dried fruit helped to swell railway revenue, and put fruit growers on their feet.

Feeding troops

The biggest job ever done by the Refreshment Services Branch was the feeding of troops during the

Second World War. As many as 2,850 meals were prepared in one day. It was not unusual for the Branch to be informed at 6 p.m. that hundreds of men would require breakfast at some remote station during the night or early next morning. At Spencer Street, a special refreshment station was set up on one of the platforms.

Altogether the Branch supplied nearly two million meals to members of the armed forces during the war.

Changes

Over the years, changes have been made in the Branch; some functions have ceased, new ones have developed.

The dining car service—it began in 1908—was controlled by the then Transportation Branch until the formation of the Refreshment Services Branch.

In the early days of the century, the first attempt to provide food on the train was that favourite device of the Victorian era—the hamper. From Refreshment Rooms, passengers could obtain a hamper containing half a chicken, ham, tongue, salad, sweets, bread, cheese, fruit, and tea, for about 30 cents.

The first dining cars were attached to the Sydney and Adelaide expresses and the Mildura trains.

In April 1937, the first buffet car was introduced, on the Melbourne-Bendigo line. It is interesting to

note that it was the first air-conditioned vehicle of its kind in the British Commonwealth. It aroused interest overseas and a Canadian railway administration asked for constructional details, later building such a carriage for their crack train.

In 1924, the Branch took over the operation of the Mt. Buffalo Chalet, and followed a policy of progressive improvement so that the Chalet soon became one of the world's leading mountain holiday venues.

A butchery has been operating since 1922. Among the meat at present distributed from it are an annual 22 tons of sausages. Fact absorbers might like to know that, laid end to end, they would reach from Melbourne to Mordialloc.

Among the food used to provide the 2½ million meals that are served each year, are 360 tons of meat, 120,000 loaves of bread, 480,000 bread rolls, 56 tons of sugar, 100,000 gallons of milk, and 120 tons of potatoes.

In addition to the activities previously mentioned, the Branch operates staff hostels and canteens, bookstalls, a poultry farm, laundry, and a hairdressing saloon.

With a background of solid achievement, the Refreshment Services Branch can confidently look forward to entering a second half-century of efficient service to the public.

TOBACCO ON THE MOVE

Last month more than 12 million lb. of tobacco leaf began moving by train from north-eastern Victoria right to the broker's store at Brooklyn for this season's auctions.

As usual, there will be three sales: June 10-27, August 5-21, and September 2-11.

The entire crop is railed, and the bales of tobacco are being consigned from Myrtleford, Ovens, Eurobin, Porepunkah, Bright, Gapsted, Wangaratta, Everton, Bowman, Rutherglen, Nathalia, and Cobram.

The Victorian sales quota this year is 10.5 million lb., but it is expected that the 1.5 million lb. over-quota leaf will eventually be sold. The value of the crop to the grower should average about \$1 per lb., or a total \$12 million.

To reduce the possibility of blue mould developing in the crop, many of the plants for this season's crop were specially grown at Nathalia, then railed to Myrtleford, the main area of the Ovens Valley tobacco growing.

Each acre of plantation has about 6,500 plants. Every plant needs individual treatment during growth,

and it is estimated that about 1,200 man-hours of labour are needed to produce one ton of tobacco.

The leaves ripen from the bottom of the plant upwards, and when picked are tied and hung on sticks. The leaves are then cured by careful regulation of heat and ventilation.

A newer type of leaf, called *Burley* tobacco, is air dried in large open sheds. This is a lower priced tobacco, and is used to blend and flavour cigarettes. Only about 100 acres of this type of tobacco are grown in Victoria.

The first tobacco planted in Victoria was in 1838, when 4½-acres were grown on land where Spencer Street station now stands.

By 1856, tobacco growing spread to the Ovens Valley, and to Ballarat by 1861. Early this century, tobacco growing was also tried at Bairnsdale, Briagolong, Pomonal, Dadswells Bridge, Horsham, Edenhope, Laanecoorie, Mildura, Shepparton, and Yarrawonga.

In 1931, Victoria had about 12,000 acres of tobacco plantations, but by 1948 there were less than 1,000 acres.

Today, Victoria has about 9,000 acres of tobacco most of which is in the Ovens Valley.



In one of the drying sheds at Buffalo River, grower Mr. R. Gigliotti inspects a stick of tobacco; it weighs 2 lb. and is worth about \$3.

LINE FROM OTHER LINES

A MOSAIC OF COMPANIES

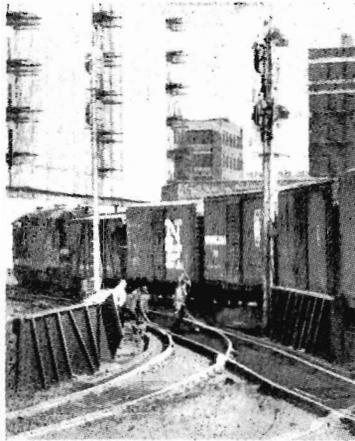
The American railway industry is made up of hundreds of companies, large and small. They are chiefly railways carrying freight and passengers between cities and towns, and companies doing switching and transfer services. At December 31, 1967, there were 76 railways designated by the Interstate Commerce Commission as Class I railways (those with annual operating revenues of \$US5 million or more), and 22 Class I switching and terminal companies. In addition, there were about 470 Class II railways and switching and terminal companies, with operating revenues under \$US 5 million.

So closely have these many railways co-operated to standardize track, equipment, and operating procedures that the nation's commerce moves from one system to another without interruption as though all railways were one. In 1967, this integrated system comprised about 211,000 miles of railways, about 30,000 locomotive units, 20,000 passenger carriages, and 1,850,000 wagons.

Railways of the U.S. also operate about 50,000 freight terminals and thousands of passenger stations. In total, the American railway industry represents an investment of more than \$35,000 million in private capital. Hundreds of thousands of Americans, including many railway employees, are railway share-holders, most of them people of modest means who own less than 100 shares.

U.S. railways pay heavy taxes to federal, state, and local governments, and in many cases are the largest single source of tax revenue. In 1966, as in many previous years, Class I railways paid over \$US 968 million in federal, state, and local taxes.

The Interstate Commerce Commission exercises regulatory control over the railways. Rates charged for transportation of freight or passengers must be filed with the ICC, and are subject to its approval. Such rates must be published and strictly observed. A railway may not build a new line or abandon an old one without ICC approval. A railway must also have the Commission's approval before it can sell securities, merge, or borrow money.



A train moves over the St. Charles Airline in U.S.A. (*Illinois Central Magazine*)

The pygmies

Most Australians look on the U.S. lines as really big business—as, indeed, the bulk of them are. But in this world of the giants, a few pygmies still survive. In fact, many of the big U.S. railways grew from the amalgamation of shorter lines. The New York Central was formed from the merging of 400 independent railways. The hundreds of smaller railways still operating, vary from less than a mile of track to about 200 miles.

Some of these small concerns rejoice in such names as the Belfast & Moosehead Lake, the Tweetsie, and the Arcade & Attica. Others are run by small operators who are real rail enthusiasts—they like rail-roading.

But apart from a few that thrive as tourist attractions (like Victoria's *Puffing Billy*), most are profitable concerns. Some make money by specializing in the carriage of bulk freight, such as grain or minerals. Others serve isolated industries that are off the main lines, or prosper because they are links between other systems.

St. Charles Air Line

Perhaps the oddest named piece of track in the U.S. is the St. Charles Air Line, which, despite its name, is a railway. Strictly speaking the St. Charles Air Line is not really a

company but is the name given to seven-tenths of a mile of track in Chicago.

Although small, it is a very important piece of track because it is the link between several systems that jointly own it.

COMMUTERS WOULD DIAL-A-BUS

Dr. Daniel Roos, assistant professor at the Massachusetts Institute of Technology, and Nigel Wilson, research assistant at the Institute, believe their Computer Aided Routing System—CARS—will be a flexible mass transportation system able to service low-density areas economically.

The system involves use of a central computer that would give dispatching instructions to a fleet of vehicles, probably mini-buses carrying about five persons each.

According to Dr. Roos, who outlined the system at a symposium in Montreal, you would slip a pre-punched dial card into your telephone, sit back and relax, and a few minutes later a mini-bus would arrive at your door to take you to the suburban railway station for the trip to work.

Riding the train home from work you would hand over a destination card, and at the station you would be allotted by computer the correct mini-bus to take you home again.

Cost to the consumer would be from 28 cents to 69 cents, depending on the number of users in a given area.

"An important consideration," Dr. Roos said, "is that this system is economically and technically possible within existing technology.

"The central computer could accumulate charges for use of the system. The users could then be billed at intervals, thus eliminating fare collection".

This would speed up collection of passengers and remove the current psychological problem of mass transit—"the continual reminder of the cost of the system, experienced whenever a fare is paid", Dr. Roos said.

(*The Australian Financial Review*)

New manager

A Scot from the high seas has been appointed manager at Mt. Buffalo Chalet. He is Mr. A. K. ("Jock") Harcus, who has taken over from Mr. Ken McIver, now back in Melbourne as an inspector with the Refreshment Services Branch.

Before joining the Railways two years ago, Mr. Harcus spent 35 years as chief steward on Orient line ships, where he controlled catering and other activities on such ships as *Orcades*, *Orsova*, *Oronsay* and *Orontes*. During the war, Mr. Harcus served in the Royal Navy for seven years.

"My first trip to Australia was in 1934", Mr. Harcus said. "On that trip, I decided that Australia was such a great country that one day I would settle here. It was 28 years before I left the high seas."

Mr. Harcus is looking forward to the snow season atop Mt. Buffalo plateau.

"They tell me that the area in winter looks like Scotland, my home country. Unfortunately, I saw little of home before I got the wanderlust, so snow-time will be exciting for me."

Startled the cows

Before Mr. and Mrs. Ken McIver left the Mt. Buffalo Chalet, they were entertained by the staff at a farewell party. And as Ken and his wife left for Melbourne, the engine room whistle signalled their departure with a prolonged blast. The whistle drifted down to the quiet valley below, from where a property owner rang the Chalet to ask what it was all about. He said his cows were grouped in circles, and staring anxiously up to the Chalet, 4,000 ft. above. Perhaps, suggested the Chalet's Frank Smyth, the cows were hoping for a new Ferdinand from outer space.

Mr. McIver had been manager since December 1964.

New style of safety shoe

A new style of safety shoe is now on issue. It is 10 oz. a pair lighter than the *Banded Derby* style it has replaced, and is fitted with the type of rubber sole used in some of the best brands of shoes on the market.

These shoes have been introduced mainly to meet the needs of staff who

are in contact with the public, and who felt that a shoe of neater appearance was required.

Watch that gold!

When Suburban Guard G.W. Capp was an operating porter—back in the 'twenties—he worked on the picturesque Moe-Walhalla narrow gauge line, and also relieved at Trentham. In those days there was a gold mine at Trentham, and the gold sent away was among the mail items handled at Trentham station. Mr. Capp says he has never forgotten the solemn injunction given to him by the S.M. when he arrived at the station. "Whatever you do, watch that gold".

Mr. Capp, who retired in April, was at Carrum from 1940.

Better to die warm

Before Yard Checker George Weston of Newport, joined the V.R. he was an engine driver with what was then known as the North Eastern Railway, in Britain. Although he drove Royal Trains and 90 m.p.h. expresses, the train he will never forget was a humble goods that he took to Hull during the Second

Dinner Dance



The fourth annual dinner dance of the Newport Workshops Foundry and Pattern Shop Social Club went with a swing. Held last month at the Footscray Town Hall, over 100 attended the very enjoyable function. Winners of the major prizes were Mrs. M. Johnson and Mr. F. Cherry (door prizes) and Miss J. Vincent and Mr. R. Bennet (Monte Carlo). Those at the table are (from left) Mr. and Mrs. W. Turner, Mrs. and Mr. H. Stone, Mrs. and Mr. A. Foreman, Mr. and Mrs. E. Reichardt, Mrs. and Mr. L. Smalley, Mrs. and Mr. J. Sharp.

World War. That happened to be the night when the big air raids on Hull began, and the Germans bombed it five nights in succession, destroying 11 miles of docks, and leaving fires that burned for weeks.

When Mr. Weston's train arrived, bombs were falling everywhere. The crew could not leave Hull for 13 hours. First, they went into an air raid shelter, but the cold was so intense that they returned and sheltered on the locomotive.

"We reckoned it was better to die warm than die cold," says Mr. Weston in his rich Yorkshire burr.

Mr. Weston joined the V.R. in 1960, and retired recently.

Another head saved

The safety hat saved a man's head from serious injury at Welshpool, recently, when a crane hook that had not been secured fell from a lifting beam on to his head. Fortunately, he was wearing the safety hat, and was thereby saved from an injury that could have caused paralysis or even death.

Worth quoting

Gaol is the appropriate penalty for youths who damage public telephones—or any public property, for that matter People responsible for damage to public property do not deserve bonds.

—Northcote "Leader-Budget"

Anzac Day at Mt. Buffalo



Probably the highest Anzac Day ceremony held in Australia was that at Mt. Buffalo Chalet, when a group assembled at Bent's Lookout, 4,500 ft. up. Among those present at this year's ceremony were Messrs. G. F. Brown (Chairman of Commissioners), F. P. Kennedy (Superintendent of Refreshment Services), A. K. Harcus (Manager, Mt. Buffalo Chalet), and their wives, together with a large number of guests and day visitors. A short address was given by Mr. Wilfred Pier, an 80-year-old veteran of the 1st. A.I.F. The Chalet's Bill Marriott is shown raising the flag.

—(Photograph; Geoff. McWigan)

Top apprentices

Apprentice Electrical Fitter Norman Swanwick (from Norlane) won the award as the top apprentice attending the V.R. Technical College, and Apprentice Coppersmith Phillip Wenn (from Heidelberg) was the outstanding apprentice in his trade in Victoria. On May 30, both



Mr. Swanwick



Mr. Wenn

The youths were presented with Railway prizes they had won, on May 26. Two days later, they were guests at a Rotary Club luncheon.

Tree Planting prizes

There will be a total of \$2,408 in prizes to be won in the 1969 competitions for tree planting and decoration of stations, locomotive and works depots, barracks, and rest houses. The results for the 1968 competitions were published in Weekly Notice No. 23.

They decorated the train



Foreman H. S. McKenzie of the Lighting and Power Division at Batman Avenue recalls that shortly after he began as an apprentice at Jolimont Workshops he was surprised to see a group of women entering the workshops. They had arrived to help in decorating the electric train for the opening of the suburban electrification scheme on May 28, 1919. All the yard lighting, in those days, he says, came from carbon arc lamps and there was an endless job for the men who looked after them, as the carbons had to be trimmed every day. Mr. McKenzie had 50 years and two months service when he retired. He is shown holding one of the set of bowls that were among the gifts presented to him at his farewell.

RETIREMENTS

ROLLING STOCK BRANCH

Andrews, C. B., Benalla 28/3
Bollman, L. Ballarat Nth, 14/7
Carroll, C. M., Ararat, 17/3
Carroll, W., South Dynon, 18/3
Chandler, G. J., Ballarat North, 27/7
Duncan, J. H., South Dynon, 25/3
Fowler, G. W., Newport, 20/7
McCann, P., Jolimont, 11/7
McDonald, J. A., South Dynon, 11/3
O'Shea, C. H., Traralgon, 17/3
Phillips, M. T., Newport, 17/7
Ruggieri, S., Jolimont, 11/7
Scholes, J. R., Bendigo North, 6/5
Smeed, T. S. B., Wodonga, 5/3
Stevenson, C., Bendigo North, 9/7
Tratt, F. L., Head Office, 13/7
White, N. D., North Melbourne, 16/5

TRAFFIC BRANCH

Broughton, A. M., C/- Metro. Supt. 4/7
Cameron, J. E., Melbourne Goods, 20/7
Carroll, H. R., Murrumbidgee, 5/5
Edwards, L. W., Maryborough, 23/4
Hartnedy, T. V., Melbourne Goods, 17/7
Hyndinan, W. G., Flinders Street, 3/7
Ireland, R. H., Flinders Street, 25/4
Jouaneau, J. L., North Melbourne, 15/7
Keck, W. A., Geelong, 30/4
Kingston, P., Melbourne Goods, 4/7
McClaghlin, V. T. L., Melbourne Goods, 2/4
Nicholson, W. M., Chiltern, 17/5
Oakley, W. R., Nunawading, 10/5
Sexton, G. J., Flinders Street, 12/7
Stewart, J. T., Wangaratta, 20/7
Watson, A. G., Leitchville, 31/5
Watts, C. C., Melbourne Goods, 21/7
Williams, L. K., Coburg, 29/7
White, T., Flinders Street, 17/5
Znaniecki, Z., Jacana, 28/4

WAY AND WORKS BRANCH

Bolger, T. P., S. & T. North Melb., 17/7
Bowyer, H. T., Head Office, 14/7
Castledine, E. M., Shepparton, 20/7
Daigtry, E., Spotswood, 1/7
Davidson, R. B. H., Mirboo North, 30/7
Denning, A. P., Cressy, 24/7
MacGuire, L. R., Laurens Street, 21/3
McCluskey, F. J., Kororoit, 25/7
McLachlan, H., Lilydale, 17/5

McPherson, E. H., (Mrs.), Taradale, 11/3
Wain, W. G., Spotswood, 11/7
Wilson, J. E., Ballarat, 31/3

STORES BRANCH

Hicks, L. A., Newport Workshops
Storehouse, 30/4
Turner, J. McC., North Melbourne
Workshops, 11/4

ELECTRICAL ENGINEERING BRANCH

Casha, A., Testing Division, 20/7
Howson, C., Distribution Division, 5/7
Whalley, S., Distribution Division, 19/7

ACCOUNTANCY BRANCH

McNamara, K. T., Head Office, 2/5

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH

Andrew, G., Newport, 5/5
Andrews, C. L., Newport, 1/5
Clapton, C. R., Nth. Melbourne, 8/4
Walker, R. S., Newport, 12/4

TRAFFIC BRANCH

Carkeek, N. C., Cudgewa, 3/5
Delmenico, H. R., Flinders St., 13/5
Ryan, J. T., Melbourne Goods, 18/5

WAY AND WORKS BRANCH

Buob, J. R., Ironworks, 19/4
Evans, H. W., Colac, 8/4
Finlay, D., Glenalbyn, 1/4
Harrison, C. E., Hamilton, 21/4
Kane R. A., C/o Foreman Plumber, 13/5
Reid, R. J., Branxholme, 30/4
Taylor, V. S., Craigieburn, 8/4
Wood, H. F., Head Office, 7/4

STORES BRANCH

Russell, E. W., Spotswood
General Storehouse, 5/4

Mileposts IN V.R. HISTORY

1860

Geelong and Melbourne Co.'s railway purchased by Government, September 3.

Melbourne and Suburban Railway Co.'s lines extended from Richmond to Pic-Nic (near Burnley), September 24, and from Cremorne to Windsor, December 22.

Melbourne and Essendon Railway Co.'s line opened, November 1.

1861

Flemington Racecourse branch line from Newmarket opened by Melbourne and Essendon Railway Co., February 28.

Government railway extended from Sunbury to Woodend, July 8; Melbourne and Suburban Railway Co.'s line extended from Pic-Nic to Hawthorn, April 13; and St. Kilda and Brighton Railway Co.'s line extended from North Brighton to Brighton Beach, December 21.

1862

Melbourne and Suburban Railway Co. sold to the Melbourne Railway Co., March 31.

Government railways opened: Geelong to Ballarat, April 11; Woodend to Kyneton, April 25; Kyneton to Bendigo, October 21.

ELECTRIC TRACTION

The 50th. anniversary, last month, of the inauguration of electric traction on Melbourne's metropolitan railway system, was an occasion for recalling some of the lesser known facts about electric traction.

The first electric locomotive was built in Scotland, in 1841, by Robert Davidson.

Davidson's locomotive was described in a newspaper report at the time as "large and massy"—it was 15 ft. long and weighed five tons—but it represented the first experiment in electric traction that had been carried out on "a grand scale".

The maximum speed of the locomotive was four miles per hour when tested on a level section of the Edinburgh-Glasgow Railway, and therefore it could not compete with steam engines.

Davidson and other early experimenters had to depend on the galvanic battery because the electric generator had not been invented.

The axles of Davidson's locomotive were encased and locked in two wooden drums on which were bolted iron bars. Large electro-magnets were arranged to attract the bars and turn the axles.

The commercial possibilities of electric traction did not, at the time, seem impressive.

The next advance was the use of fixed conductors, such as the third rail or overhead cables, for bringing current from adjacent power stations to the motors.

Electric trains undoubtedly dated from 1879, exactly 90 years ago, when a miniature electric train was one of the attractions at the Berlin Industrial Exhibition. Current was produced a short distance away by a steam-driven generator and brought to the motors through the rails. The motorman sat astride his motor as though he were riding a horse.

Four years later, a 6-mile long hydro-electric railway was built in Ireland. This was the first time that

water-power had been used for generating current for a railway.

Rapid progress followed in America and England although, generally, railway companies were slow to take advantage of the new mode of traction. Pioneering was left to the tramway systems.

Melbourne citizens of 50 years ago were highly appreciative of their electric trains, but their descendants of today have an additional reason for valuing that amenity. In an age worried by the smog and air pollution that threatens most of the big cities, nearly a quarter of a million Melbourne people are daily carried to and from their work without any increase in atmospheric pollution.

It is interesting to note that as long ago as 1837, an electric motor car was built by Thomas Davenport, an English blacksmith. And today, air poisoning by motor car fumes is forcing manufacturers to have another look at electric traction for the motor car.

VICTORIAN RAILWAYS

NEWS LETTER

JULY



1969



V.R. CHAIRMAN COMMENTS

HOW MANY COMPLAINTS ARE GENUINE

Complaints against the Railways are not uncommon in the Press. After all, the Railways, like most public utilities, are sitting ducks for anyone to shoot at.

However, all specific complaints about the Railways are carefully investigated. But not infrequently these inquiries come to a dead end, as the writer cannot be found.

A letter of complaint was published in *The Herald* of March 25 last, under the name and address of "John Barrington, Osborne St., Williamstown". In it the writer alleged that he had reported to a railwayman at Flinders Street that two girls had put their feet up on the seat of a Williamstown train and refused to remove them. The railwayman, according to the writer, then "simply smiled and walked away to the other end of the platform". The girls, the writer added, said that the Railway staff "never do anything about anything".

This complaint was, of course, treated by the Department as a serious accusation, and an investigation was immediately made. The Railway staff concerned denied that any passenger had made such a complaint.

An effort was then made to obtain the full address of the letter writer, and interview him to ascertain, if possible, the identity of

the employee to whom he had spoken.

But the writer's name and address did not appear in street or other directories; he was not known to Williamstown postal officials or police; his name was not on the electoral rolls; and there was no record of him being a periodical ticket holder.

No one, it seems, had heard of "John Barrington of Osborne St., Williamstown". He was more elusive than the Scarlet Pimpernel.

Newspapers do not disclose the address of letter writers, but a representative of *The Herald* advised that they would forward a letter to Mr. Barrington. The letter, asking Mr. Barrington if a railway officer could interview him, was accordingly sent. At the time of writing, two months have elapsed but no reply had been received.

This leads one to ask just how many of these complaints are really genuine, for we all know that by far the majority of railwaymen are doing an excellent job, and are a credit to the community.

We all know, too, that there are some few who do not pull their weight, and thereby can wreck the good image of railwaymen in general. This should not happen, and won't, if all our staff do their utmost to be helpful to our customers and each other.

Wise dog

Basset:—Lost. Last seen boarding city bound train from Moorabbin. Reward. 97 xxxx

A reader sent in a cutting of the the above *Lost and Found* advertisement from *The Sun*. He pointed out that even the dogs have now got the railway message about the safe and reliable train service. (Railway advertising staff are, of course, extremely gratified to know they have readers among good, middle class dogs; but—let's face it—rail revenue can hardly afford free rides to every Tom, Dog, and Harry!—Ed.)

Beans, bananas, tomatoes

More than 5½-million lb. of Queensland beans are expected to be bailed to Melbourne between June and November, and big consignments of tomatoes from three states are also arriving by train for Melbourne's market.

Beans are from the Gympie district and Brisbane, and tomatoes from Geraldton (W.A.), Port Pirie, Adelaide, and Murray Bridge (S.A.), and Bundaberg (Queensland). They are railed in ventilated louvre bogie vans.

Big banana railings also began last month when virtual train loads of bananas reached Melbourne from Queensland and northern New South Wales. However, this year's banana crop will be lower than normal because of unfavourable weather conditions in growing areas.

More light

By the end of this year, all 189 metropolitan railway stations should be fitted with fluorescent lighting. The conversion programme, started in May 1966, is now more than 75 per cent complete, with 144 stations brightly lit with modern lighting.

The old-type lighting being replaced, in most cases, was put in about 1919 with the introduction of electric traction.

Fluorescent lighting was first installed on railway stations in 1960, when use was made of commercially

available fittings of the twin 20-watt street lantern type. These proved unsuitable because the lamps were directly visible to train crews and caused interference with the viewing of signals.

A deeply shaded fitting was then tested, but because it had too sharp a cut off, the lights had to be more closely spaced to avoid producing pools of light divided by dark areas.

It was not until 1963 that railway engineers designed a special fitting, using two 20-watt tubes, that was found for railway stations.

FRONT COVER

FOUR V.R. MAYORS: (*From left*) At the Essendon mayoral ball, the Mayor of Essendon (Cr. N. Sinclair) chats with the mayors of Fitzroy (Cr. L. J. Martin), Port Melbourne (Cr. R. F. Julier), and Footscray (Cr. R. Bassett). All four are railwaymen; Mr. Sinclair works at Spotswood Workshops Storehouse, Mr. Martin at Melbourne Goods, and Messrs. Julier and Bassett at North Melbourne Workshops and Newport Workshops respectively.

U.S.A. and freeways

"The Herald (31.5.69) contained two rather conflicting articles. On page 15 was 'Melbourne will need 300 miles of freeways costing thousands of millions of dollars'. On page 17 appeared four pictures of small cars being developed by General Motors to replace the conventional family car in case congestion and air pollution problems force it off the roads in city areas.

"Having just returned from the U.S.A. and discussed air pollution with a number of people in many cities, there seems no doubt that exhaust fumes from motor vehicles are by far and away the prime cause, and in spite of every endeavour to neutralise the fumes no device has yet been successful.

"In some centres such as Los Angeles, where I experienced two weeks of constant smog, there is talk of closing or even destroying the freeways and banning motor vehicles from entering the city area.

"Should we not heed the advice of experts and research workers on air pollution before we spend money endlessly ?

"I believe one of the world's acknowledged experts and research workers on air pollution is Dr. Werner Strauss at Melbourne University, under whom is at least one well-qualified American research worker, of Columbia University, who has come specially to Melbourne to take his Ph.D. on air pollution. We have fame in our midst and presumably are not taking advantage of it.

"I am convinced that if we allow urban centres to exceed 1½ million then motor vehicles will be banned within 10 years.

"Why plan to spend thousands of millions before we gather all the facts and study the trends in overseas centres ?"

—Letter in "The Herald" of 11.6.69, from (Sir) L. J. Hartnett, Mt. Eliza.

Freeways "wreck cities"

The architects of the second-biggest building in the world (the Hancock Centre in Chicago) were in Melbourne recently, and said some interesting things about the local traffic problems. The architects were Mr. Bruce Graham and Dr. Fazlur Kahn, of the firm Skidmore, Owings and Merrill. Keith Dunstan (*The Sun*, 29.5.69) wrote "These men were surprised to read of our plans for the Eastern Freeway. They think we are about to make all the horrid mistakes that were made in the U.S. 15 years ago when super-highways were allowed to wreck cities without any regard for human values".

Virus-free from Toolangi

A generation ago, Toolangi (in the Healesville district) achieved some fame through one of its residents—C. J. Dennis, author of *The Sentimental Bloke* and many other works. Today, Toolangi is known in many parts of Australia through the strawberry plant.

Because the strawberry industry faced disaster from virus disease about 10 years ago, the Victorian Department of Agriculture set up a plant growing area in 1962. Toolangi was chosen because its height (2,300 ft.) provided a natural barrier to the virus, and the cold climate stimulated buds.

Today, Toolangi is the only certified virus- and disease-free strawberry plant growing area in Australia, and the plants are in demand interstate as well as locally.

Each year, Toolangi growers are supplied with mother stock that is planted at the rate of about 2,000 plants to the acre in plots of six or 12. Each parent plant produces about 70 runner plants in 9–10 months. The growers are under strict supervision and each plant is inspected at least three times during growth.

Toolangi's 20 growers produce six varieties of strawberry plants on about 60 acres. More than 60 per cent are *Red Gauntlet*, which crops the best. The other types are *Cambridge Vigour*, *Talisman*, *Shasta*, *Torrey*, and *Red Rich*.

Runner growers are not permitted

to allow plants to fruit at Toolangi, and flowers are immediately plucked, because of the risk of birds taking strawberries and dropping seeds, which in turn could re-introduce virus. Besides, without fruit, greater plant growth is induced.

The railing of 2–3 million strawberry runner plants, grown at Toolangi, began last month and has just finished. Plants were sent, almost daily, from Healesville to railway stations throughout Victoria, and to W.A., S.A., N.S.W. and Queensland.

In the interests of the industry, the Railways charge only half normal parcels rate for railing strawberry plants.

Before the Toolangi scheme was introduced strawberry growers were fortunate to crop 1½ tons to the acre. Today, they average seven tons to the acre, with some up to 15 tons.

The Secretary of the Toolangi Certified Strawberry Runner Growers' Association (Mr. Jim Hodge) said: "Rail service is chosen not only because of the lower rates, but also for quick transport. For example, plants leaving Healesville at 11.30 a.m. are in Adelaide, 522 miles away, by early next morning."

Asked for the ideal strawberry for the Victorian home gardener, Mr. Hodge suggested *Cambridge Vigour* because of its moderately sweet flavour, or *Red Gauntlet*.

"But, *Red Rich*, that has blood-red fruit, is best for jam makers," Mr. Hodge added.



Assistant Stationmaster T. L. Thompson loads boxes of strawberry plants into the rail car at Healesville.

NEW STATION BUILDING AT NUMURKAH



Numurkah opening.



Mr. Wilcox officially opens the barrier gate to the new station building. At right is Mr. Brown.

On May 29, the new station building at Numurkah was officially opened by the Minister of Transport, The Hon. Vernon F. Wilcox, M.P.

"Time and again, when there is a real emergency, it is the railway that is turned to by the community," said Mr. Wilcox, who urged the business people of the district—"in a 'border hopping' area"—to think before turning from rail to road. "It's necessary to look a little beyond our noses," he said.

With 32 employees drawing over \$100,000 in wages each year, the railways' district contribution was not something "that you can lightly snap your fingers at". Mr. Wilcox added that although the occasion was very much a railway one, there was a place in a modern community for every form of transport.

Mr. G. F. Brown, Chairman of Commissioners, said the railways were out to give a service, and out to get and hold business.

The Leader of the Country Party (and local Parliamentary representative), the Hon. G. C. Moss, remarked that the old station had served the people for 80 years. The new modern building would cater for Numurkah for a very long time, and would not interfere with a standard gauge line that he hoped would run from Tocumwal to Seymour.

The new building, of light toned bricks, covers 3,000 sq. ft., and has wide eaves, a fully insulated steel roof and a meal room for drivers and guards. The construction was done in stages to avoid interruption to the normal work of the station. (See *News Letter*, January 1968, page 10)

LOST PROPERTY AT NUMURKAH

Numurkah is a station that has retained its original Lost Property register, and the Numurkah Historical Society has delved into the old record, and produced some interesting facts.

Numurkah line was opened on September 6, 1881, but it was not until April 9, 1884, that the first item of lost property was found. It was a gun case found on the platform, and duly returned to William T. Russell eight days later. Only four other entries appear for the rest of that year.

But in the later years came shawls, bags, books, boxes, walking canes, hats, coats, jackets, umbrellas and scarves. Swags, billies, saddles and whips appeared often. And—foreshadowing Davis Cups of the future—two “lawn tennis bats” were found on 18.11.1890.

Boots appeared frequently. Did the passenger take boots off when travelling and walk off without them, or did they carry their spare boots in their hands as luggage?

A drum was lost on 2.2.1897, a dressing chart (what on earth is that?) on 4.2.1897 and a dog on 12.2.1897.

Gentlemen lost three umbrellas and ladies lost eight between June 1899 and September 1901. This was about average.

The pair of fox skins left on the platform on 28.1.1901 were never

claimed, nor was the golf stick left in a second-class carriage on 7.10.1902.

One pair of corsets was found on the platform on 23.10.1895 and another in 1964. What mystery lies behind these entries? And the pair of socks found in a carriage 16.10.1896, the pair of trousers on the platform 9.1.1897, and the pair of pyjamas on 1.8.1927?

One “Squirrel” was found in the Cobram train on 16.1.1896. Was it live, or a fur?

A book entitled “Unravelling Knots” was found on the platform on 15.5.1933 but the railway staff could not unravel this knot and it was duly sent to the Lost Property Office at Seymour.

People were more careful during the First World War. Only 20 items were lost from 1914 to 1918. Only two entries concerning soldiers’ effects appear (1918). During the Second World War a record number of items was lost including many by soldiers.

The coming of the car was shown by a motor coat lost on 30.12.1918. Of course it was in a first-class compartment.

The first camera appears in 1925 and the first fountain pen in 1939. A revolver was left in the train on 24.2.1919.

A bottle of gin was found on the platform on 10.11.1927. Nine days after a bottom set of false teeth was found but never claimed.

How did the passenger who left the crutches in 1963 get off the train?

Tape measures, fishing rods, bike pumps, badges, dolls, dog-collars, diamond rings, biscuits, a plate, pipe joints, a “wire gadget”, a cup and saucer engraved “Dad”, a spirit level, a driver’s licence, ballet shoes, two roasting forks, a light shade and bamboo rings complete a list showing the endless variety of articles carried (and forgotten) by feckless travellers.

PERTH SERVICE

Journey time by passenger train to and from Perth was cut by up to four hours from June 13. Passengers who left Melbourne for Perth by *The Overland* on June 12, travelled on the first run of the through standard gauge passenger train service in Western Australia.

The luxuriously appointed and fully air-conditioned *Trans-Australian Express* now runs from Port Pirie in South Australia through to Perth, eliminating the change of trains at Kalgoorlie.

Passengers from Melbourne now arrive in Perth at 7 a.m. on the third morning, four hours earlier. From Perth the train now departs at 9.30 p.m. instead of 6.20 p.m.

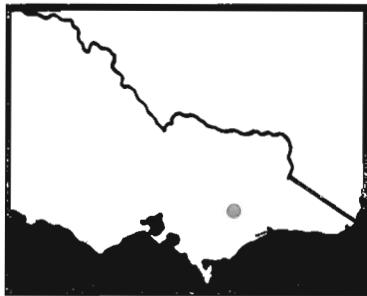
Freight trains have been operating through from Port Pirie to Perth since November last, and elimination of the transhipping at Kalgoorlie has reduced transit times by 24 hours.

RICHMOND 1882

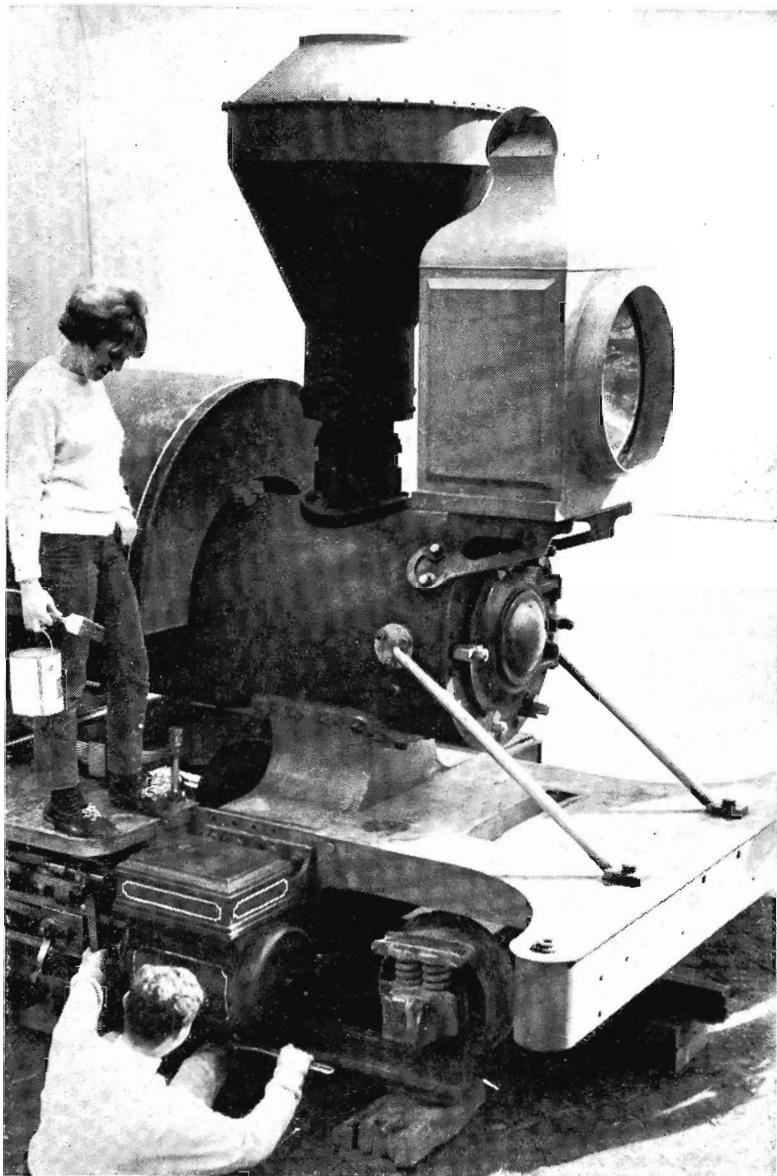


Picture shows the signal box at Punt Road, Richmond, as it appeared on October 1, 1882, the date the box was opened. The area at right became the Richmond Cricket Ground.

THE W. & T. R. S. T.



It is just over 25 years since the first closure of a section of V.R. narrow gauge line. That was the Platina-Walhalla section of the Moe-Walhalla line. It now appears that, owing to the enthusiasm of Mr. Ron Kain (a former V.R. electric train driver) and a group of volunteers, scenic Walhalla may again resound to the puffing and whistling of a steam train.



Volunteers Nancy Steain and John Robin are at work on the W. & T. R. S. T. locomotive.

The story began when Mr. Kain found a derelict locomotive at The Basin, a township in the Dandenong Ranges. The locomotive had originally hauled coal from the wharves to the West Melbourne gas works, and had spent its life on an elevated track 30 ft. above the ground.

A friend suggested that the derelict could be restored and put in working order. However, an inspection showed so many alterations were required that it was decided to redesign it as a 2-4-2 instead of an 0-4-0.

The locomotive was bought and removed to Mr. Kain's home in Belgrave.

The new design provided for a locomotive twice as long as the original, with new frame, longer boiler, and an entirely new superstructure. In fact, with the exception of the wheels, axles, and motion gear, it will be virtually a new engine. In appearance it will be like an early American western loco, with polished silky oak panelled cab, diamond stack, giant headlight, and, of course the conventional wooden-spoked cow catcher. It will be painted in colours appropriate to its period—about 1880.

"I realized that to do all this, it was necessary to drop social life, make many sacrifices, and work on it constantly when not eating, sleeping or earning one's crust," said Ron.

Where would it run ?

At this stage, Ron had not the slightest idea where the engine would run or what it would pull. But a visit to Walhalla soon settled that. The wonderful mountain scenery would be an ideal place for a colourful little train, running on a track 200 ft. below the road or over trestle bridges, 50 ft. above Stringers Creek.

And around would be the gorge with its sheer mountain sides rising many hundreds of feet above the railway. The gorge itself is, of course, wonderfully scenic at any time of the year. Late winter brings the wattle blossom, then the pink

heath and masses of purple sarsaparilla, and later comes marvellous autumn colour from the trees brought by the early gold miners. And the gorge would give interesting sounds from the exhaust blast of the engine. Most people, especially children, expect a steam locomotive to puff. Working fully loaded up the grade the engine would probably be doing only about 5 or 6 m.p.h., and the blast reverberating from the rock walls would really tell of hard work. And a full-sized whistle should give multiple echoes from one end of the gorge to the other.

The eyes and ears of even the most demanding railfan should be completely satisfied.

Improvising

Joined by a group of enthusiasts, Ron began the herculean task of rebuilding the engine. They had little equipment at first, but soon became masters at improvising. They learnt quite a lot about the old engineering methods and the metals that were then used. Greatly different from the mild steel of today, the iron plate in the loco was quite weak and could not bend without breaking. It could not be cut successfully with oxy-acetylene, and contained so many impurities that it quickly ruined the best hacksaw blades and files. However, it withstood rust, and provided that the sections were heavy enough to withstand the loads it would apparently have a long life.

Some problems

More volunteers joined in, until up to 50 people were working on the project at various times. Some put in only a few hours but others managed to give over 1,000 hours. At first, heartbreaking difficulties were encountered due to the lack of proper tools. The lathe was not large enough to machine the four wheels, so a makeshift extension had to be added. A device had to be made for rolling up the funnel cones. As there are no 240 volt welding plants made that were adequate for the heavy welding needed, they had to make a 2-cwt. transformer, cutting each lamination by hand.

One major problem was to find a firm that would make up a new boiler barrel. A firm at Reservoir, George & George Pty. Ltd., kindly offered to make a boiler at cost price, as they were quite taken up with the project. They also donated all the material for the job. The next thing was to have the boiler riveted to the fire-box. "This", said Ron, "was also a problem, as riveting in Victoria had almost become a lost art, having been replaced by welding."

"Through the generosity of Mr. Ron Bass (of G. T. Laird & Co. Pty.



Painting the first of the W. & T.R.S.T. vehicles, a 25-ft.-long works wagon. Ultimately it will be fitted with padded bench seats and used as an open carriage on fine days. (From left) Nancy Steain, Vincent Bosua, and John Robin.

Ltd.) our riveting job was done free," he said.

To provide compressed air for the project, an air plant using a reconditioned compressor from one of the old V.R. Elwood trams, was built. It was driven by a car engine, and the air receiver was the main reservoir from an R class steam locomotive.

Improvisation was, however, a feature of the early stages only. There are now adequate tools and a well equipped workshop.

Hot dog

Of course, there was the unexpected incident, now and then. On one occasion, a chuck key fell into the welding plant, and accidentally connected the primary and secondary winding. The welder still worked, but put a potential of 240 volts on the engine frame that Ron was welding.

"Though in rubber boots at the time, I thought it felt a bit vicious for a welding voltage", said Ron. "The question was settled by the family dog (who wasn't wearing rubber boots) when he sidled up to show his affection for me, and placed his two front paws on the engine frame. Pandemonium broke out, and the last view of the dog was a black and white streak on the horizon. He wasn't seen again for a week.

On another day, one of the helpers made a paint stirrer to use in the electric drill. All went well until he pulled it out of the paint while it was still spinning and covered everyone (including visitors) with orange paint. Fortunately, the sense of humour of the team was such

that it could quite easily cope with these little contretemps and turn them into pleasantries.

Local support

Councillors and local people at Walhalla became interested and gave strong support to the project. The Lands Department has made Crown land available to operate a tramway, Mr. Kain said.

Rolling stock and equipment

An old 2 ft. 6 in. gauge goods wagon was found on a property at Emerald, and permission obtained to remove it. Brought to Belgrave and reconditioned, it has a strong frame, an oregon floor, and polished wooden sides and ends (see picture). This became the first vehicle for the tramway. Ultimately it will be fitted with padded bench seats and used as an open carriage on fine days. From the Erica Mill, near Walhalla, 10 narrow gauge wagons, and about 800 ft. of 60-lb. rail and points were obtained.

The biggest problem at present is where to get rails. The Australian Portland Cement Ltd. made available about half a mile of 45-lb. rail at a price the group could afford. Together with the rail from the Erica Mill, it will provide enough track for a while, but the group is very anxious to get some more—at a price they can afford.

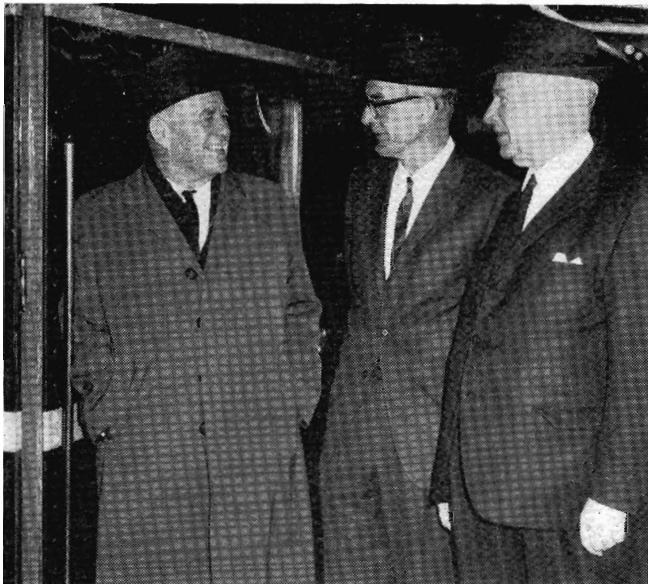
At Walhalla

Each week-end, a group of helpers work at Walhalla on the track and structures. At present, a train shed and workshop has been completed, excavating has been done for the track to the train shed, and about 100 ft. of track to the shed laid.

The platform will be 125 ft. long, with a pinewood waiting room, milk bar, gas platform lighting, and log fencing. Other structures will comprise a locomotive shed, coal stage, water tank, and—high up on a rock shelf above the station—a caretaker's residence which will also house the week-end workers.

"As soon as sufficient track is laid, passengers will be carried, and the Walhalla and Thompson River Steam Tramway will be in business. As the track laying progresses, the rides will become longer until the Thompson River is crossed. On that day it is planned to have an official opening, and the Tramway will be one of the Latrobe Valley's major tourist attractions," said Ron.

Visitors are welcome to look over the project at Walhalla, providing they don't mind if the group keeps on working. Time is one of the scarcest commodities. And the visitors will be particularly welcome if they know where any 45-lb. or 60-lb. rail can be obtained cheaply.



TECHNICAL EDUCATION: Last month, the Victorian Advisory Council on Technical Education met for the first time outside Melbourne, when 11 members travelled by special train to Shepparton. Shown at Spencer Street before boarding the train are (from left): Messrs. G.F. Brown, (Chairman of Commissioners) who is also Chairman of the Council, E.T. Jackson, Director of Technical Education, and A.C. Eldridge, President of the Apprenticeship Commission.



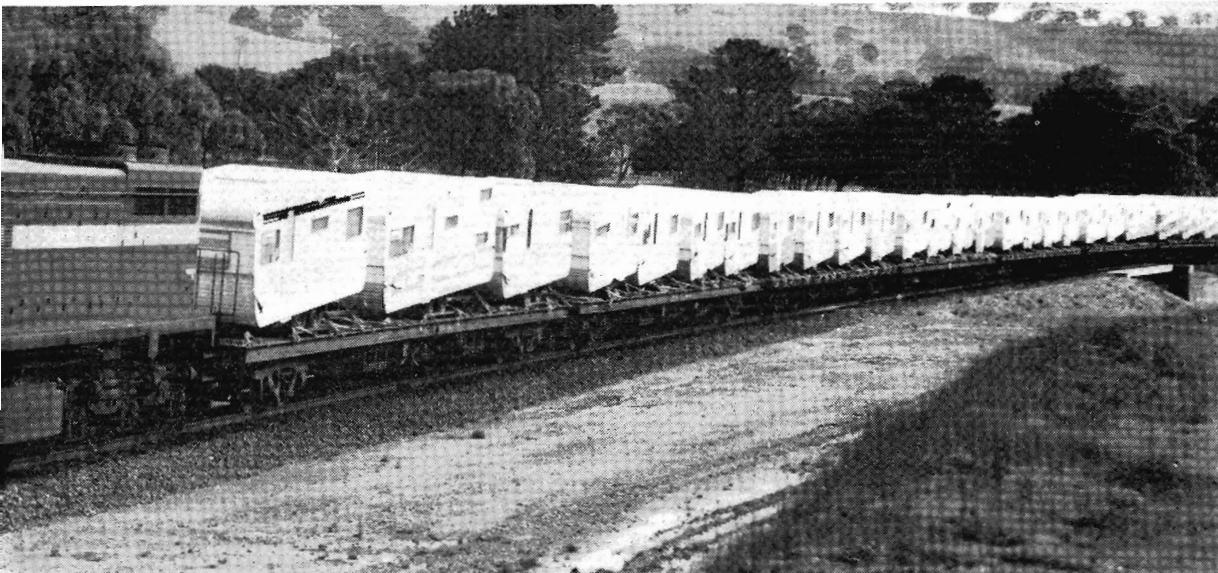
CARAVAN TRAIN: Aus

VIEWS OF NEWS

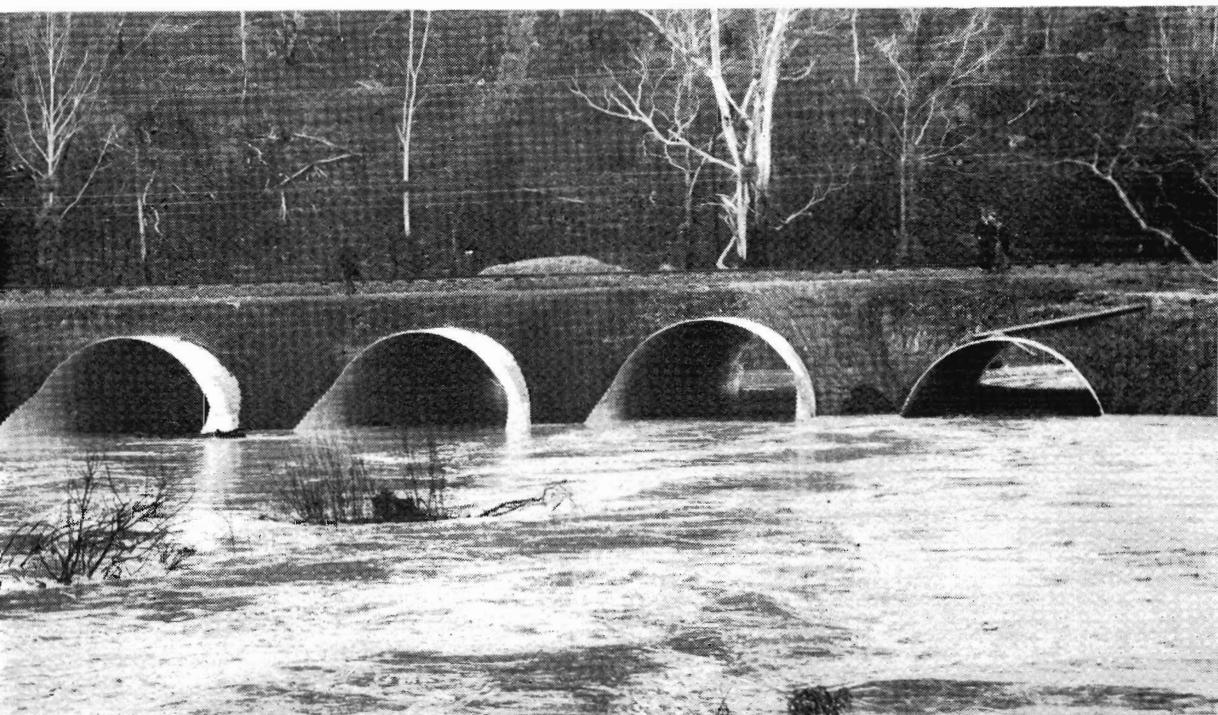


FLOOD: One of the Hurst
This

MODELLING MODELS: Re
admire scale models of conf
at the Internatio



...a's first train-load of caravans is shown near Kilmore East on its way to Perth from Sydney. There were 63 Millard caravans on 19 flat-top rail wagons.



...ge line bridges that has been built from corrugated steel pipes, is shown coping with a large volume of water. Contrasts with the picture of the same bridge shown on page 68 of May 1969 *News Letter*.

...ay typistes Mrs. Ivy Alexander from the Rolling Stock Branch (left) and Miss Anita Pettit (Traffic Branch) ...er wagons loaded with miniature Wridgways, A.C.T., and O.C.L. containers that were used for the first time Container Exhibition in Melbourne, last month. These wagons will also run at this year's Royal Show.

INES FROM OTHER LINES

Rail-road bus for N.S.W.



This NSWGR rail-road bus has seating for 22 passengers and can carry up to three tons of luggage and parcels.

A bus that can travel on both rail and road will soon go into service with the N.S.W. Railways.

The bus, which is the first of its kind in Australia, was the idea of the N.S.W. Commissioner for Railways, Mr. N. McCusker. A Comeng subsidiary, Aresco Trak Chief Pty. Ltd., of Adelaide, built the bus to Mr. McCusker's requirements.

The bus will be used for cross-country work, in areas of light traffic and can carry 22 passengers and their luggage, as well as parcels.

At time of writing consideration was being given to its use on the Moree-Mungindi-Boggabilla or Narrabri-Walgett-Pokataroo lines, which are cross-country lines of light traffic density.

The rail-road bus, valued at about \$40,000, is being thoroughly tested and if successful under actual operating conditions, as anticipated, additional units of similar design will be ordered.

The exterior of the coach is finished in aluminium, with blue and off-white trim. A pleasing colour scheme has been adopted inside and such features as heat-resistant glass, adjustable armchair-type seats, and jet-air ventilation will provide comfortable travelling conditions for passengers. The unit is 26 ft. 8 in. long and has a loaded weight of 8-10 tons.

A 6-cylinder diesel engine operating through a conventional gearbox gives it a cruising speed of 50 m.p.h. on either road or rail. On

the road the vehicle runs on six rubber-tyred wheels.

The two front wheels are used for steering, and the two rear wheels drive the vehicle. The remaining two rear wheels, supported on a trailing axle, are mainly for weight distribution.

The vehicle can be transposed to either rail or road operation in a matter of minutes. It is driven across the tracks and an in-built hydraulic jack raises and positions it for readjustment of the wheel arrangement. Two sets of flanged rail wheels, front and rear, are lowered to the tracks and the front rubber-tyred wheels are raised clear. The rear rubber-tyred wheels drive the bus on rails as well as on the road and the four rubber-tyred wheels left on the rails are used for braking.

The hydraulic jacking system enables the vehicle to be turned through 180 degrees, if necessary, to reverse its direction.

Self-powered wagons

An automatic railroad using self-powered wagons, has been described by Mr. S. W. Freeman, Vice-president, engineering, of America's General Railway Signal Co., as a possibility by 1980. He predicted that the automatic wagon would be built like a highway trailer. Its structural strength needs only to be sufficient to carry its loading, control equipment and a small power plant, somewhat less than 20 h.p. Transmission would be automatic.

Each wagon would have compact, solid-state equipment to receive and implement controls and to sense and transmit information. A man at the consignor's plant would, by operating push buttons, move wagon from the loading dock to a section of pick up track where the railway would take over control of the car.

By sending supervisory controls to this track section, the wagon would be started on its journey to the consignee's factory. As the wagon passed designated check points it would transmit its identity and its location to a central computer, thus triggering the computer to transmit proper routing controls.

These controls could be transmitted to the wagon inductively by wayside loops or superimposed on high-frequency track circuits. Such circuits, which do not require insulated joints, would provide proper spacing between these self-propelled wagons. These wagons would have horns that could be sounded at level crossings. Commands would be sent to the wagon via wayside loops near the crossings. At the consignee's drop off point, remote manual control would locate the wagon.

"In operational advantages, this automatic railroad is much like a pipeline, or continuous conveyor belt system, with appropriate switching facilities. It offers prompt service to consignors, extremely low labour costs per ton mile, elimination of all classification yard and interchange delays," says Mr. Freeman.

(*Railway Age*)

Overhead circus

If you have travelled the great railway that links Moscow with Vladivostock, you have noticed in the course of the 7,000-mile journey, occasional groups of men working on the overhead electric power lines. These men are rejects from the great circus schools of Russia. To be a circus performer in Russia, you have to be good, but if you don't quite make the grade, this is one of the jobs you are offered. Your audience may be only a passing train load of passengers, but you are among the most respected workers on the country.

—(Graham Reed in "The Western Australian Railways Institute Magazine," March 1969).

Although its warmest admirer would hardly commend the *Weekly Notice* for its glamour, that very useful publication could probably claim to be more closely scrutinized than *Life*—or even *Playboy*. Packed with information essential to railway working, and items of personal interest to the staff, the W.N. has a secure hold on its readers that could well be envied by the editors of those more colourful journals.

From the earliest days, it had been the practice of the Department to issue instructions to the staff by what became known as "A" and "C" circulars. Some of the more important circulars were collected and published in book form, but those of more local interest were kept in the memorandum files. As these have disappeared, it is difficult to discover what went on in the early days.

No. 1 *Weekly Notice* has on its first page a general notice that "the system hitherto used in the issue of circulars will cease, and all instructions, except those relating to train movements, live-stock traffic, and other urgent matters will be communicated to the staff by the pamphlet to be known as the *Weekly Notice*".

The excitement of the new issue, was, apparently, too much for the printer, who numbered two clauses 13 and left 14 out.

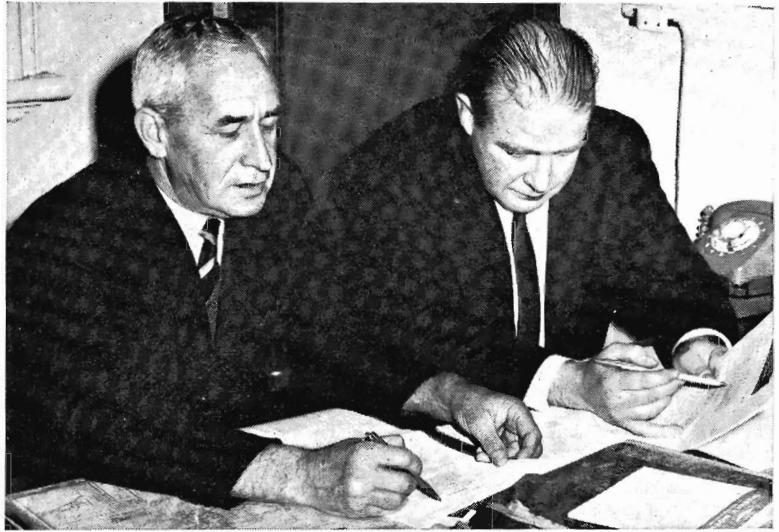
One of the earliest orders issued in the new publication admonished drivers not to start or move trains without first receiving a signal from their guard. Apparently the communication between driver and guard, in those days, was not all that it should have been.

Although the W.N. was strictly confined to railway matters, its issues, nevertheless, throw sidelights on the history of their periods.

The contents of No. 4 reflect the depressed economic conditions of the times. The land boom had run its course, work was scarce, and cuts were made in Government expenditure. Percentage deductions from railwaymen's salaries, ranging from 7 per cent for the \$200 to \$300 section to 25 per cent on those receiving \$2,000 a year were listed. Station staffs were drastically reduced. The S.M. at Scarsdale, for example, was provided with a tricycle so that he could control Smythesdale, Cardigan, Trunk Lead, Haddon, and Nintingbool.

Excursions

Tourism was evidently flourishing



Mr. A. V. Ward (left), O-in-C of Room 55, checks *Weekly Notice* proofs with Mr. S. Thomas.

in the nineteenth century. Cook's excursions to Adelaide or Mount Gambier at \$4 first-class, and \$2 second-class, and to Sydney for \$6 and \$4, all available for a period of one month, were included in *Weekly Notice* items. As a special concession to gold miners, tickets from Maryborough to Ballarat cost 30 cents, while Ballarat to Ararat were slightly dearer at 48 cents.

Among some of the older generation, pleasant memories will be revived by *Weekly Notice* instructions stating that tickets were to be issued to city holiday-makers who intended to spend the week-end on a round trip from Melbourne to Queenscliff. For 75 cents, the traveller had the privilege of making the round trip in either direction by rail or boat, the old paddle steamers *Hygeia* and *Ozone* supplying the necessary sea transport.

Other famous old ships mentioned in an early *Weekly Notice* were the *Gem* and *The Queen*. They ran the ferry service between Railway Pier, Port Melbourne, and Williamstown. Free travel on them was granted to holders of periodical railway tickets or gold passes.

A sign that times were changing and new ideas were being introduced was indicated by an instruction that permission had been granted for stationmasters' residences to be equipped with gas lamps instead of kerosene lamps.

1914

The outbreak of the First World War, early in August 1914, was almost immediately reflected in the issues of the W.N. for that month. The *Weekly Notice* of August 17 had a paragraph on travelling warrants for members of the Imperial Army Reservists. The issue for the following week had several items revealing the impact of the far-off events in Europe that were soon to engulf what was then a distant colony of the British Empire. Paragraph 18 warned that "owing to the war there may be difficulty in obtaining supplies of the stores and materials that are essential for the working and maintenance of the Railways . . . stocks should therefore be husbanded as much as possible . . ."

Another item directed that owing to the uncertainty of supplies of gas mantles, "every mantle must be kept in use as long as possible". Paragraph 21 stated that permission had been granted for the exhibition on railway stations of posters calling for volunteers for military service. The next paragraph advised of vacancies for tradesmen in the Expeditionary Force that was already being raised. Finally, a message from the Commissioners dealt with the issue of subscription lists for a "Patriotic Fund in connection with the War in Europe".

The editing of the *Weekly Notice* is done by the Chief Traffic Manager's staff in Room 55 at Head Office. (*To be continued.*)

RAIL USERS SAY...

Spencer Street

... Would you please convey our gratitude to the person who was honest enough to hand in our duffle bag left on platform 2 (Stock Assistant L. Violic of Spencer Street Cloak Room—Ed.)

—Mrs. Cooper, Omar Street, South Caulfield, writing to the Stationmaster, Spencer Street

Lost property

... I would like to thank the staff at Richmond, Ferntree Gully, and South Yarra for their help in returning my handbag that I inadvertently left on the platform at Ferntree Gully.

—(Mrs.) Ruth W. Adeney, Hutton Avenue, Ferntree Gully.

S.O.P.

I went to Sydney on my own, on a blind pass by *Spirit of Progress*, and returned by *Intercapital Daylight*. I wish to return thanks for the wonderful attention I received from all your staff, especially the hostess. I received remarkable attention, both going and coming, they could not do enough for me, and I valued their attention as I could not get anyone to go with me. They would not take a tip.

—N. H. Madden, Murray Drive, Burwood, writing to the Secretary.

Ballarat, Maryborough

As district secretary for a Somers Camp for 160 boys from Maryborough Inspectorate, April 14-23, I extend to you and the Railways officials concerned my sincere thanks and congratulations on the wonderful co-operation, courtesy and friendliness accorded us on the various lines involved. The eight teachers who accompanied me were amazed that such a large movement could be accomplished so smoothly.

I feel that I must specially mention staff members at Ballarat where so much luggage had to be handled on each journey.

At Maryborough, Mr. Rogan, S.M., was most helpful concerning tickets and in arranging to have a rail motor ready to join the Donald-Ballarat train on the day of departure.

—Wm. C. Richardson, Maryborough State School, writing to the Chief Traffic Manager.

Bendigo train

As I am moving interstate after six-and-a-half years of travelling to work on the Bendigo line, I felt a final letter was in order. This time, instead of criticism, I want to

draw your attention to the splendid job your conductors do on the Bendigo train . . .

—(Mrs.) Joan N. Hall, Pohlman, St., Kyneton, writing to the Commissioners.

Rail link to docks

Recently this Division of our Company was involved with your organization in the establishment of a rail link to the new container docks.

We should like to extend our congratulations on the speed, obvious efficiency, and skill displayed in this work, and our warm appreciation to the officers concerned and the Department for the way they co-operated in ensuring the absolute minimum of interruption to our business. The end result is a credit to your Department; the overall appearance excellent, all in all we are pleased with your attitude to, and understanding of our needs.

—The Manager, Timber Division, Gibbs Bright & Co. Pty. Ltd., writing to the Commissioner.

Ballarat

On behalf of the Girl Guides and their leaders who travelled from Ballarat to Melbourne, I would like to express our thanks to you and your staff for their help and courtesy . . .

—(Mrs.) A. S. Reiher, District Commissioner for Girl Guides (Camberwell-Hartwell), writing to the Stationmaster, Ballarat

Lost Property Office and Darebin

About two weeks ago my son left his school cap in the train from Darebin to Melbourne. My husband made two phone calls to your Lost Property Office but it was not there. A few days later I got a 'phone call from them to say they had it ('phone number was inside) and when I said it was difficult for me to get in, they sent it to Preston station.

On Friday last my boy left a valuable trumpet in the waiting room at Darebin, I rang the station and the man on duty looked for it, found it, and we were able to pick it up that evening.

We met with the greatest courtesy and helpfulness on both occasions. In these days it is rare to get either and we would like to commend the men concerned, and all without any mention of reward.

—(Mrs.) E. Hill, Fitzroy St., Preston writing to the Commissioners.

Daylesford rail car

My family and I have travelled on the small Daylesford train on many occasions and wish to bring to your attention the friendly and courteous conduct of the gentlemen who man this train. They are a credit to the service.

—William Lamb, Clifton Street, Richmond, writing to the Secretary.

BUS-TRAIN TICKETS

A large scale test of public reaction to combined bus-train weekly tickets to Melbourne is being made on four suburban bus routes. The test began on Monday, July 7. If successful, these combined weeklies will be introduced on other bus lines running to railway stations.

The bus routes on which the tests are being made are to :

- Croydon station from North Croydon,
- Glen Iris station from Huntingdale Road, Ashwood, via High Street Road,
- Newport station from Altona North, on two routes—via Blackshaws Road and via Mason Street.

This co-operation between the private bus operators and the Department results from the efforts of Passenger Services Co-ordination Committee which was set up recently by the Minister of Transport (The

Hon. Vernon F. Wilcox, M.P.)

Bus travellers, it is hoped, will reap the benefit of this joint working, in several ways.

Firstly, there will be no inconvenience or time lost in paying fares each time they enter the bus. Secondly, and probably most important, buses will be able to keep to their schedules more easily, as time will not be lost collecting fares from regular travellers as they board the bus.

IGNORED FLASHING LIGHTS

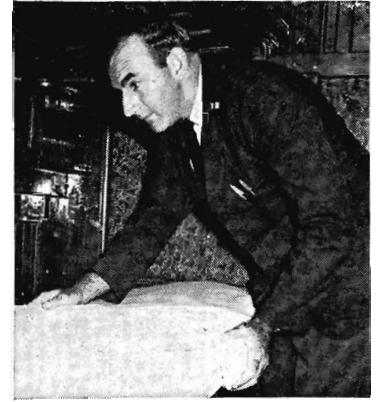
In the Werribee Court, last month, a man who drove over a level crossing while warning lights were flashing, was fined \$40. Later in the same month another motorist who ignored the warning lights at Boundary Road, Merlynston, wasn't so lucky as to be fined; he was killed.

SOME MILDURA PERSONALITIES

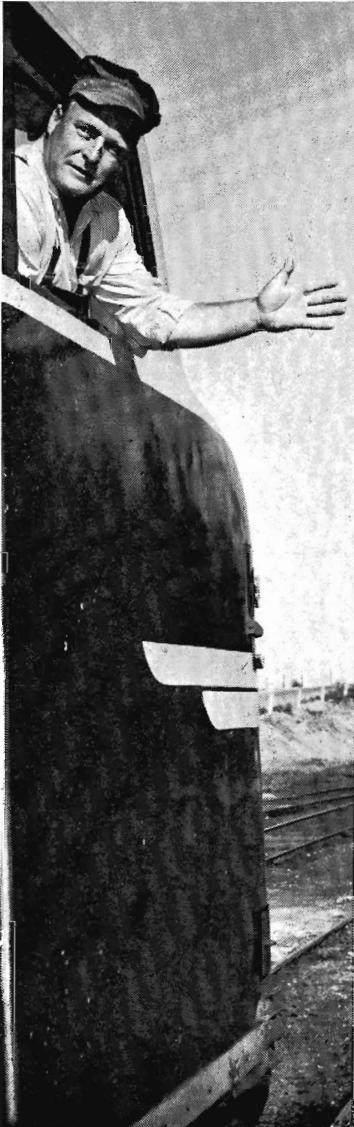
These railwaymen were among those met by *News Letter* staff during a recent visit to the Mildura district.



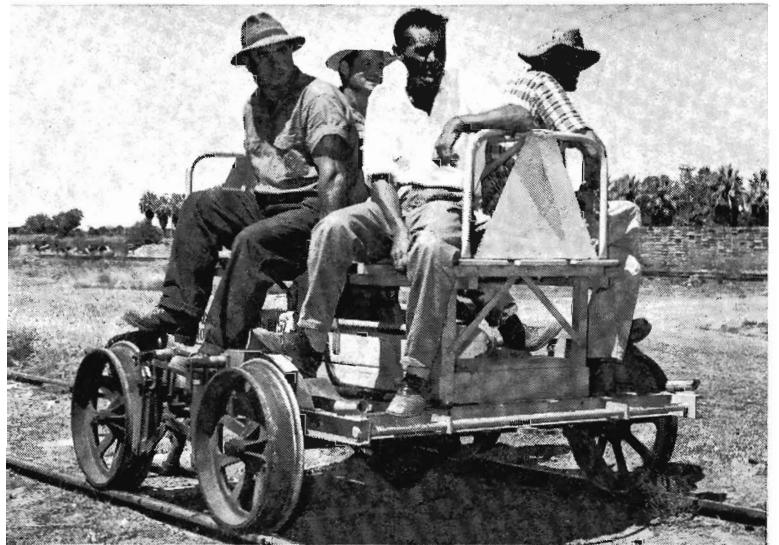
Stationmaster C. Hynes, O-in-C at Mildura.



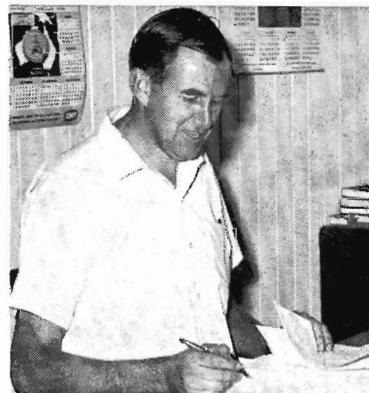
Conductor F. J. Fields makes up bed on Mildura-Melbourne train.



Driver K. W. Bland about to depart on the express goods from Mildura.



Members of No. 29 gang that maintains the section from Mildura to Yelta—(from left): front, Repairers L. Beasy and L. Barnes; back, Repairers C. Chiera and A. Steward.



Clerk W. Cook, O-in-C of Mildura goods section, is making entries in cash book.



Clerk K. Kenshilwood calculates rates and looks after claims in the goods office.

He made a garden

Along the Shelter Shed buildings where they front Dudley Street, is a narrow strip of land, averaging 5 or 6 ft. wide and about 300 ft. long. Quite a lot of rubbish had to be regularly removed from it, as passers by would toss empty cigarette packets and other litter over the fence. Now they don't.

The reason is that one of the men at the Shed has made a garden there. His name is John Jurczszuk, (pronounce it *Jurish*, he says). He came to the Shelter Shed five years ago, and decided that something could

be done about the untidy strip of land. Mr. Jurczszuk is fond of growing things—his people were farmers in the Ukraine—and he has a good fruit and vegetable garden at home. He graded the land, put planks in as borders, and planted it with a few shrubs, annuals, and perennials such as snow grass, dahlias, roses, marigolds, etc.

As well as improving the surroundings (and Dudley Street can certainly do with it), the garden is appreciated by passers by. Which might show that the prevalent Australian disease of litter dumping is not always incurable.



Mr. Jurczszuk and garden.

Safety secretary



Mr. John Chown who has retired as a fitter at Newport Workshops has long been interested in safety. For 28 years he has been a member of the Workshops Safety First Committee, and its secretary for the past eight years. Mr. Chown is a firm believer in the virtues of the country life, and has now exchanged the din of the Workshops for a quiet life tending his garden at Emerald. He is shown examining the stacking of some carriage doors.

Creepy

Moving around a big building in the midnight hours has its eerie moments, Mr. C. O'Hara told *News Letter* just before he retired last month, as Caretaker at the Flinders Street railways building. Before

Senior typist retires



Miss Ivy O'Brien, who retired recently, was one of the best known personalities in the Electrical Engineering Branch, as she had been a typist in the head office of the Branch for 33 years. Miss O'Brien (*centre*) is shown at her farewell, making a reply after a presentation by Mr. I. G. Hodges, Chief Electrical Engineer (*right*).

coming to Flinders Street, Mr O'Hara was on the Caretaker's staff at Head Office where he was a night watchman for over five years.

"Moving along an empty corridor, you hear a door suddenly close even though no one is in the room. It's probably caused by vibration or draughts, but it certainly sounds creepy," he said.

Mr. O'Hara also worked as a liftman in the building, and in the 'thirties was a member of a track gang.

B.E.M. for Goods Checker

Goods Checker E. J. Barrett, of Geelong Goods, was among those awarded the British Empire Medal in the recent Queen's Birthday honours list. The award was made for his services to the Geelong City Urban Fire Brigade, and his work in controlling the fighting of bush fires of last January.

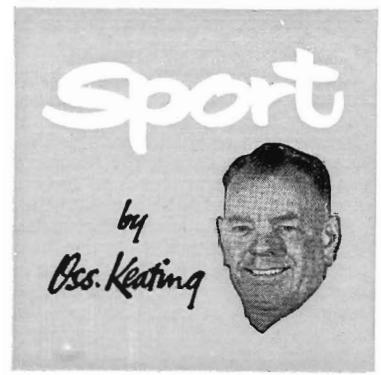
Mr. Barrett joined the fire brigade in 1930, and has been secretary since 1948. He is a life member of the Geelong Inter-brigades Association,

and also a life governor of the Geelong Hospital. Mr. Barrett has been in the Department for 41 years; both his parents worked in the railways, and a brother, Mr. Michael Barrett, is a driver at Ouyen.

Photographic exhibition

The sixth photographic exhibition of the Australian Railways Institutes was held recently at Port Augusta under the auspices of the Commonwealth Railways. The results were: *black and white prints*—highest total of points went to the V.R.I. Camera Club; best print, C. O'Neill (N.S.W.); highly commended, E. Weizenegger and A. Smart (both from V.R.); *colour slides*—highest total of points, South Australian Railways Institute Camera Club; best slide, A. Smart (V.R.).

Mr. Smart, from the Medical Section of the Secretary's Branch, has now won the award for the best slide on three occasions.



Weight lifter

Last month, Nick Kiss von Soly went with a team from the Hawthorn Weight Lifting Club to compete against a representative New Zealand team. Contests were held at Wellington and Auckland, and the Hawthorn team won in both cities—6-1 and 4-3.

Nick has been weight lifting for four years; last year, he was the Australian interschool, light heavy-weight champion, and runner-up in Australian junior championship. His best lift has been 267 lb. and a total of 699 lb. in the three lifts—press, snatch, and clean jerk.

Nick, who came to Australia from Hungary eight years ago, is a junior clerk in the Traffic Branch at Head Office. He also lived for a time in Sweden. (J.M.)



Mr. Kiss von Soly

Social side of Golf carnival

Following last month's report on this carnival, I thought it only fair to the organizing committee to give you some idea of their great efforts in providing entertainment for our interstate visitors during their stay in Melbourne. On Monday, April 28, while the men were playing golf at Victoria, the ladies were taken on a city sight-seeing tour, finishing at approximately 4 p.m. at the Princes Gate Restaurant where a most enjoyable afternoon tea was served by our Refreshment Services Branch. On Tuesday, the ladies visited the Hickory garment factory and Petersville and on Thursday they went to Mornington in a rail car. Friday was probably the entertainment highlight of the carnival. A cabaret ball was held at the Hotel Federal, and a magnificent floorshow, provided by

RETIREMENTS

WAY AND WORKS BRANCH

Bishop, S. L., C/o T. & T. Supervisor, 22/8
Bowden, A. J. P., Kerang, 30/5
Brislane, G. V., C/o Foreman Plumber, 14/5
Brown, H., Newport, 3/8
Fletcher, A. S., Hastings, 27/8
Kyriacou, P., Spotswood, 23/4
Martin, C. W., Korong Vale, 25/8
McLennan, R. P., Darling, 21/8
Pannel, J. E., Bendigo, 22/8
Peevers, L. D., North Melbourne, 18/8
Radnell, E. C., Maryborough, 2/8
Shannon, W. H., Spencer Street, 13/8
Swain, W. T., Bruthen, 26/8
Welsh, L. R., Benalla, 27/8

ROLLING STOCK BRANCH

Jenkinson, G. E., South Dynon, 15/4
Belleff, F. J., Maryborough, 14/4
Pattinson, T. W., Bendigo North, 21/4
Komodromos, C. C., South Dynon, 23/4
Duffin, H. W., Newport, 25/4
Ladas, A., Newport, 2/5
Nithsdale, D., Newport, 1/8
Stephenson, J. S., Newport, 1/8
Booth, W. F., Dimboola, 3/8
Finlayson, J. F., Newport, 3/8
Scott, J. A., Newport, 3/8
McAuley, F. H., Newport, 6/8
Burrroughs, G., North Melbourne, 12/8
Hudson, A. A., Mildura, 12/8
Thompson, L. F., T. L. Depot, 18/8
McErlain, W. T., Newport, 19/8
Curtis, T. C., North Melbourne, 20/8
Walker, E. McC., Newport, 21/8
Curtis, L. J., Head Office, 23/8
McCluskey, J., Newport, 26/8
Welsh, L. R., Benalla, 27/8
Ballard, M. J., E. R., Deport, 28/8

TRAFFIC BRANCH

Wardley, S., Spencer Street, 13/8
Ross, A. E. M., Melbourne Goods, 17/8
White, R. H., Tottenham Yard, 18/8
Mogan, L. A., Melbourne Goods, 19/8
Bawden, E. A., Coburg, 20/8
Burcon, L., Ballarat, 23/8
Beckerleg, W. T., Bendigo, 28/8
Corben, A. S., Spencer Street, 20/6
Gullick, R. M. L., Blackburn Group, 31/5

Williams, C., Melbourne Goods, 26/5
Fowler, A., Melbourne Goods, 5/6
Williams, L. H., Geelong, 7/3
Hinchcliffe, Mrs. M. G., Newlyn, 13/5

STORES BRANCH

Timmons, J. P., Head Office, 2/5
Slatter, E. J., Perm. Way Materials Depot, 9/5
Musicka, J. H., Clothing Depot, 26/5

ACCOUNTANCY BRANCH

Nash, P. N., Flinders Street, 14/6

ELECTRICAL ENGINEERING

Camfield, J. A., Lighting & Power Division, 3/8
Holdercroft, H., Overhead Division, 2/8

COMMERCIAL BRANCH

Lawlor, T. M., Head Office, 5/8

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

WAY AND WORKS BRANCH

Benzie, T. J., Power Sigs. Const., 3/6;
Carroll, E. L., Spencer Street, 7/6
Izard, J. W., Tallarook, 7/5
McDougall, W., Dimboola, 20/5
Nicolaci, P., Power Sigs. Const., 31/5
Sulinski, J., Benalla, 26/5
Williams, K. L., Geelong, 13/6

ROLLING STOCK

O'Brien, C., Ballarat North, 18/5
Clark, L. F., Ballarat North, 26/5
Gerring, E. C., Newport, 12/5
Manfredi, O., Jolimont, 16/5
Dixon, J. A., Ballarat North, 25/5
Petersen, R. L., Newport, 26/5

TRAFFIC BRANCH

Fowler, A., Melbourne Goods, 5/6
Ferguson, G. K., Head Office, 8/6

STORES BRANCH ;

Triggs, C. B., Electrical Depot Storehouse, 15/5

the hotel management, was thoroughly enjoyed by all present.

Mention of the Sunday trip to Emerald was made last month, and on Monday, the day for the final of the Commissioners' Shield and the Tintara Cup, a bus tour of the Mornington Peninsula was arranged for our interstate visitors, with the exception, of course, of the competing teams. This trip was timed to arrive back at the Metropolitan Club House at about 3.45 p.m. so that the tourists could watch the final few holes of this game. The farewell dinner was a fitting climax to a great carnival, and the 260 guests were most appreciative of the excellent meal provided, the wonderful entertainment and the short, crisp and witty speeches. The Chairman of Commissioners, Mr. G. F. Brown, set the pattern in his opening remarks, and this trend was followed by the other speakers, with the result that although the evening was long, at no stage did it become boring. A special word of thanks is due to the small but energetic and capable ladies' committee, headed by Mrs. J. Kennedy, and to our very good friend Lou Curtis, who, with his musical talents, helped make many an evening a memorable one.

Golf

The next few months should be busy ones for our railway golfers. During this period a number of our country centres hold their annual tournaments, our country week is staged, and the annual match against the Australian Postal Institute is played. So that intending players can arrange their itinerary, I am giving below the dates (or approximate dates) of the various events.

Eastern District Golf Tournament

The tournament will be played as usual at the Newry Golf Course on Saturday and Sunday, August 9 and 10 respectively. The major events (A, B, & C grade), of course, will be played on Sunday, August 10. The entry fee, \$3.50, includes lunch and afternoon tea. Overnight accommodation can be arranged if required.

Hamilton District Golf Tournament

The newly formed V.R.I. Golf Club at Hamilton will hold its first tournament at the Hamilton Golf Course on Sunday, August 10.

Wimmera Golf Tournament

The Dimboola Golf Club is the venue for this fixture which will be played towards the end of August. Players who are interested should

contact Bill McComb of Dimboola, or myself on Auto. 2445 as to the exact date.

Country Golf Week

This event will be played at Rossdale Golf Course, Aspendale, from September 15 to 18 inclusive. Events include teams championship (teams of five players), open championship, minor championship and various other events. Lunch will be provided on each of the four days, and a presentation dinner held on Thursday, September 18. Entries close on Monday, August 18.

V.R.I. v A.P.I.

The annual match against the Australian Postal Institute will be played on Friday, September 26 at Northern Golf Club, Glenroy. As we are the holders of the perpetual trophy, it is hoped that a really representative team will be available to defend the shield.

North-eastern Golf Tournament

Sunday, October 5, has been set down as the date to hold this fixture—on the Golden Vale Golf Course, Benalla. The usual large number of trophies will be provided and an excellent day's golf is assured.

Ararat Golf Tournament

This is a new fixture, and it is hoped that as many railway golfers as possible will visit Ararat on Sunday, October 26, to give this new club the encouragement it needs to continue with this type of tournament. Play will be on the picturesque Chalambar links, a course which I feel will appeal to our golfers.

Western District Golf Tournament

This tournament is usually the last of the circuit and is played on the Colac course, on the first Sunday in November. Further information can be obtained from either Ian Patterson, of Geelong, or myself on auto. 2445.

Basketball

The team selected to represent Victoria in the Intersystem Basketball Carnival to be held in Hobart from October 26 to November 5 this year, is Mel Boyd, captain and coach (Bendigo), Ian Read (Bendigo), Barry Matthews (Ballarat), Geoff Lyons (Ballarat), Mick Leehane (Ballarat), Danny Stewart (Geelong), Geoff Grant, Ivan Rendell, Geoff Nicholas, Des Wilson and Brett Newman (all from the metropolitan area). Ernie Huber, from Spotswood, has been appointed manager—but the way he's looking he could finish up a player as well.

The team will fly both to and from Tasmania. On paper, it looks to have far more depth than the team that represented us in Adelaide, but this is a necessity if we are to have any chance in the Commissioners' Shield. One of the disappointments of this carnival is our failure to field a girls team. With the exception of a few, no enthusiasm at all was displayed by our girls and, keeping in mind that we were, together with South Australia, responsible for the introduction of a girls section in this series, it was most disheartening to have to inform the Tasmanian officials that we would not be represented in this particular section.



At Albert Park the V.R.I. team is playing Victour in the Victorian Business Houses basketball competition.

VICTORIAN RAILWAYS

NEWS LETTER

AUGUST



1969



V.R. CHAIRMAN COMMENTS

SMOKING IN TRAINS IS "DRAWBACK" TO CLEANLINESS

Serious thought must be given to banning smoking in suburban electric trains.

In Japanese and most American suburban trains, cleanliness is assisted by the fact that the law prohibits smoking and travellers accept this.

In Victoria, smoking is forbidden in theatres, for example, but smokers manage to survive.

Our longest suburban train trip lasts only about an hour—most passenger journeys are less, the average journey being 25 minutes—so is it unreasonable to ask travellers to refrain from this habit for so short a time? Smoking offends other passengers and contributes to dirty trains that, no doubt, the same smoking people complain about.

In addition, smoking compartments deteriorate more quickly and need painting more often than other compartments.

Four years ago, following a review of the public's smoking habits, suburban trains were changed to approximately two-thirds no-smoking and one-third smoking capacity. Previously the proportions were the opposite. Signs

were amended from the traditional "non-smoking" to "no smoking" as a more positive command to smokers who smoke in compartments not provided for the purpose.

Most suburban carriages have been fitted with latest type ventilators which circulate fresh air, and clear cigarette smoke quickly.

But our biggest problem from smoking passengers is the mess they leave at the end of their journey. Apparently, it's too difficult to replace dead matches in the match box and knock ash, burnt pipe tobacco and old butts into a pouch or old packet. Cigarette packets are easily disposed of in litter bins that are on most stations.

Although considerable thought has been given to the provision of ash trays in suburban trains, no satisfactory solution has yet been reached, nor do we know of any other railway system in the world which has the complete answer to this problem.

What a pleasure it would be to operate a suburban train service in a city where passengers refrain from smoking in the carriages. It's one answer to cleaner trains.

Gaol for station breaks

Three men charged with having stolen \$1931 worth of goods from six railway stations were sentenced to a total of six years gaol at a recent suburban court hearing.

Two of the three men who pleaded guilty to the charges, had broken into the stations over the period of a week, but the other man broke into three stations on the one night.

The stations concerned were Hurstbridge, Wattle Glen, Heidelberg, Greensborough, Macleod and Eltham.

Time on the move

The clock that once stood high above Spencer Street station is now at Lakes Entrance where it will rest atop a tower at the Griffiths Sea Shell Museum and Marine Display.

Made by Gaunt's of Melbourne, the clock was first placed, in 1883, at the Elizabeth Street entrance to the old Flinders Street station.

When a new tower was built in 1901, it was moved to Princes Bridge where it carried on faithfully until 1910 when another home was found for it at Spencer Street where it ticked away until 1926. Station alterations necessitated its removal to a spot about 30 yards from its previous foundation and it was once again put into action on Christmas eve, 1926, by the then Chairman of Commissioners, Sir Harold W. Clapp. It stayed in this position until August 19, 1967.



The old clock.

After providing the time for train travellers for so long, the clock will now tell holidaymakers when their time is running out.

V.R. radio advertising

In the face of substantial rate rises in recent years by many country broadcasting stations, the Department is keeping its advertising expenditure in this medium as stable as possible, by cutting back the State-wide railway messages to three days a week (Mondays, Wednesdays and Fridays), except in the Geelong area as this is not covered by country TV advertising introduced last January. TV commercials will continue each Wednesday and alternate Thursdays between the local and Melbourne-relayed news.

FRONT COVER

Near Melton, T class locomotives, operating as a multiple-unit, are hauling an "up" goods.

More diesels

The Department is buying another six main-line diesel electric locomotives.

The new locomotives, similar to the X class, but with an engine of about 2,200 h.p. will be the most powerful in the diesel-electric fleet. They will be used to haul main-line passenger and goods trains.

Delivery of the diesels from The Clyde Engineering Company Pty. Ltd. will start in May 1970.

At present, the most powerful diesel-electric units of the V.R. are the 1,800 h.p. S and X class locomotives.

Apprentices

The Department will have vacancies for 243 new apprentices, in 1970. Most of the vacancies, covering 21 trades, will be in the metropolitan area, but some will be at Bendigo and Ballarat.

Of the 205 young men who started their apprenticeship this year, 21 per cent had a father in the Railways; and another 21 per cent had other Railway relatives.

A 14-page full-colour booklet, with details of the present wide range of trade training has been produced for interested school-leavers and parents; copies are available from most stations, workshops or the Employment Officer, Head Office.

On time

On the average, 81 per cent of Victorian metropolitan trains run strictly on time, 10 per cent within one or two minutes of time-tables, and only nine per cent beyond two minutes of time-tables.

Also, on the average, more than 86 per cent of Victorian country trains are strictly on time.

More Gippsland trains

Following an extensive review of traffic handled through the 35 passenger stations in south Gippsland, more trains are now running to and from Wonthaggi and Leongatha each day, except Sundays.

Main alterations are:

- an increase to three trains a day, in each direction, between Melbourne and Leongatha on Mondays to Saturdays;
- an increase to two trains a day, in each direction, for Melbourne-Wonthaggi passengers on Mondays to Saturdays;
- a late-afternoon train from Melbourne to Yarram (instead of to Leongatha) on Mondays to Thursdays; evening train to Leongatha only on the same days;
- a 55-minute earlier departure from Yarram of the Monday to Saturday train.

Award for V.R. report

For the second year in succession, the public edition of the Department's annual report has been given an Award of Merit by the Sydney division of the Australian Institute of Management. Fifty awards are made by the Institute of Management, and the final selection was made from a group of 300. The Department has entered only two of its annual reports in this competition.

OVERSEAS CONTAINER TRENDS

Mr. J. Taylor, Railways of Australia U.K. representative, last month attended the Containerization Conference in London, promoted by the College of Production Technology. A summary of his report is published because the current trend in freight movements suggests that the Australian container traffic is only in its infancy.

The 8 ft. x 8 ft. module (of varying lengths) may be in danger as an international standard. Already the West Germans have produced, in quantity, a container 8 ft. 2½ in. wide to facilitate pallet loading. But, at this stage it is only intended for internal use and not for overseas traffic.

There is a more definite trend to containers 8 ft. 6 in. high. So much so that the British Railways have already published the routes over which these can be carried and are taking positive and urgent steps to remove obstructions on many other main line routes to try to prevent road hauliers getting the business. The British Railways Board is very much involved in international container traffic as well as local movements.

Long distance and international road hauliers view the 35 ft. x 8 ft. x 8 ft. 6 in. container as the near perfect load in relation to economical haul, and as best suited to road laws, permissible axle loads, etc. These already exist principally in the Sealand organization. If the International Standards Organization accept them, and they move into Australia, then this is the container for which the

road haulier will quote his most competitive rates. Whether it becomes an I.S.O. unit or not will have to be considered when containerization starts between America and Australia.

Research has shown that in Europe about 15 per cent of container traffic is moving in 40-ft. containers and this percentage is steadily increasing.

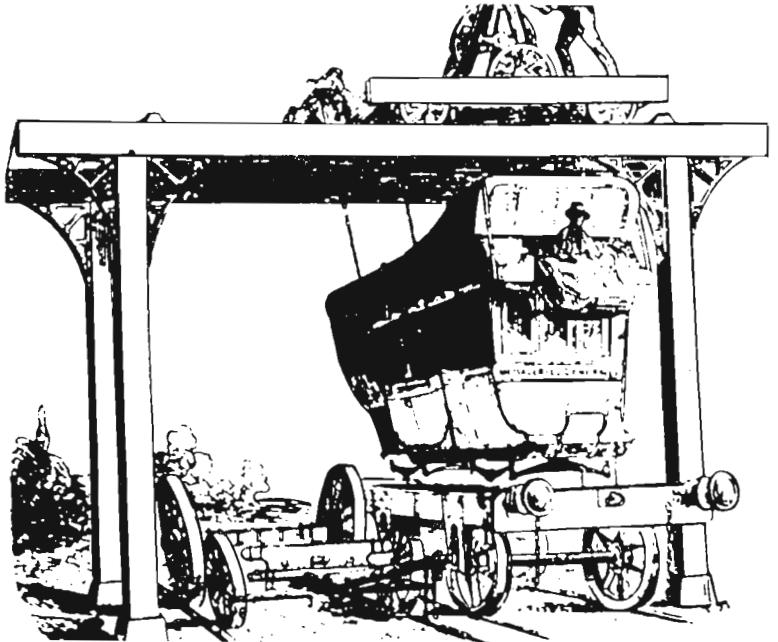
"Intercontainer" was a co-operative company set up by 18 European Railways to handle international containers only on behalf of all members less than a year ago.

It is also associated with "Interfrigo", another co-operative previously set up by 17 European railways to handle only refrigerated containers.

Both "Intercontainer" and "Interfrigo" use 60-ft. flat top wagons at the moment for all traffic. Refrigerated containers are handled on these wagons by plugging in to a separate power van attached to the train. However, "Interfrigo" will soon build new 60-ft. wagons for their own use. Each wagon will have some type of generator underneath the chassis.

British Railways provide for "clip-on" refrigerator units and their wagons are 62 ft. 6 in. long.

Railways of Australia have for some time been saying that containers are not new—they have been handling them for many years—and this is correct. But British Railways actually transferred their first container from road to rail at Paddington station in London in 1842.



Container transfer at Paddington in 1842

UNDERGROUND AT SERVICETON

THE STATION WITH DUNGEONS

Relics of early history are under SERVICETON railway station, close to the Victoria–South Australia border; sturdy lock-up, customs and excise officer's quarters, mortuary, and dungeons; most are still accessible.

They're musty, cobwebby, dusty; there are no windows and little natural light, and the only air comes from small metal ventilators. A section is propped-up with wooden bearers to prevent cave-in.

In the cells area, a double row of little cubicles, separated by a passage way, is beneath and parallel to the Melbourne end of the railway platform. At the entrance are two small sections built as guard-houses.

Iron rings were once fastened to the brick walls so that prisoners could be secured, presumably by chains or handcuffs. The rings have long been removed by souvenir-hunters but the space they previously occupied can be seen.

Local opinion is that this unique jail held miscreants travelling interstate under guard. While most passengers had a meal in the refreshment rooms, prisoners were secured below, until train departure time.

Another suggestion is that the area was for solitary confinement that was necessary from time to time for the convicts that worked on the station building.

But it seems more likely that the prison was intended as a lock-up for smugglers caught in the act, because Serviceton was a border settlement where custom duties had to be paid in the 'eighties and 'nineties.

Close to the cells area is a section believed to be the mortuary. It is an eerie chamber with pieces of wood on the stone floor and cobwebs hanging from the rafters of the low ceiling.

A customs inspector had a section under Serviceton platform in another area. His duties were to prevent smuggling and to collect duties. Anyone caught evading payment was arrested and locked up to await trial.

Even the 11 South Australian Railways employees and their families had to pay customs duties to Victoria, up to 1890, after which business at the station was conducted by Victoria.

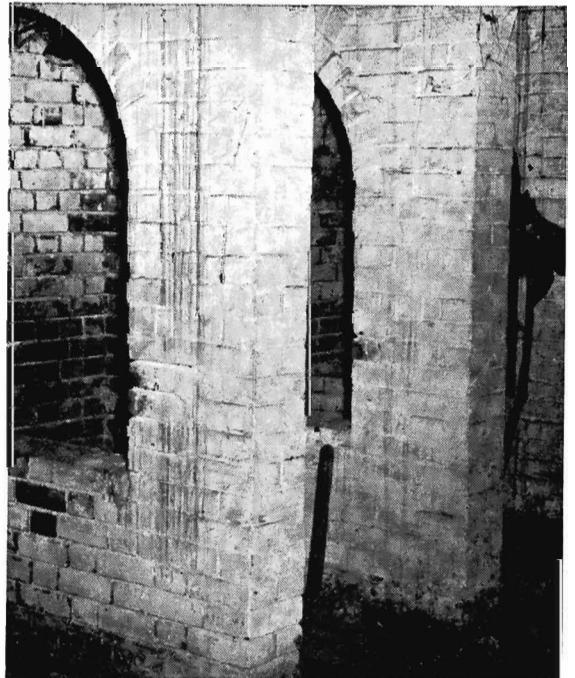
The last customs officer, Mr. J. Mears, vacated Serviceton station buildings on February 24, 1900. A part-time officer took over until the Federal Government acquired customs on January 1, 1901. A Departmental report at the time stated that "the customs section is not required for Railway purposes . . . other sections have been vacant for a considerable time . . ."

Today, one section of the dungeons is used by the Refreshment Services Branch. Mr. M. J. Bakes, Manager of the Refreshment Room, stores kegs of beer for use in the station bar.

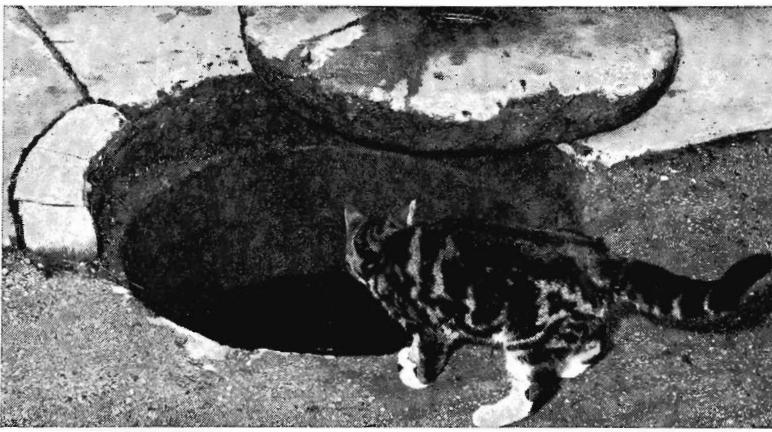
"The beer is beside early-day relics, such as old china, enamel candlesticks, egg cups, and other 19th century objects," Mr. Bakes said.



The mortuary adjacent to the cells.



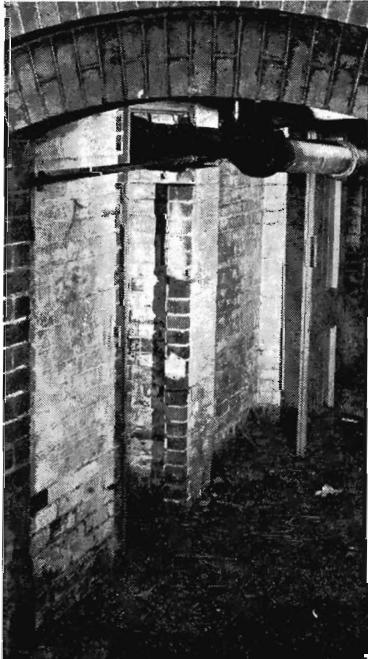
Cells area where prisoners were detained.



A cat is interested in one of the three wells.

Bricks removed recently for installation of a stove in the kitchen were found to have been imported from England. But, according to the South Australian Parliament's Hansard of July 11, 1888, "... bricks were brought from Horsham for the building..." Brick construction was used because "the prevalence of white ants in the Serviceton area meant that a wooden building would be destroyed in a few years".

One of the two stationmasters at Serviceton, Mr. C. Anderson, said "One hears different stories about the early days of the station. Somehow, I don't think I would have liked to have been the stationmaster in the 1890's".



Entrance to the dungeons. One guard-house was at the left.

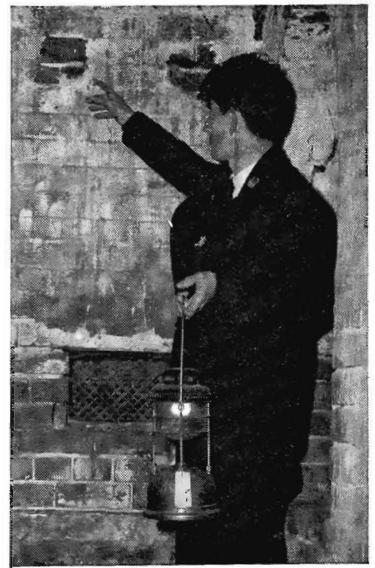
Three underground water tanks are in the platform yards, but only one is now accessible. It is believed to be about 6 to 8 ft. deep.

Disputed territory

Serviceton station is located in what was called the "disputed territory". A line of the border had been marked, in 1839, by Surveyor C. J. Tyers along the 141st meridian, but as the three modes of calculation made by Tyers brought different results, other surveys were made. Henry Wade and Edward White marked new lines, but a check of their surveys in the 1860's proved a discrepancy of nearly two miles.

South Australia claimed this 2-mile strip of land, running north and south; so did Victoria. The "disputed territory" quarrel was not finally settled until the Privy Council found in Victoria's favour in 1914.

In this "disputed territory," neither colony could legitimately assert authority. A man arrested by Victorian police could claim he lived in South Australia, and vice versa.



Stationmaster N. D. Fry points to where rings once held prisoners.

The two colonies imposed customs duties, as a result of which smuggling goods across the border was rife.

Serviceton station cost \$116,305, and is typical of a solid 19th century building. It was completed in 1889 by contractors Walter and Morris.

Work was paid for by Victoria, but South Australia later declined to pay its share until the "disputed territory" question was settled. It was not until 1917 that Victoria was reimbursed.

Today, Serviceton station is controlled by Victorian staff, and the cost of operating and maintaining the station and yard is met jointly by the two Railways on the basis of use. At present this is 56 per cent Victoria and 44 per cent South Australia.



Serviceton station buildings

RAILWAYS JOIN CONTAINER SHOW

The third International Container Services and Equipment Exhibition, held recently in Melbourne, brought together buyers and sellers with compatible interests, to view and display goods, services and information available.

The vital role the Railways of Australia play in the container era was graphically explained by tiny working models, larger scale models of railway wagons and their containers, and prominent coloured photographs of Australian rail wagons.

The various shapes, sizes and capacities of the many containers at present handled by Australia's Railways were also shown.

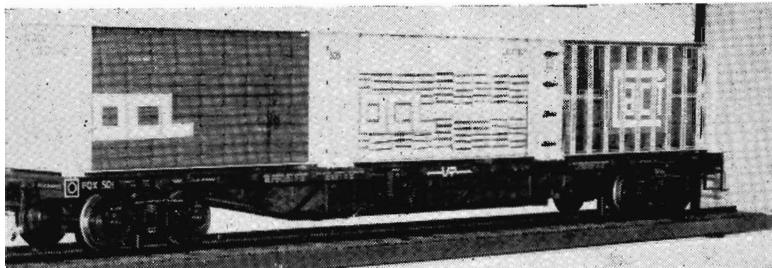
In the main, they are variations and adaptations from the following basic types :

- enclosed containers with either end or side doors
- containers for bulk goods in grain or powder form, built to suit a particular type of discharge
- tank containers for liquids
- insulated containers for goods requiring temperature control
- containers fitted with refrigeration units for frozen goods
- tray-type containers with removable gate or sides.

The Railways exhibit was designed by the V.R. Public Relations and Betterment Board on behalf of Australia's seven railways, and Commercial Agents conducted an advisory bureau.

Rail facilities in models

Millions of dollars worth of equipment was featured in the 72 stands.



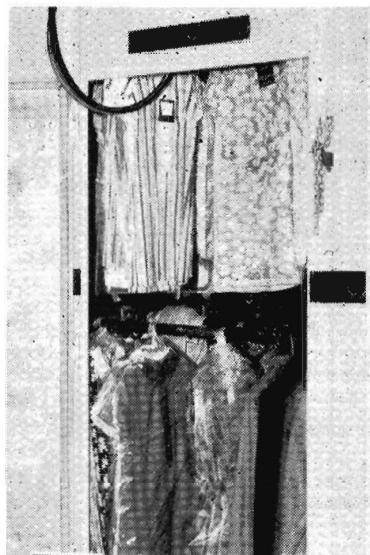
Scale model of loaded container wagon.

Models of Melbourne's new Swanson Dock and the Sydney container terminal showed the part rail plays at each, and were major attractions.

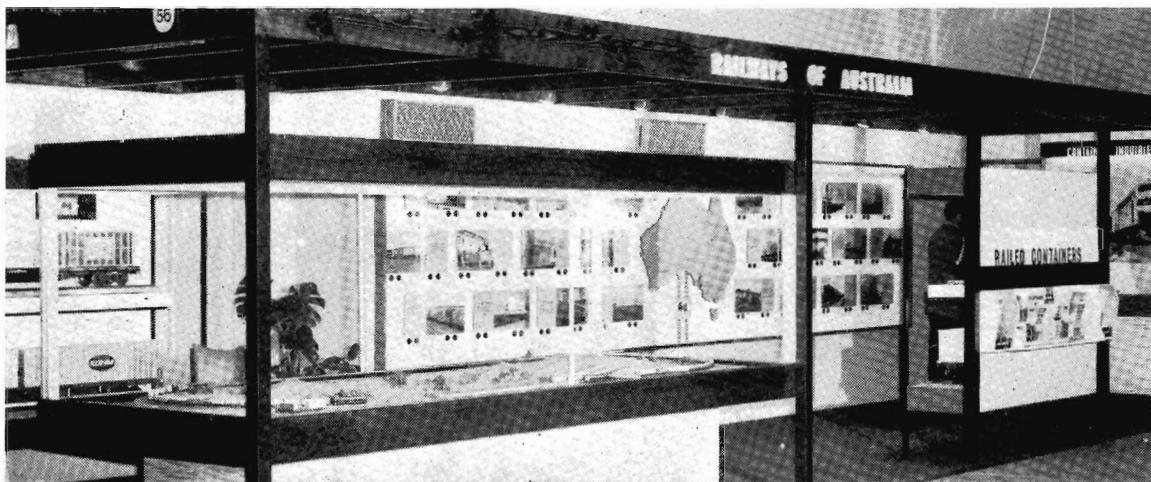
The exhibition showed how the containerization era has brought into sharper focus than ever before, the interdependence of all modes of transport. For efficiency and low cost, containers, bulk-handling and unit loads require some integration of rail, road and/or sea carrying, according to particular needs.

Shipping's freight-handling revolution now sweeping the world—the use of containers in a streamlined, sea, rail and road service—is designed to deliver cargo direct from the seller to the customer's door, perhaps half a world away.

Railways of Australia first moved containers landed from ships in 1959. These containers moved between Tasmania, and Melbourne, Sydney, Brisbane and Adelaide, and often involved a succession of movements—road-rail-sea-road-rail-road.



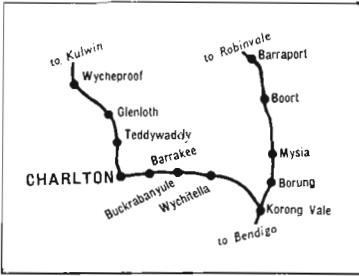
Even clothing can be containerized.



Railways of Australia stand.

CHARLTON

situated on the Avoca River, 173 $\frac{3}{4}$ miles from Melbourne, is on the Korong Vale-Kulwin line.



The station is controlled by Stationmaster W. L. Hedges who came to Charlton two years ago after being at Linton, Casterton, and Kaniva. Assistant Stationmaster I. W. Guy was at Nyahwest, Donald, and Wedderburn before coming to Charlton five years ago. Also at Charlton are Clerical Assistant M. G. English and Station Assistant J. L. Loncar.

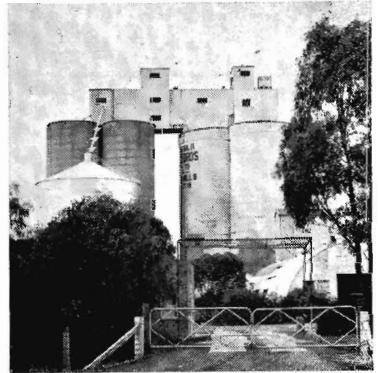
Charlton is served by a rail motor that runs Mondays to Saturdays in each direction between Sea Lake and Bendigo, where it connects with Melbourne trains.



Stationmaster Hedges keeps his bookwork up-to-date.



Station Assistant Loncar weighs films for return to Melbourne.



Noske Bros. Pty. Ltd. flour mill, adjacent to the station.



A goods train at Charlton.



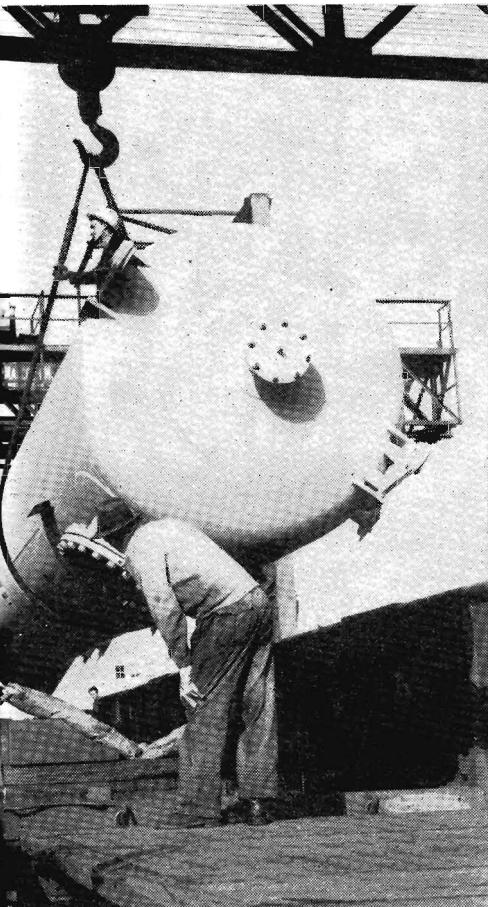
VIEWS OF NEWS

VICTORIA'S HIGHEST RAIL BRIDGE: Forty years ago, last month, Victoria's highest rail bridge came into use with the opening of a double-line goods railway between Albion and Broadmeadows. The bridge, 1,261 ft. long, spans the Maribyrnong River, and rail level is 179 ft. above the river bed. In 1961, one of the tracks was altered for standard gauge trains, and the Melbourne-bound "Spirit of Progress" is pictured crossing the bridge.

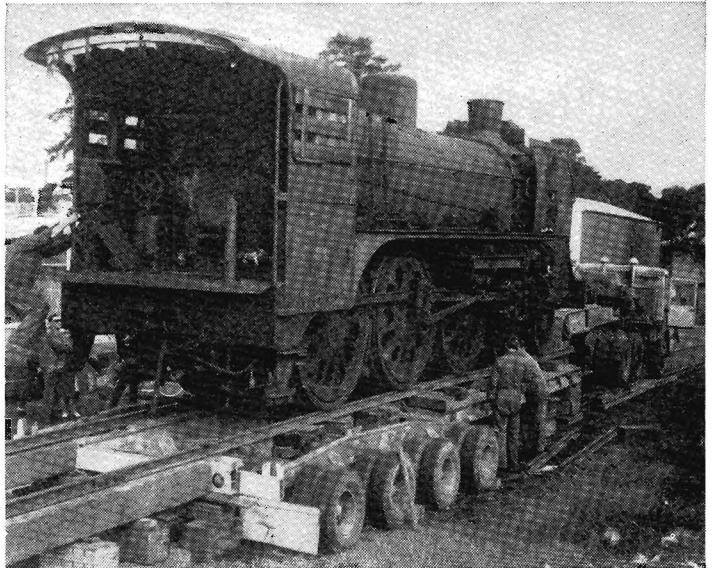


ORCHESTRA BY TRAIN: The California Youth Symphony Orchestra travelled from Brisbane to Melbourne by train for recitals. The 120 members were met by a welcoming party when they arrived by "Intercapital Daylight" at Spencer Street Station.

CONTAINERS OF YESTERYEAR!
 To the Railways, movement of goods in containers is old hat as this picture, taken in 1925 – 44 years ago – shows. A consignment of Sunshine biscuits from Ballarat is being unloaded in Melbourne.



PRESSURE VESSEL ACROSS AUSTRALIA: A tail gas scrubber weighing 21 tons was loaded on to an SKX wagon – with a safety wagon at each end – at Melbourne Goods Yard and railed to Kwinana, Western Australia. The vessel was 83 ft. 9 in. long with a diameter of 7 ft. 6 in.



LAST RESTING PLACE: Steam engine A2 964 has been sold to the City of Preston and now rests at Edwardes Park, Reservoir. A special ramp was built at Reservoir to get the old loco on to a road vehicle for transport to the park. Built at Newport Workshops in 1921, A2 964 has travelled 1,300,421 miles.

TELEPRINTER SECTION EXPANDS

The teleprinter section in the basement of Head Office has grown since January 1948 when, as an experiment, a teleprinter service was installed between the then Spencer Street Telegraph Office and Melbourne Goods.

Today the section has 19 teleprinter machines capable of sending immediate messages to railway stations at Albury, Seymour, Benalla, Wodonga, Shepparton, Bendigo, Korong Vale, Echuca, Swan Hill, Ballarat, Maryborough, Ouyen, Mildura, Ararat, Hamilton, Dimboola, Geelong, Camperdown, Korumburra, Warragul, Traralgon, and Bairnsdale. In addition machines are installed at the East and West towers of Melbourne Yard and the Tourist Development Authority. Messages can also be fed into the P.M.G. system.

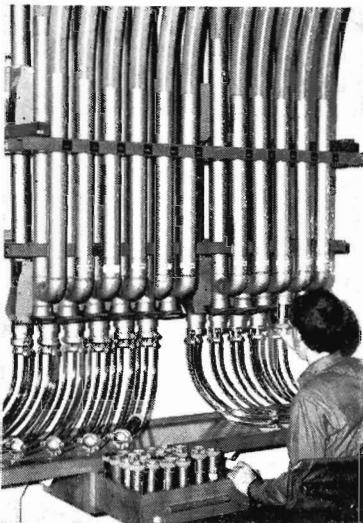
A duplex system is available for messages between Melbourne, Sydney and Adelaide.

The section has been enlarged over a period of years as Morse became obsolete and was replaced.

The division wall has been removed between the two rooms previously used for Morse to make a larger

teleprinter room that is air conditioned.

A feature is the false flooring made of 2 ft. square panels placed on steel framing. The sub-floor houses cabling and is easily accessible.



Eleven sets of pneumatic tubes (with air suction) receive and dispatch messages within Head Office.



Part of the enlarged teleprinter section

NEW VIGILANCE CONTROL

Patent rights are being taken out for the modified vigilance control that is being fitted to 162 locomotives of the B, S, T, H, X, and L classes this year. As the new X locomotives come to hand they will also be equipped.

Under the old system, either driver or fireman could operate the vigilance control. With the modification, both men must acknowledge the warnings which are given in the locomotive cab.

With the new system, the fireman and the driver press their respective vigilance control buttons alternately. Unless the driver has pressed his button, the fireman cannot cut off the air warning, that sounds every 90 seconds, from his vigilance control.

A special control button, on the driver's console, lights up every 90 seconds, and at the same time an electrical warning sounds; thus the driver has visual as well as audible warning that it is his turn to press his control.

When the driver presses his control, the light is extinguished and the warning sound ceases. It then becomes the duty of the fireman to press his vigilance control button when his air warning (as distinct from the driver's warning) sounds.

The device is so constructed that once the button on the driver's console is illuminated and his warning sounds, the fireman cannot, by operating his own control, prevent his air-operated warning from sounding. A press on the driver's control button is necessary to allow the fireman to stop the air warning, by pressing his (the fireman's) button.

In the event of crew members not carrying out their vigilance control duties, the driver's visual and audible warnings are followed by the fireman's air warning in the cab for 10 seconds. After that a high pitched warning siren sounds in the cab for a further 10 seconds, followed immediately by a simultaneous cut-off of engine power and an emergency application of the train brakes.

ANOTHER RECORD

Standard gauge freight trains on the Sydney-Melbourne service continue to haul big tonnages. During the week ended August 2, trains hauled 115,745 tons. The previous best was 109,072 tons during the week ended July 19. Increased consignments of steel, general freight and motor vehicles contributed to the record tonnage.

RINGWOOD IMPROVEMENTS

A \$25,000 project now under way at Ringwood will improve both parcels and goods handling facilities, and the car parking area will be increased from 115 car spaces to 170.

A new sanitary block and crew rooms on No. 1 platform have already been completed; a goods shed being

built on the platform, as well as a loading dock for easier transfer of goods to road vehicles, will give access to the roadway on the northern side of the station.

In addition, the parcels office will be greatly enlarged to handle more parcels traffic. All work is expected to be completed this month.

CREDIT CARDS

Victorian, New South Wales, Queensland, South Australian, Commonwealth, and Western Australian Railways have agreed to honour Railways of Australia credit cards for passenger rail travel.

A 3-man committee comprising officers from the Traffic, Accountancy, and Commercial Branches is formulating details for the introduction of the credit cards in Victoria, as well as deposit bookings, and a lay-by system for the purchase of rail tickets.

The credit card system soon to be introduced is in line with modern business practices whereby the majority of transactions are conducted by some form of credit arrangement.

The scheme will operate on the basis of a telephone application for a reservation to the Central Reservation Bureau, Spencer Street, followed by personal application by the credit card holder to any main railway booking office to collect the tickets. The cost will be debited to the appropriate account and payment made at mutually acceptable intervals not exceeding one month. In certain circumstances, mail delivery of tickets may be arranged.

300 FLASHING LIGHTS

The Department's 300th flashing light and warning bell installation at a level crossing came into use at Hammill Street, Donald, on July 24.

In addition to flashing light installations there are 67 crossings with boom barriers, 57 have signal box operated gates, and 70 have hand-operated gates. All other crossings have reflectorized warning signs.

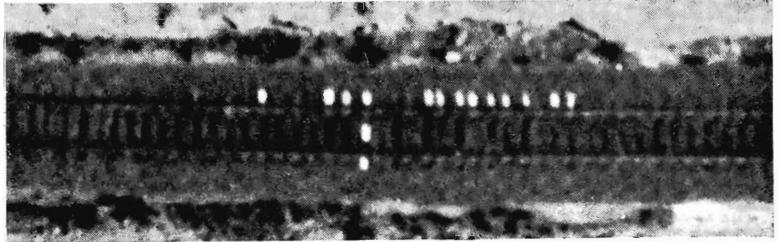
This means that there are now 494 level crossings protected by gates or automatic warning devices, and this number is increasing by about 30 each year, yet the number of accidents per year at level crossings continues to rise.

In 1953, 309 crossings were protected. There were 275 accidents, of which 65 involved trains; 19 people were killed and 47 injured. In 1968, of the reported 451 accidents at level crossings, 93 (about 20 per cent) involved trains; 20 people were killed and 59 injured. Of the 93 accidents involving trains, 34 were at crossings protected with mechanical devices.

Automatic level crossing protection in Victoria dates back to August 22, 1923, when the Amess Street level crossing between North Carlton and North Fitzroy stations on the "inner-circle" line was equipped with "wig-wag" signals.

The first flashing light signal in Victoria was installed at the Warrigal Road crossing near Mentone on March 16, 1932.

MORE SURVEYS FOR CHANGING TRACK



Sleeper painting as seen from the air. This photo was taken at the 139 mile post on the Serviceton line, near Great Western.

A start has been made to re-survey, by aerial photography, 693 miles of Victorian rail lines because of the many changes to buildings, overpasses, loops, station yards, level crossings, etc., that have taken place in recent years.

The lines concerned are Sunshine-Serviceton, Newport-Geelong, and Ararat-Portland, which were last photographed about 17 years ago, and the electrified suburban area, which has not been photographed since 1962.

In addition, Frankston-Crib Point and the Hastings area, Geelong Yard, Portland Harbor Trust area, Ballarat and Hamilton stockyard sidings, and Ararat locomotive depot are included in the contract.

As a mileage indicator for the photographers, sleepers are code-painted at each mile and half-mile post. These landmarks are also used for identification of photographs when recording and indexing.

Paint fades

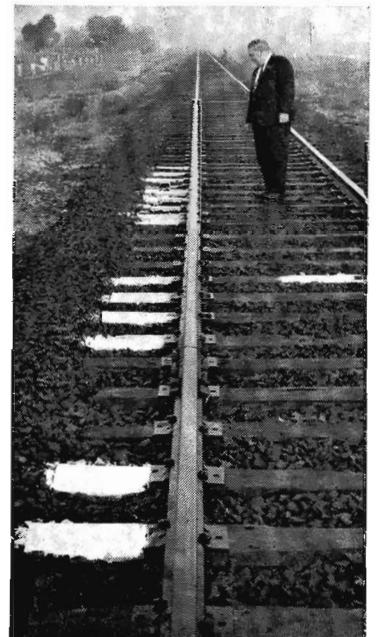
One of the problems in the past has been to avoid deterioration of paint prior to photography because of the impracticability of preparing and undercoating sleepers before painting. However, a one-coat plastic paint is being tested in the current programme.

A private contractor is engaged to take the photographs, at a height of about 1,920 ft. above rail level, and a 12 in. focal length camera is used to provide a scale of one inch to approximately 160 ft. The contractor supplies original negatives and prints to the Department for filing and indexing by the aerial survey section which is under the control of the Engineer of Track and Drainage, Way and Works Branch.

The aerial survey special library

has almost 70,000 aerial photographs that can be used by any branch for general planning and investigation work. The photographs are frequently used to assist in locating D.R.'s, property boundaries, private sidings, boom barriers and flashing light installations, signals, points and crossings, pole lines, curve realignment, bridges and approaches, culverts, workshops and station layouts, substations, advertising signs, and for accident and drainage plans, and land valuations.

The Victorian Railways were among the pioneers in the use of aerial photography in Australia when in 1947, the whole of the State's rail system was photographed.



Mr. W. McDonald, O.I.C. of the aerial survey section, inspects sleepers painted at the 249 mile post, near Nhill.

69 Staff Ball

The "NEW" Annual Staff Ball, held on July 17 attracted 750 guests headed by the official party comprising Messrs. G. F. Brown, Chairman of Commissioners, Mr. E. P. Rogan, Deputy Chairman, L. A. Reynolds, Commissioner, C. S. Morris, Chairman of the Staff Board, S. F. Keane, Chief Mechanical Engineer, I. Hodges, Chief Electrical Engineer, A. Weeks, Chief Commercial Manager, and their respective ladies.

The word "new" was so applicable as this, the 14th Annual Ball, was held at the new Camberwell

Civic Centre Hall. Generally, the Ball was well launched and most went into orbit with the new surroundings.

Last year, the 13th Annual Ball, took the form of a "Jinx Ball". This jinx apparently worked only too well and with the demise of the Palais de Danse it was necessary to find a new venue.

Many long standing patrons renewed the acquaintance of 12 months ago and, with every Branch represented in cordial co-operation, the atmosphere was right for a typical RAILWAY ball.

70 years association

End of the financial year was also the end of a 70-year family association with Railways. Mr. Walter Welch, Senior Suburban Time-Table Officer, who retired on June 30, had followed his father's footsteps; Mr. Leonard Welch had joined the Department in 1898.



Mr. Welch

Mr. Welch has been responsible for suburban train time-tabling, including the Stony Point and Healesville lines, parcels coaches and special suburban traffic such as football and racing trains. During his 16 years in Train Services, he has seen the introduction of "Harris Trains", third track running and 8-carriage trains.

Talking of the future, Mr. Welch who has 46 years experience in all aspects of traffic operations behind him said: "The underground is the answer to smoother suburban train running and will iron out all our problems."

On the sporting side Mr. Welch played interstate cricket with the V.R.I. and is still handy with a golf club. Many railwaymen have been closely associated with him in his position as Treasurer of the Catholic Memoriam and Bursary Association, a role he has held for the past 27 years.



A section of the crowd at the Ball.

Off to the fight

Mr. Frank Oakes, who is supervising work at the Yarraville overpass, has accompanied Lionel Rose and trainer Jack Rennie to America for the world bantamweight title fight.

Lionel, who will fight Ruben Olivares at Dodgers Baseball stadium, Los Angeles, on August 22, will have Mr. Oakes in his corner to assist between rounds. Mr. Oakes was Lionel Rose's original trainer, back in 1960 at Warragul.

"It will be more like a working holiday" Mr. Oakes said, "I think Olivares's style of fighting will suit Rose and I expect the Australian champion to retain his title."

S.O.S. for News Letter

A request has been received from Mr. H. W. Smith, Box 15, Harcourt, who requires October 1950 and March 1951 issues of *News Letter* to complete his collection.

Traffic v Commercial

At a pleasant barbecue and challenge footie match between these branches at Batterham Reserve, Boronia, the Commercial Branch (captain, Len Butcher) proved victors, taking a slight lead in the closing stages after a grim struggle. Scores were 12.13 to 12.6. The Traffic Branch captain was Tom McBean.

The vigor, talent, and standard of play surprised the large number of spectators. Kev Bolton (Traffic) proved a worthy field umpire.

The large attendance, particularly of the younger staff with their relatives and friends, was most rewarding to the joint organizers, Len Butcher and Tony Worsdell (Traffic). It is hoped that this will become the forerunner of future such games.

"Terang great"

"Terang is the ideal station and handy to everything", Stationmaster A. Lyons said. Coming to Terang nine years ago after terms at Kaniva and Berrivillock, Mr. Lyons has seen Terang's powdered milk rail business grow to about 4,000 tons a year. It is railed to Melbourne, mostly for export. "Daily consignments of eggs also keep us on our toes" Mr. Lyons said. The eggs from three poultry farms are railed to Camperdown, Warrnambool and Pirron Yallock.



Mr. Lyons

Loads wine



Mr. E. Knowles, who has been shedman at Merbein for 10 years, unloads empty wine containers for the Mildara winery. Wine is railed regularly from Merbein, mainly to satisfy the tastes of private connoisseurs.

Overseas scholarship

Mr. John A. Clarke, a boilermaker-welder at Newport Workshops, leaves Melbourne this month to take up the 2-year Victorian Overseas Foundation Scholarship that he won last year.

For the first six months, Mr. Clarke will be assistant to a foreman in the British Railways, and later will gain experience in other English industries before continuing to Canada.

Mr. Clarke is only the second V.R. man to gain one of the scholarships since their introduction in 1957. They are provided each year, for outstanding, qualified tradesmen between 21 and 25 years old.

Don't discard them

Old photos and documents often find their way into incinerators and waste paper baskets, but before discarding anything relating to railway history, please let us know.

The Public Relations and Betterment Board, Room 98, Head Office (phone auto 1367) would like to hear from anyone with such material.

Signal to proceed

When a touring party from the Corangamite Bowling Association visited Beachport Bowling Club in South Australia, they found the Bowling Club on the site of the old railway station. The station nameplate still hangs on the wall of the clubhouse and a signal is used to indicate whether the green is open or closed; the station bell calls members to order when so desired.

Astronomer

"This is only the start of exploration of the solar system", Mr. W. G. H. Tregear said when asked what significance last month's moon landing had to astronomers.

Mr. Tregear, a professional engineer with the Structural Design Division, Way and Works Branch, has been President of the Astronomical Society of Victoria on two occasions. This society is an amateur organization wherein some 450 members from diverse walks of life are able to share a common interest. He is director of the Society's demonstrations section and, in conjunction with the Public Telescope Demonstration scheme under the auspices of the Institute of Applied Science of Victoria, is responsible for teaching astronomy and rostering a panel of 40 demonstrators for duty.

Living in the country as a lad, Mr. Tregear became fascinated with the stars in a brilliant night sky, and later when he moved to Melbourne,

took a more active interest in the subject.

For the purpose of making celestial observations, Mr. Tregear has a 5 in. refracting telescope, much of it made by himself, representing many hundreds of hours of painstaking work. It is electrically driven to follow the apparent movement of the stars, giving a maximum magnification of 250.

At present, Mr. Tregear is engaged in rostering people to man Victoria's first-ever Astronomical Exhibition, that will be held in the Lower Melbourne Town Hall from August 23 to 26.

Mr. Tregear is Honorary Curator of Astronomy at the Institute of Applied Science of Victoria and a member of the Observatory Council, being liaison member between the Institute and the Astronomical Society. He is also a Fellow of both The Royal Astronomical Society (London) and The British Interplanetary Society, and is a member of similar American institutions.

Tramway recording

The Tramway Museum Society of Victoria has produced a 12 in. L.P. record of Melbourne's electric tram sounds—titled "Destination City". One track was recorded at Gardiner signal box to obtain sounds at the busy Burke Road level crossing, where trams cross the Glen Waverley railway line—one of only four localities in Melbourne where trams cross the suburban electrified railway.

The sounds as Signalman Chris Vanderplas moves signal levers, closes

and locks gates, are followed by those of the train approaching; then two sharp blasts from its air-brake control as it stops at the station. When the train is ready to move off again, the guard's bell is heard, followed by the acknowledging driver's whistle.

Other tracks on the record include evening peak at Batman Avenue, Elizabeth Street in the morning, and the sounds of various vintage trams.

Records are available from the Director, Tramway Museum Society of Victoria, 13 High Street, Prahran, 3181, and cost \$5.50.



Mr. Ormond Slater from 3DB, who recorded the sounds, presents Signalman Vanderplas with a copy of the recording.

Ambulance Officer

New Ambulance Officer Mr. H. L. Wignall has had 21 years experience in first aid—having obtained his first certificate in 1948. He began



Mr. Wignall

his Departmental career as an apprentice fitter and turner at Newport Workshops in 1938 and, after completing his apprenticeship, worked at North Melbourne Loco. Later he became a diesel maintainer. He was a leader of the North Melbourne Loco first-aid team and, in 1953, was a member of the team that won the All-Australian competitions. Mr. Wignall, who has been Assistant Ambulance Officer since 1965, has been nominated by the Commissioners for a Life Governorship of the Victorian Civil Ambulance Service and will be presented with the certificate at a ceremony later this month.

Mr. R. C. Grace who retired as Ambulance Officer last month, joined the railways in 1933 and was a train examiner at various places before going to Ouyen in 1939. While there he was instructor and leader of the Ouyen first-aid team which was successful in many annual competitions. He was on the Ouyen Hospital Committee for 14 years, and president for four years. While there, Mr. Grace organized a voluntary aid service among railmen and so prevented the closing of a section of the hospital. For this, he was made a life governor of the hospital, and subsequently awarded the Queen's Coronation Medal.



Mr. Grace

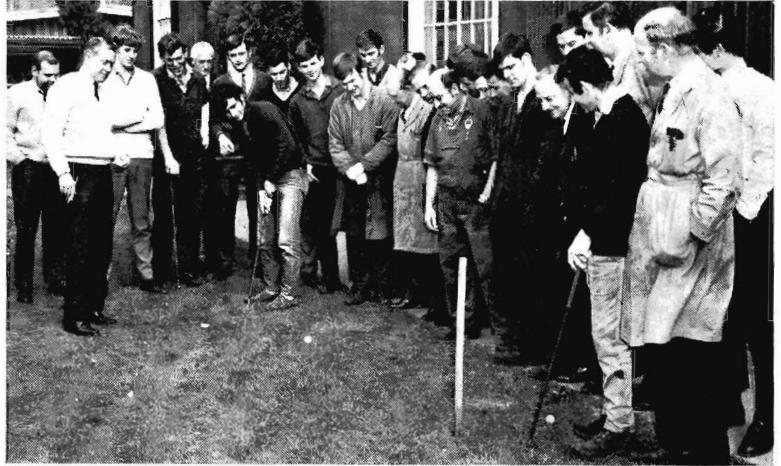
34 years at Mooroopna

Mr. H. Stent, who retired recently as assistant stationmaster at Mooroopna, had held that position since 1935. He joined the Department in 1921 at Ararat and during his 48 years railway service worked at Toolamba, Caulfield, Dimboola and Shepparton. Mr. Stent is a local authority on gardening, and a keen fisherman.

Printers' golf

Printing Works staff forget their printing worries each lunchtime by getting in 10 holes of golf on the lawns fronting and at the side of their works in Laurens Street, North Melbourne.

Fairways vary from 30 to 50 yards and each player is handicapped, up to seven strokes, according to performances. The last series was won by John Spoto who defeated Laurie Smith, 24-31. Hec. Saultry won the previous series, defeating Arthur Shilton and Ron Palm in a spectacular grand-final.



Watched by Printing Works staff, Basil Halkias puts the ball down the fairway.

RETIREMENTS

ELECTRICAL ENGINEERING BRANCH

Schober, H. K., Distribution Division, 8/9
Mitchell, P., Warragul, 26/9.

REFRESHMENT SERVICES BRANCH

Kline, Miss A., Richmond, 5/7

ROLLING STOCK BRANCH

Allen, J. P., Newport, 25/6
Bourke, W. T., Jolimont, 21/9
Capraro, A., Newport, 24/9
Cochinos, M. J., Newport, 26/9
Dagys, R. M., E.R. Depot, 8/5
Derham, T. B., Newport, 1/7
Fleming, T. W., Newport, 16/9
Granger, W. J., Jolimont, 29/9
Hanks, W. E., Newport, 11/9
Howell, H. P., Nth. Melb., 3/9
Hunt, O. R., Newport, 20/9
Keogh, T. P., Head Office, 25/9
Lehman, L. A., South Dynon, 8/9
Lynch, F. J., Nth. Melbourne, 3/9
Nicholls, W. H., Nth. Melbourne, 29/9
Richards, W., Ballarat North, 25/6
Rowson, T., Newport, 11/7
Slide, C., Donald, 3/9
Spencer, I. C., Traralgon, 22/5
Toscano, P., Shelter Shed, 9/6
Williams, W. H., Ballarat North, 11/6

TRAFFIC BRANCH

Darcy, H. J., Melbourne Goods, 26/9
Donaldson, W. K. R., Melb. Yard, 3/7
Goodsir, W. C. P., Flinders St., 29/7
Hansford, C. L., Korumburra, 28/6
King, W. F., Geelong, 4/7
McCarthy, (Mrs.) T. M., Spencer St., 4/7
Mayer, P. E., Bendigo, 24/9
Smith, W. J., Castlemaine, 6/9
Stent, H. M., Mooroopna, 18/6
Stonehouse, C. J., Geelong, 6/9
Vaughan, W. G. J., Glenroy, 27/9
Vincent, J. H., E. Camberwell Group, 25/9
Wells, F. K., Maryborough, 2/5
White, S. G., Viaduct Junction, 13/6

When *News Letter* visited the Printing Works, Ken Deller holed in one, and many "coaches" were vocal in their encouragement to the players.

For some players, the ball is attracted by fairway hazards such as occasional long grass, holes in the middle of the green, hedges, and even a diesel-oil trap door near hole three.

When the competitions began, home made clubs, shaped from old water pipe, were used, but the golfers became so keen that they now have proper clubs.

WAY AND WORKS BRANCH

Barnes, W. J., Nth. Melbourne, 2/9
Clarke, E. (Mrs.), Wendouree, 16/5
Deckert, N. K., Hamilton, 28/5
Densworth, V., Seymour, 9/9
Fitzpatrick, W. J., Carisbrook, 15/9
Fraser, A. V., Bairnsdale, 9/9
Harris, W. G., Warragul, 21/9
Irwin, J. E., Laurens St., 5/6
Mason, A. C., Spotswood, 10/9
Ratcliffe, E. M., Eltham, 20/9
Rawiller, S.F., Marong, 1/9
Roberts, B. P., Spencer St., 27/5
Sheridan, V., Willaura, 2/9
Sutton, T. J., Foreman Painter, 25/7
Tiley, V. A. N., Beaufort, 2/9
Turner, S. E., Flinders St., 7/9

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

REFRESHMENT SERVICES BRANCH

Megson, N. W., Flinders St., 9/6

ROLLING STOCK BRANCH

McDonagh, H. C., Newport, 29/6
McKay, D., Newport, 10/6
Morskyi, I., Nth. Melb., 23/6

TRAFFIC BRANCH

Kennedy, T. P., Lalor, 13/6
Loveland, H. J., Ballarat, 5/7
Narramore, S. A. R., Melbourne Goods, 13/5
Parrissien, R. J., Seymour, 12/7
Walters, M., Melb. Goods, 14/6

WAY AND WORKS BRANCH

Petrie, R. J., W.F. Ballarat, 15/6
Williams, K. L., S. & T. Geelong, 13/6

LINES FROM OTHER LINES

Study of high speed tube train

Trains that could zip along at 400 m.p.h. in sealed tubes on existing railroad rights of way were described at a symposium on transportation in New York by an aeronautical engineer. Dr. William B. Brower, of Rensselaer Polytechnic Institute, Troy, N.Y., has been working on the project for the last two years under a federally-sponsored research grant. "Project Tubeflight" involves a 2,000-ft. scale model of the tube system, in which a train would be propelled by a jet engine and glide almost friction-free on a cushion of air supplied by its own pumps. A working model would have a train 9 ft. in diameter and a tube 15 to 18 ft. wide. — *Railway Age*

Rail tractor

Portland Harbor Trust has obtained a new rail tractor for use on its local track.

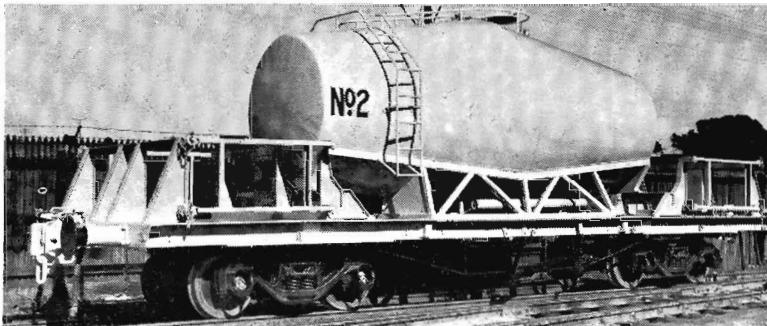
The machine, built by Aresco Trak-Chief Pty. Ltd. of Adelaide and powered by a 180 h.p. diesel engine, is the first of its kind to be commissioned inside Australia, and is the prototype for what the designers expect will have a sound export potential.

The tractor eliminates the conventional chain drive used on machines of this nature; its control equipment is somewhat similar to that of an automatic car.

The Trust's Chief Engineer, Mr. R. F. Harrison, said the tractor had been bought to cope with the growing volume of rail traffic being handled in the port area.

Capable of moving 400 tons of traffic at a time, the diesel-hydraulic drive tractor has been designed to handle the work previously carried out by two smaller units operated by the Portland Harbor Trust since

Dual purpose rail wagon



The wagon showing cigar-shaped container.

A new wagon for the Queensland Railways is equipped to carry 22 tons of bulk cement or a similar weight in copper ingots. Twelve of the wagons are being built at the Ipswich Railway Workshops for use on the Townsville-Mount Isa line.

The wagons will carry cement from Stuart, near Townsville, for use in Mount Isa Mines, and on the return journey, will transport copper ingots to the company's treatment plant at Townsville.

The cement will be carried in the cigar-shaped container, which is 18 ft. 8 in. long, 8 ft. diameter in the centre and 5 ft. diameter at the ends.

The platforms at either end will be loaded with six copper ingots, each weighing 4,000 lb. The ingots will be handled by a forklift, which will load through gates at either side of the wagon ends.

the establishment of the port rail system in 1960.

Tasmania's "ghost line" runs again

Increased mining activity on Tasmania's west coast is bringing new life to an abandoned section of the private Emu Bay Railway.

Once connecting Burnie with Zeehan, the E.B.R. in recent years ran only as far south as the mining centre of Electrolytic Zinc at Rosebery.

However, the plan to expand the output of copper concentrates and pyrites from the Mt. Lyell mine, at Queenstown, has required the establishment of new loading facilities on the disused track at Melba siding, 13 miles south of Rosebery.

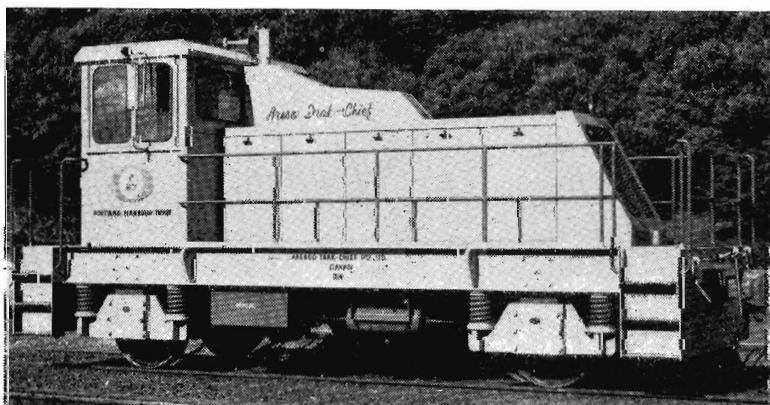
Five new 700 h.p. diesel-hydraulic locomotives are being built to cope with the expanded traffic expected next year.

Five trains weekly, each hauling about 1,000 tons, are to run from Melba Siding to Burnie.

The cost of loading facilities for road trucks at Queenstown and for railway transport at Melba Siding will be about \$500,000. Completion is scheduled for the end of this year.

Argent tunnel, one of the major engineering works on the colourful E.B.R., is to be re-opened for the mine traffic.

An inspection of the Rosebery-Zeehan section earlier this year revealed that the track was in reasonable order, considering that the last train ran on it in 1965. But, it is expected that much of the track will be relaid. Concentrates from the Mt. Lyell mine were originally delivered on the spectacular "cog" railway of 24 miles from Queenstown to Strahan, which was ultimately replaced by road haulage.



Aresco Trak-Chief at Portland.



Football

I know that there have been a few good coaches around, what with Clarkie out at Essendon, Barassi at Carlton and even that young fellow Whitten who is starting to make his presence felt out Footscray way but I reckon we have the daddy of 'em all. Jack Sharp of South Dynon Loco. is the bloke I'm talking about. Just look what he's done with our V.R.I. teams in 12 months.

Last year we were completely outclassed by our Postie mates in the annual match, but this year, oh boy, what a different story. It's even rumoured that Smithy is trying to get Jack to help out down at South. Of course, Jack would be the first to admit that he received mighty backing from George Peters and Graeme DeYoung, both of Newport Workshops, his co-selectors, who were a tower of strength behind the scenes, and also from the players themselves who were behind him to a man.

Now for a little about the game. Due to injuries received over the previous week-end, it was necessary to make a number of alterations to the selected side but in fairness to the boys who stepped in at the last minute, it was obvious, as the final result indicated, that this in no way weakened our side. The one thing that did worry the selectors was the loss of height around the field, and this fear appeared justified when, in the first quarter, after winning the toss and electing to kick with a fair wind behind us we were struggling to get the ball past the centre line. A.P.I., on the other hand, looked a well trained, purposeful combination and by quarter time had gone to a handy 11 point lead, the scores being V.R.I. 2.1, 13; A.P.I. 3.6, 24.

Keeping in mind that we had to kick into a strong wind, Sharpie made a couple of positional changes, offered some advice to a few of the lads, and asked the team for a big effort against the breeze. The re-

sponse was magnificent and immediately play began in this quarter we looked a different team, with players starting to fire all over the field. Young Wheatly started to earn kicks against Sullivan in the centre, George Allen at full forward pulled down a couple of beauties and kicked truly and the back line tightened their play and so restricted our opponents' scoring opportunities.

The result of all this was a terrific six goal burst while the opposition could manage only one major for the quarter. At the long interval the scores were V.R.I. 8.5, 53 to A.P.I. 4.9, 33.

The second half of the game was a fairly even tussle, but although the Post Office boys battled hard, they could not bridge this gap. At three-quarter time we still held a 17 point advantage and, in spite of finishing with only 17 men, we were able to run out winners by 18 points. Final scores, V.R.I. 14.10, 94; A.P.I. 10.16, 76.

Best players for V.R.I. were L. Murphy, who played a great game in the ruck all day, J. Wheatly who, after quarter time, dominated the centre, G. Allen for an excellent game at full-forward, D. Pinczuk, who won all day on his wing, and captain Eddie Power at centre half-forward. Our goal kickers were Allen (5), K. Hardy (3), Power (2), Crick, McPoyle, Murphy and Gibb.

Mr. W. Walker (Secretary for Railways) representing the Commissioners, Mr. M. L. G. McKenzie (President, V.R.I.), Mr. F. M. Mitchell (General Secretary, V.R.I.), Mr. D. O'Donnell (Vice-Chairman, V.R.I. Sports Committee), some few V.R.I. Councillors and quite a number of off duty railwaymen were interested and at times quite vocal spectators.

At the presentation function after the match, our host, Mr. J. Carroll, the genial President of the Australian Postal Institute, welcomed us with his usual sincerity, ably supported by Mr. G. N. Smith (Director of Posts and Telegraph). Mr. McKenzie and Mr. Mitchell suitably res-

ponded. Mr. Smith then handed the perpetual trophy over to our captain, Eddie Power. And might I add, it's nice to see another trophy come back home.

Mention has already been made of the help received from Jack Sharp, George Peters, and Graeme DeYoung but before concluding I feel I should express my appreciation, particularly on behalf of the Institute, to our time-keeper, Tom O'Neill, goal umpire, Ern Huber, our trainers, boundary umpires, first aid attendant and those players who made themselves available for selection. Finally, let me say to Bob Martindale and our other friends in the Postal Institute that although beaten, they were in no way disgraced; except for our second-quarter burst, there was nothing between the sides. The score on the perpetual trophy is now three wins each. Look out for next year, that match will be a beauty.

Bowls

Railway bowlers should keep in mind that the next Intersystem Bowling Carnival will be conducted in Launceston, Tasmania, from Sunday, February 22, to Friday, March 6, 1970, inclusive. Capitation fees will be; males \$18, females \$10. Any bowler interested in being considered for selection, providing, of course, that he has been a financial member of the V.R.I. for at least six months, should make sure that his application reaches the Hon. Secretary, V.R.I. Social Bowling Club, or myself, c/o V.R. Institute, Flinders Street, Melbourne, not later than Monday, September 15, 1969.

The Social Bowling Club advises that the 1969-70 season opens on Sunday, November 9, with a visit to the V.R.I. Bowling Club. The first metropolitan game will be at the Toorak Bowling Club on Thursday night, November 13. Subscriptions (metropolitan \$2, country \$1) for the new season are now due. New members are also being sought, and further information can be obtained by ringing Auto 2445.



Ruckmen fly at a centre bounce in the game V.R.I. v A.P.I.

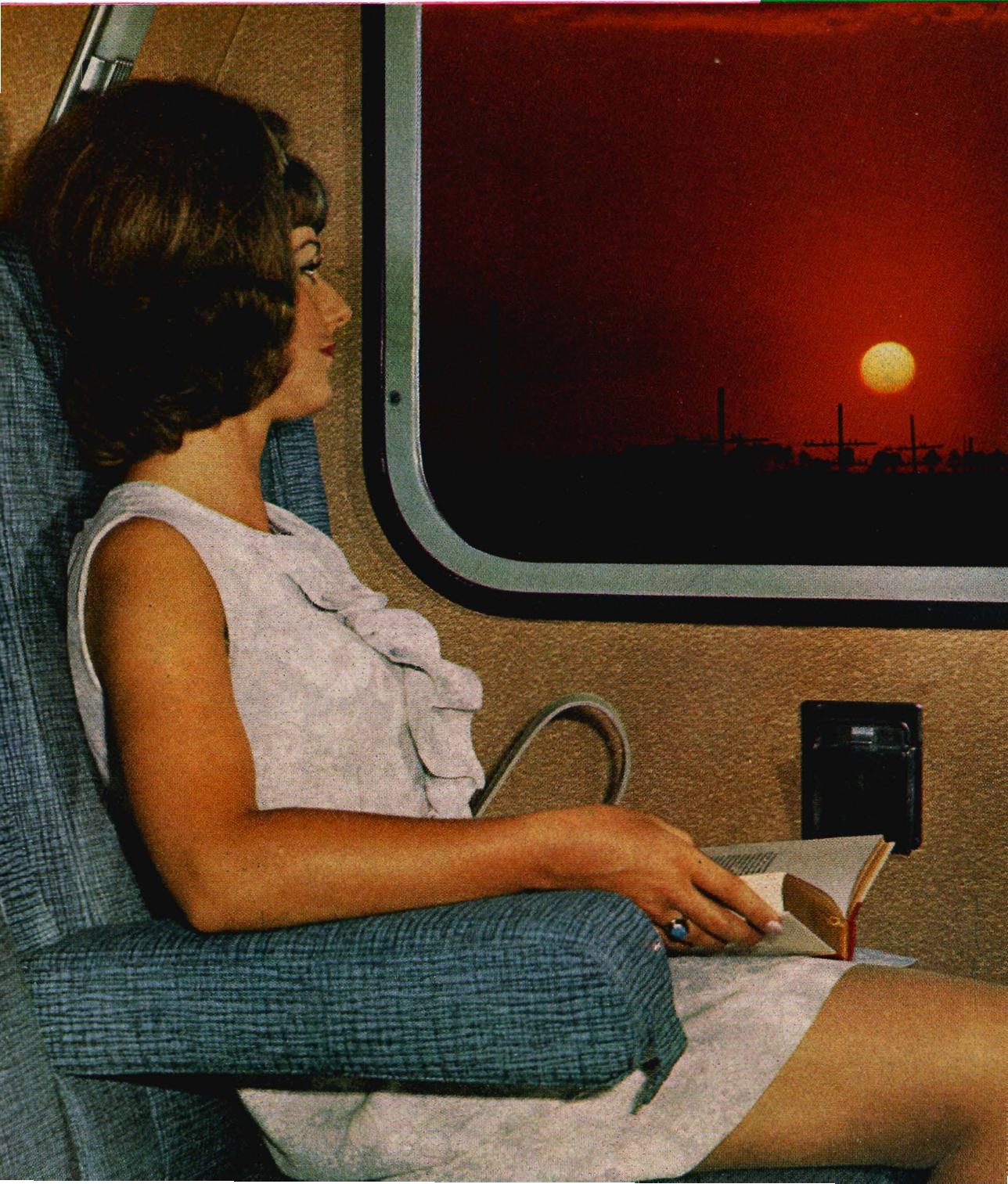
VICTORIAN RAILWAYS

NEWS LETTER

SEPTEMBER

VR

1969



MUST THIS TRAGEDY HAPPEN?

Victoria could face a serious accident if drastic action is not taken to police road vehicles carrying over-height loads, and bring home a sense of responsibility to drivers of all road vehicles even though their loads may be within the legal height limit.

With loads that are too high to pass under many rail bridges, some of these vehicles are hitting bridges and causing serious structural damage.

A few weeks ago, only a motorist's vigilance prevented such an accident at Gardenvale. The motorist stopped a road transport within inches of the Nepean Highway bridge. Later, it was found that the transport driver did not have a permit for his load of 14 ft. 5 in. height which was in excess of the legal limit of 13 ft. Even when some of the load was removed, it was still too high to pass under the bridge. Signs attached to the bridge stated that the clearance was 13 ft. 8 in.

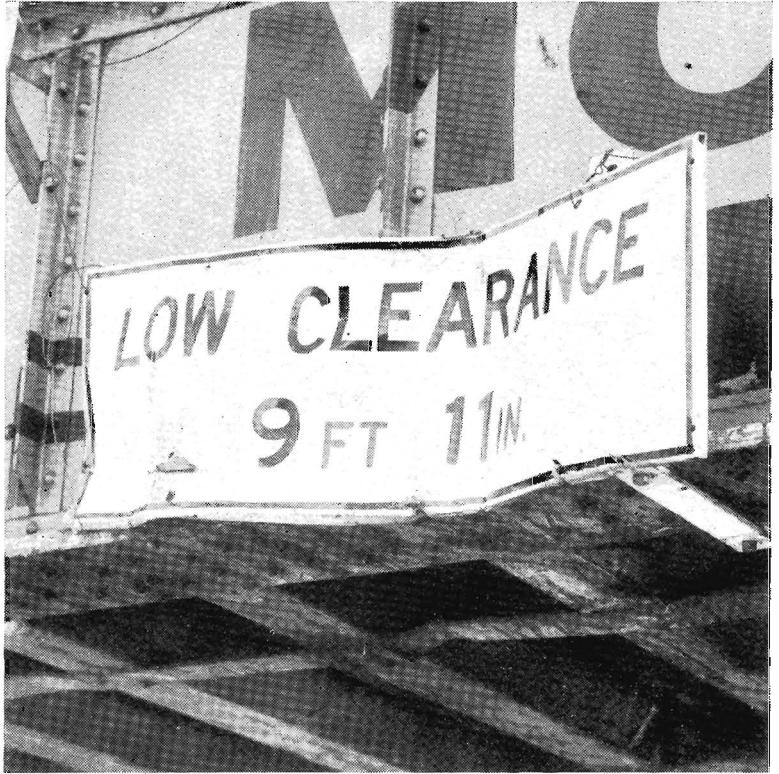
The Gardenvale incident was followed by another one at Ascot Vale. A few days after, on August 4, a vehicle tore a protective angle iron from the edge of a goods line bridge over Dudley Street, West Melbourne. On the same day, a furniture van struck the bridge over Merri Parade, Northcote.

At Ascot Vale, the bridge was struck by a *hit-run* driver who apparently did not consider the incident worth reporting. Fortunately, an alert bystander warned us and provided details. Damage to the bridge will cost \$500 to repair.

We are also gravely concerned about the irresponsibility of drivers who fail to report their accidents when hundreds of lives could be in danger.

When a bridge is hit, there is an ever-present danger of the girders being moved and a train derailed. Consequently, both tracks and bridge must be thoroughly examined by engineers. This causes serious train delays and much inconvenience to passengers.

Last year, the bridge over the Nepean Highway at Gardenvale was struck twice within a period of 10 days, by over-height motor vehicles. In each instance, we were not advised of the accident until about



Damaged sign on bridge at Montague struck by load of road vehicle.

half an hour after it had occurred. During that time, passenger trains continued to run over the bridge. Had the damage been more serious, one of those trains could have crashed on to the highway.

A few months previous to those incidents, the same bridge was also struck. On that occasion, the structural damage was so severe that had a train passed over the bridge, it might have collapsed.

At all our bridges where the clearance limits are less than the legal road transport height, signs are displayed to warn approaching transport drivers of the clearance at the bridge. All other bridges are being fitted with appropriate signs, as well.

Our problem is to keep replacing the signs that are damaged by drivers attempting to pass under a bridge even though the sign shows the clearance is not sufficient.

The responsibility is on the drivers of all road vehicles to ensure they can safely pass under bridges.

The frequency of these accidents is alarming. In one week alone, last month, four bridges were struck.

We cannot emphasize too strongly that if more care is not taken by drivers of road vehicles, a major accident could occur.

For many in this community, it is literally a matter of life and death.

FRONT COVER

To many people, familiarity has made the train a very ordinary—even if very useful—part of their lives. On the cover is a reminder—should one be necessary—that train windows can give views that are far from ordinary, such as this sunset framed by a *Spirit of Progress* window.

South Yarra development

The Department has granted a long-term lease of air-space rights over railway land near South Yarra station to Ola Pty. Ltd.—a subsidiary of Hooker Projects Pty. Ltd.—for building a shopping centre.

A spokesman for the company said the centre would cost more than \$9 million and comprise over 250,000 square ft. of retail shopping space. It would have main access at the level of Toorak Road and include five levels. There would be two basement levels of customer car parking, a ground and first floor shopping area, with a department store at ground, first, and second floor levels.

The centre had been designed primarily to complement the atmosphere that South Yarra has already developed. This would be done by using natural materials, and by reducing the large scale feeling usually associated with shopping centres.

The centre would not be surrounded by vast areas of car parking, as parking areas would be effectively hidden below and at the rear of the centre. An open plaza, about 55 ft. wide, would provide a pleasant recreation area across the front of the centre, along Toorak Road, opposite the entrance to South Yarra station.

Fully enclosed and air-conditioned, the developers expected the centre would become Melbourne's top fashion shopping complex. It is believed that the first floor level would be the first fully carpeted mall in Australia. Comfortable seating would also be provided for customers.

The developers expected that work would begin on the site next year.

Transport papers available

The Institute of Transport (Victorian Section) has arranged for a reprint of the 1967 Seminar papers entitled *Australian Transport in the next Decade*. The papers deal exclusively with goods transport, and a limited number of bound copies is available at \$2 each from Mr. F. E. Edmonds, C/- Transport Regulation Board, P.O. Box 119, Carlton South 3053.

Quick tax

Teamwork from the Data Processing section of the Secretary's Branch and Accountancy Branch staff resulted in the earliest-ever issue of taxation group certificates. The certificates were completed and issue began on July 2, beating last year's effort by 10 days.

The entire 28,000 group certificates were produced by the Department's G.E. 415 computer on a continuous run in about seven hours.

Warning to vandals

At Preston Court, last month, Mr. R. H. Barnes J.P., sitting with Mr. L. Struth J.P., imposed maximum fines of \$40 each on a youth and girl found trespassing on railway property in the early hours of the morning. Mr. Barnes added that vandalism of railway property caused such serious losses to taxpayers that he would have no hesitation in imposing the maximum penalties for that offence.

Costly words

In a recent case at the Footscray Court, a man was fined \$50 for indecent language and \$50 for threatening words he had used to a ticket collector on a Geelong train.

Crashed through gates

In Ballarat Court, last month, a driver who crashed through the railway gates in Lydiard Street, Ballarat, was fined \$50 and had his licence suspended for four weeks. He was also ordered to pay \$22.12 costs.

Trespassing

In the 6-week period to the middle of July, 13 persons were convicted for unauthorized crossing of railway lines, and 14 for unauthorized entry or exit on railway property, including station platforms. The

average penalty imposed by the Courts was \$10 for each offence.

Broke windows

"People who smash property as you have done, don't deserve leniency", the magistrate said when he fined two 17-year old youths \$50 each and ordered them to pay \$3.15 damages in Ringwood Court recently for breaking windows "for kicks".

In a statement to the police, one of the youths was alleged to have said he was sitting in the train with a girl he had met at Ferntree Gully.

Between there and Ringwood he had heard the sound of glass breaking in his carriage, so, to impress the girl he was with, he also smashed a window with his foot.

"She told me to grow up" the accused said, "but by then it was too late."

The other youth said he was also sitting with a girl and had slept for a while on the train. "I woke up and smashed a window for something to do", he was alleged to have said.

Who pays ?

"It cost the Commonwealth \$65 million annually to maintain and operate all D.C.A. airports and navigational facilities. Yet only \$25 million was recouped in air navigation charges, fuel tax and miscellaneous D.C.A. revenues such as rents and business concessions."—The Director-General of the Department of Civil Aviation, Sir Donald Anderson, as reported in *The Australian*, July 23, 1969.

Prize Winners



To celebrate the golden jubilee of the electrification of Melbourne's suburban service, the Australian Railway Historical Society arranged a competition with prizes for the person who covered the greatest rail mileage on the day, using the specially issued all-lines ticket. Mr. E. P. Rogan, Deputy Chairman, (left) is presenting prizes given by the Department, to the winners—Messrs. R. Gilbert (left) and R. Smith—who each travelled 355½ miles.

WATCH THE BIG TONNAGE ROLL

This was the title of a paper delivered recently by Mr. G. F. Brown, Chairman of Commissioners, on behalf of the Australian Railways, at a Seminar held in Perth by The Institute of Transport, Western Australian section.

The talk is summarized in two parts. In this issue, Mr. Brown discusses the requirements of a modern competitive railway system. Next month, the function of railways in today's transport world will be explored.

The Hughes & Vale Privy Council appeal in 1954, based on Section 92 of the Constitution, resulted in Australia's railways having to look themselves over very thoroughly. Competition flowing from this decision brought changes in thinking and widening of horizons.

Western Australia is leading the rest of Australia by far in the construction of new railways which are being built to handle big tonnages of bulk commodities, and illustrate better than anything else the predominant role of railways in this aspect of land transport.

On the iron ore railways of the north-west, 3,600-h.p. locomotives in multiples of three will be hauling payloads of up to 12,000 tons, on tracks laid with rail weighing 132 lb. per yard, with no brakevan in the rear. This is railroading in its most efficient form, measuring efficiency in cost per ton-mile of output.

The W.A. Railways are showing what can be done on 94-lb. rail—locomotives of 3,300 h.p. with an axle-load of 22½ tons, hauling 90 tons gross weight ore wagons, with train loads of up to 8,000 tons.

Prior to these developments, only the Commonwealth Railways, from Leigh Creek to Port Augusta, had been hauling train loads that would be regarded as big by world standards. These consist of 5,630-ton trains, of 80 bogie wagons and a brakevan, hauled by two 1,850-h.p. diesel-electric locomotives.

Most Australian railway routes, except commuter lines, operate in a condition of over-capacity, which leaves scope for a reduction in unit costs if additional traffic can be obtained.

Over-capacity is unavoidable, as the minimum rail facilities necessary to give a competitive standard of service have more capacity than most Australian routes can fully exploit.

REQUIREMENTS

To meet today's highly competitive market, a modern railway must have

• Diesel locomotives of adequate horsepower

Compared with steam engines, diesels have higher efficiency, greater availability and versatility, higher sustained speeds, and lower maintenance costs.

Also, at present-day prices of electric power, diesels have a lower fuel cost per unit of output than electric locomotives, which otherwise possess very similar characteristics to the diesels.

The effect of the big technical advantages of diesel over steam traction is reflected in a big increase in the efficiency of railway operations generally. Diesels have increased the productivity of tracks, rolling stock and manpower.

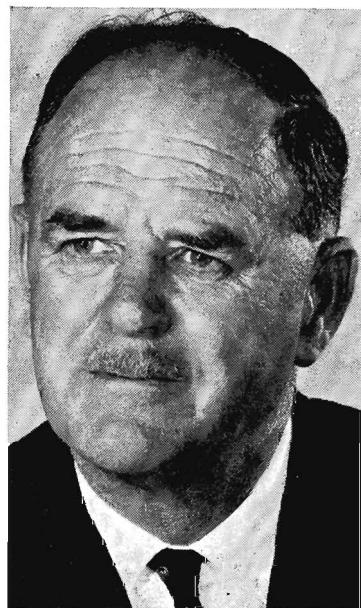
Gas-turbine locomotives are a possible development for the future. From the operating viewpoint, these locomotives are similar to diesels and could replace the latter only if they develop to the stage where they can show a cost per ton-mile of output advantage.

The concept of "adequate horsepower" depends upon such factors as maximum speed of track, terrain, minimum acceptable speed over steeply graded sections, and intensity of traffic.

The power of individual units is governed mainly by traffic demands. Economically, each System will require, for each type of application, locomotives of the highest horsepower, within axle-load limitations, for which full-time occupation can be guaranteed.

As separate units can be coupled in multiple under the control of one crew, if very high horsepower is required only for a limited proportion of total trips, it is better to meet this requirement by combining smaller units, each of which for the rest of the time can be used to the limit of their respective capacities.

In Victoria, individual units beyond the 2,000-h.p. range cannot be justified, although combinations up to 5,000 h.p. operate.



Mr. G. F. Brown

• High-speed track

The biggest problem in Australia is insufficient traffic to justify the cost of a really heavy-duty, world-class track.

This is a problem not faced by other Australian transport operators. The sea and the air are as smooth (or as rough), and will carry the same loads, the world over; Australian operators of heavy-duty road transports have adequate road pavements provided for their use.

Only the railways have to provide, pay interest on, and maintain their own track, and the cost must be reasonably related to the traffic volume. As has been proved in Japan, railway track can be built capable of competing with airline speeds over suitable distances—if enough traffic is available.

This is the reason why our track standards, and our speeds, are lower than those in more densely populated countries. Any attempt to raise our standards to overseas levels, at existing traffic volumes, would simply price us out of the market.

The mechanization of both track relaying and track maintenance has greatly reduced manpower costs and at the same time enabled track standards to be improved. Re-sleepering with a mechanized gang every few years means less disturbance to the track than the old system

of spot renewals by local gangs, and paves the way for mechanized lifting and lining, which is more accurate than hand methods and eliminates much backbreaking labour.

With this method, branch lines where traffic is light in total volume and seasonal in character can be closed for two-thirds of the year and brought up to working order each year before the seasonal movement begins.

● Crossing loops

In spacing crossing loops, running time is the governing factor; distance in miles will vary with the type of terrain.

When a tight pattern of train crosses is scheduled in single-line territory, the late running of any one train will disrupt the pattern to some extent; the shorter the running time between loops, the less this disruption will be.

With a loop spacing of between 20 and 30 minutes, disruption can usually be kept within limits compatible with the requirements of overnight deliveries between adjacent capital cities.

The length of crossing loops will depend upon operating requirements in the particular area, but with the train loads that can be hauled by modern diesel locomotives working in multiple, about $\frac{3}{4}$ -mile can be regarded as a minimum.

● Tokenless block working

With token-type single-line block working (electric staff, etc.), it is impossible to cross two trains without delaying one of them for a minimum period of about five minutes; depending on the location of the signalman relative to the ends of the loops, the actual delay to the

first train to be admitted can be considerably longer.

With tokenless working in association with 3-position power signaling, train speeds can be controlled much more precisely, and it is possible to cross two trains at low speed without stopping either.

Even if the two trains do not arrive at the loop simultaneously, departure of the first train to be admitted follows immediately upon passage of the second train; there is no delay while the token is conveyed to the driver.

● Adequate terminal facilities

The rail customer is concerned only with the time taken from loading at origin to delivery at destination. The longer this time interval, the greater the cost to him.

A further pressure on the railway systems concerned in overnight intercapital deliveries is the freight forwarder's desire to load as late as possible in the day and collect as early as possible the next morning.

It is, therefore, no use maintaining high on-line average speeds if hours are wasted at each end marshalling trains or placing loaded vehicles for discharge. The modern answer to the terminal problem, from both the operating and economic aspects, is the automated hump shunting yard with long arrival roads, separation of sorting from marshalling, and hump under-passes for simultaneous sorting of incoming trains and departure of outgoing trains.

All these features are being incorporated in the new yard under construction in Melbourne.

● High speed wagons

Today's competitive conditions demand wagons designed to meet

the needs of particular classes of traffic. This includes both bulk commodities—cement, flour, wheat, coal, etc.—and non-bulk items which require special provision to be made for them—motor cars, Flexi-Vans, and containers.

With these types of traffic the size of wagons is limited only by operating considerations—axle load and clearance. For free interchange on Australian broad and standard gauge lines, the all-up weight of 4-axle bogie wagons is limited to 75 tons (72 tons at present on broad gauge lines in South Australia). Heavier loads are being handled on standard gauge lines by the W.A. System, which is prepared to operate wagons up to a gross weight of 93.75 tons on 94-lb. rail.

Roller bearings are now standard practice, and other bogie details are being standardized to the maximum practicable extent to facilitate wagon inter-change and bogie exchange.

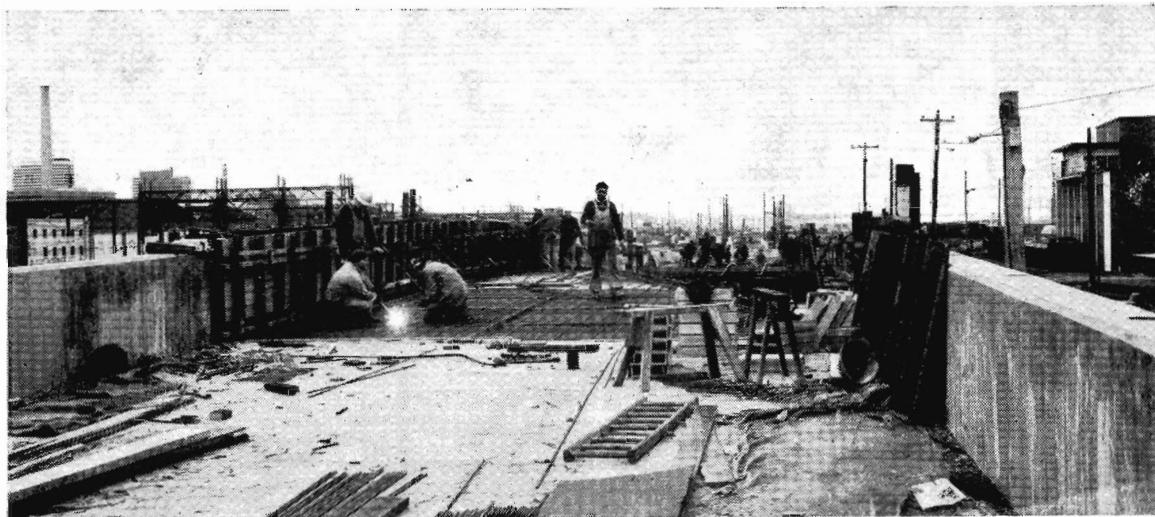
New wagons are being continually built to suit customer requirements.

● Potential capacity

With facilities of the type outlined, the Australian railway systems are, at existing traffic levels, only scratching the surface of their potential capacity for high-volume, long-distance transport.

New South Wales—the busiest system—has only one-quarter the average freight traffic density per route mile of the Class 1 railroads in America; the Australian average figure is only one-seventh as much.

We can accordingly look to the future with confidence that the natural growth in traffic following Australia's industrial expansion will enable us both to improve standards of service—better tracks, more frequent trains—and to further lower unit costs as spare capacity is utilized.



Work in progress at the hump in the new Melbourne Yard now under construction. At the extreme right is the West Tower.

V.R. RETURNED SERVICEMEN'S

JUBILEE DINNER

The V.R. Returned Servicemen's Section celebrated its jubilee this year with a dinner to country delegates at the V.R.I. Ballroom.

Guest of honour at the dinner was Major-General R. J. H. Risson, C.B., C.B.E., D.S.O., O.St.J., E.D.



Major-General Risson speaking at the dinner

Others present included the Hon. Vernon F. Wilcox, M.P., (Minister of Transport), Sir William Hall (State President, Returned Services League), Messrs. G. F. Brown (Chairman of Commissioners), E. P. Rogan (Deputy Chairman), and L. A. Reynolds (Commissioner).

Senior railway officers, past and present members, delegates, and life members joined in congratulating the section on its achievements.

"We're entitled to have national pride", Major-General Risson said. "The greatest lesson I learnt on my

recent overseas trip was that Australia, today, is the best country in the world in which to live."

Referring to the work of the section, he said that while companionship was essential in war it was needed even more in peace.

"Some of you have grown old in years, but you are also old in community service, and wiser in your later years" he added.

First meeting

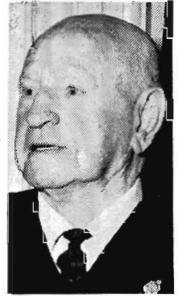
The inaugural meeting of the V.R. Returned Servicemen's Section was held in April 1919, and attended by 16 railwaymen. Since then there have been 23 presidents and only five secretaries.

Office bearers are elected from a cross section of railway staff, from junior grades to supervisors; at one stage one delegate was a nursing sister employed in a workshop.

The section has an outstanding record of achievements, both within the section and in supporting other similar organizations. For example, about \$13,000 has been obtained for Legacy.

The oldest member at the dinner, Mr. Vic Hosking who will be 88 this month, retired in 1947

as a toolsmith at Bendigo Nth. Workshops. Mr Hosking was first country member to receive a life membership of the section. It was given for his help to other railwaymen during the depression. He had a few fowls and two cows, and shared eggs and milk with other workers.



Mr. Hosking

Mr. Hosking lives at Bendigo where he is a life member of six organizations. He still enjoys a game of bowls, and for the past 33 years has been Father Christmas for the Bendigo subsection's children's Christmas party.



Some of the guests at the dinner.

Eighty-year-old Mr. Jack Allan was another veteran at the dinner.

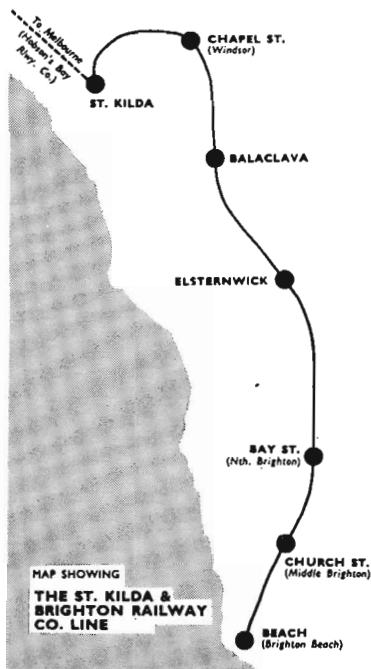
He began in the railways as a cleaner at North Melbourne, graduated to a driver, and later became an electric train driver. He would "do the same again", he said.

Mr. Allan recalled how, on Armistice day, 1924, he was driving a mixed train, and at 11 a.m. between Cannie and Quambatook he stopped the train, stepped to the ground, and played the Last Post on his bugle, as the passengers stood in silence. The incident was reported in the *Bendigo Advertiser* and, said Mr. Allan, contrary to what he had expected, he was complimented by the Department.



Mr. Allan

VICTORIA'S SECOND LINE



Electric trains ran for the first time on the St. Kilda line on August 31, 1919—half a century ago.

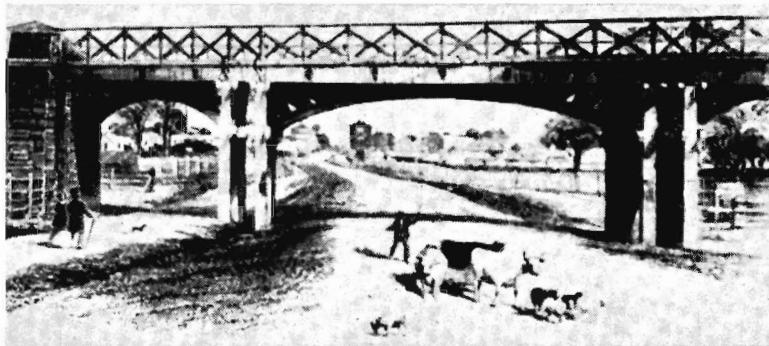
The St. Kilda line was the second completed in the big programme of converting the entire metropolitan network to electric traction. Three months earlier, the Essendon and Sandringham line had been converted to provide Australia's first electric train service.

The St. Kilda line was built by The Melbourne and Hobson's Bay Railway Co. and opened on May 13, 1857. It was Victoria's second railway line.

During the building of the line, Emerald Hill (now South Melbourne) Municipal Council took advantage of a clause in the Hobson's Bay Act of Incorporation which forced the company to erect bridges over the line where necessary for public use. There were many legal battles between the two factions until the Government ultimately amended the Act.

The great flood of December 14-15, 1863, suspended service to St. Kilda for six days. Access to St. Kilda was by travelling on the Melbourne Railway Co's line to Windsor and from there by road; Emerald Hill people used boats.

There were three stations on the



Railway bridge over St. Kilda Road.

line: Emerald Hill (now South Melbourne) opened September 1858, Butts (now Albert Park) November 1860, and St. Kilda. Middle Park station was built in later years.

However, Victoria's railway pioneers had plans that might have taken today's Sandringham line trains via St. Kilda instead of their present route.

In 1857, authority was granted to the St. Kilda and Brighton Railway Co. to build a line from St. Kilda to Brighton.

From St. Kilda the railway was built on a wooden viaduct 400 yards long over a swamp, then by a 102 ft. long timber bridge across St. Kilda road.

This 1¼-mile long section between St. Kilda and Chapel Street, Windsor, was known as the "loop" and cost \$100,000. The single line was laid with double headed 75-lb. rails in cast iron chairs and secured with oak keys and tree-nails.

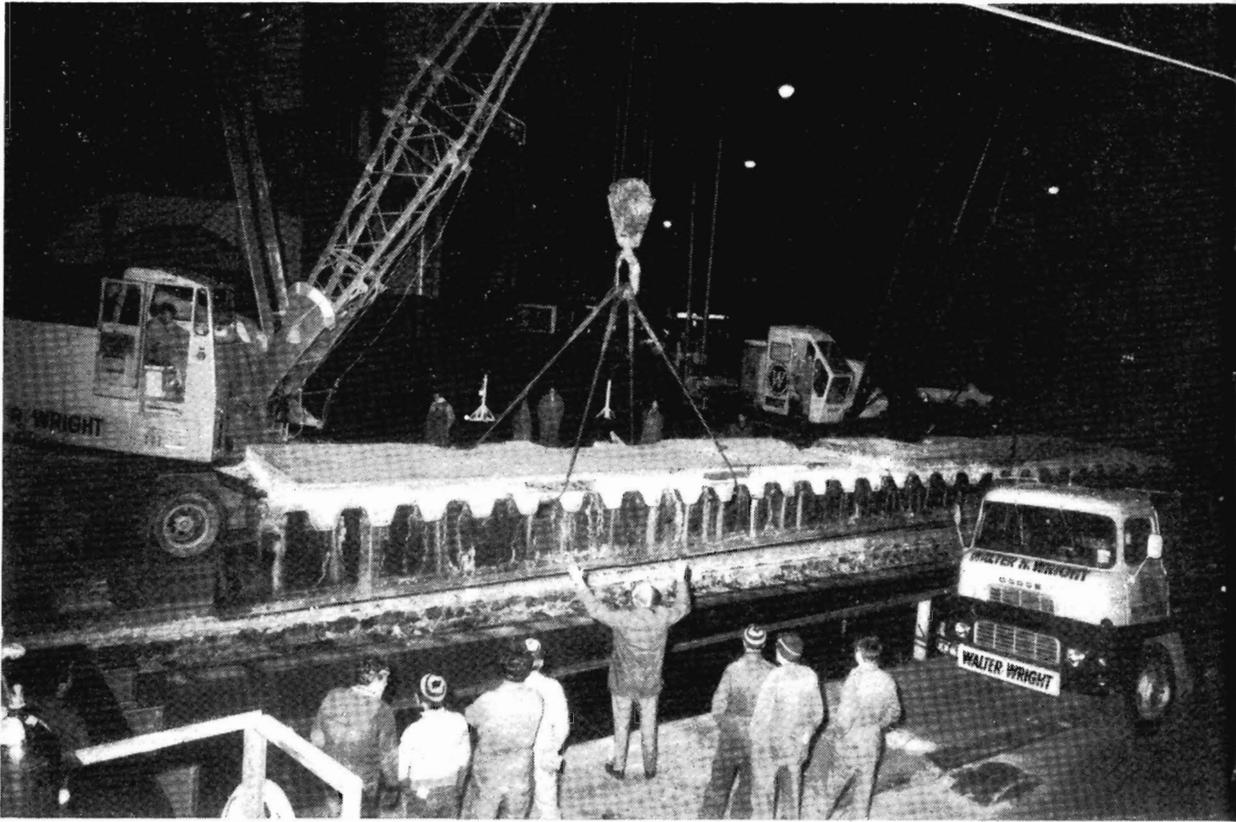
A "sumptuous cold collation" at Brighton Beach served to guests making the trial trip on December 3, 1859, was reported as "a very modest affair"; in contrast to the banquet and sports meeting arranged by the contractor, two weeks later, for the 300 workmen when a "drayload of ale and porter was consumed".

The line from St. Kilda to Brighton opened for passengers on December 19, 1859, and stations were at Chapel Street (now Windsor), Balaclava Road (Balaclava), Glen Huntly Road (Elsternwick), and Bay Street (North Brighton).

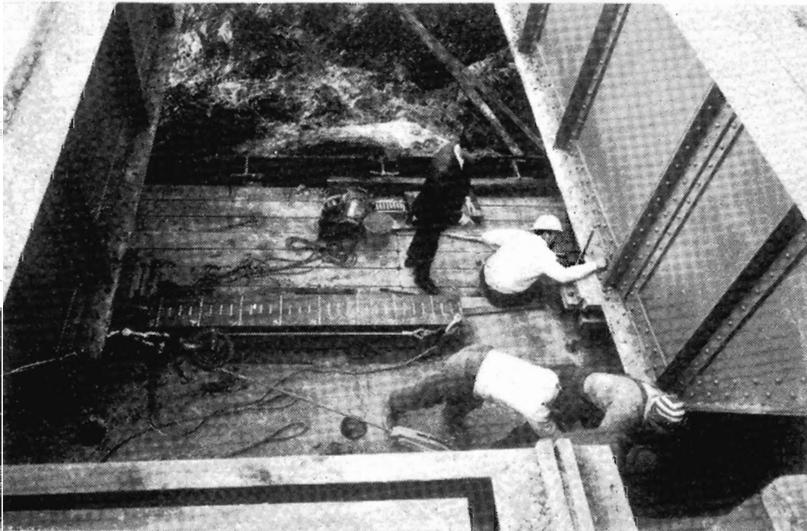
In 1862, when the Brighton and Melbourne Railway companies arranged to pool resources and run trains from Princes Bridge to Brighton, travel over the Hobson's Bay line to St. Kilda was eliminated and the "loop" to Windsor became redundant; it was dismantled in 1867.



St. Kilda station 1860.



At Flemington Bridge during the week-end of August 16 and 17, spans of a new bridge to take the rail tracks over the Tullamarine Freeway were placed in position.



A span is being winched into alignment to complete the bridge. ▶

A train passes over the new bridge. ▶

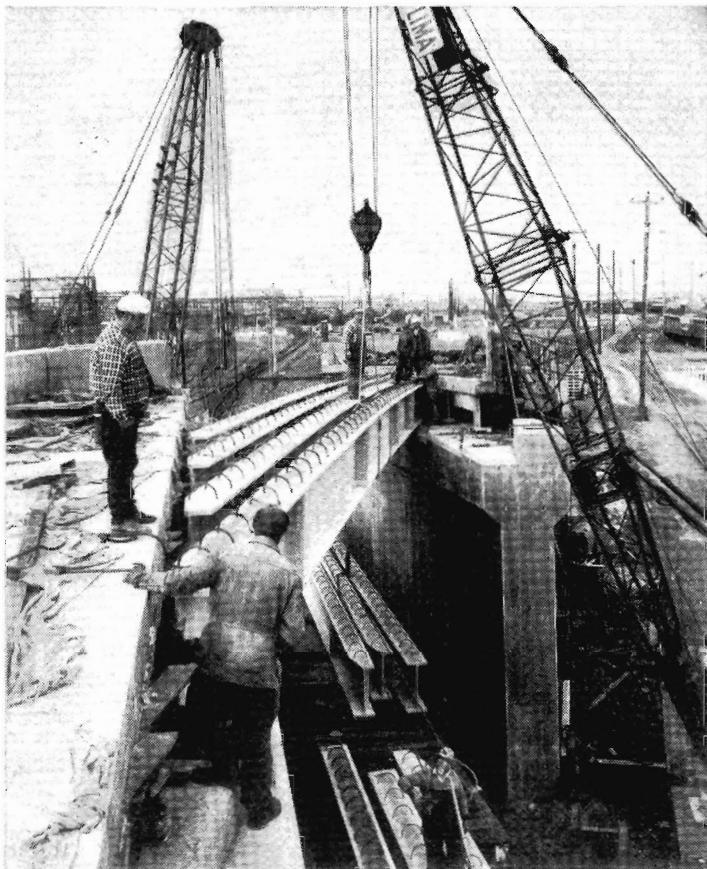


The week-end of August 2 and 3 saw completion of a further stage of the reconstruction of the Swanston Street bridge over the railway lines near Flinders Street station. While the bridge was closed to traffic, old girders, varying from 10 ft. to 44 ft. span, were replaced with new ones. The picture shows a section of old girder and roadway being lifted out.

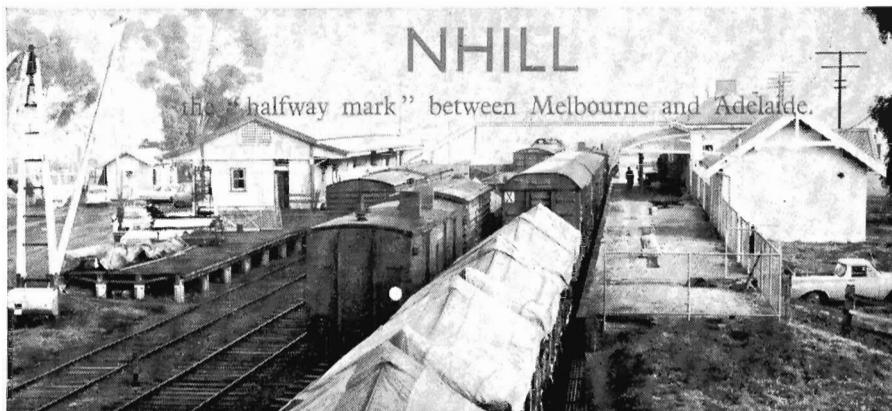
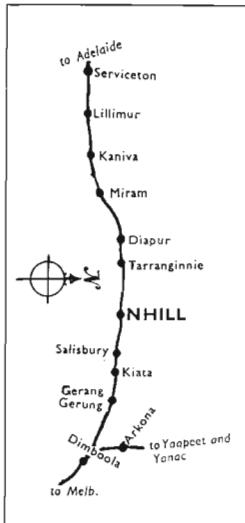
VIEWS OF NEWS

WORKS: Some spectacular jobs were carried out in the metropolitan area last month. Railwaymen worked around the clock on these jobs at Flinders Street, Flemington Bridge, and in the Melbourne Yard.

In the Melbourne Yard, after old rail tracks were removed, about 3,000 cubic yards of filling were placed and compacted before new trackwork was laid. This was an unusual and difficult operation necessitating an organizational approach similar to a factory's production line. Up to 80 men worked on the job during the week-end of August 16 and 17. On the hump section of the Yard, girders for the bridge structure over the underpass track are being lifted into position.



A busy station 248½ miles from Melbourne on the main Adelaide railway is



Busy Nhill station and yard.

A continuity of trains pass through Nhill and keep the seven Traffic Branch station staff busy. On an average about 10 freight trains a day—including intercapital express goods—pass the station; during the wheat season the number is about 20. In addition, "The Overland" stops nightly in each direction, and there is a Monday to Saturday railcar to and from Dimboola, connecting with the Melbourne passenger train.

The station is under the eye of Stationmaster R. J. Pitman, who also supervises Kiata, Tarranginnie, and Salisbury. Before coming to Nhill 4½ years ago, Mr. Pitman was at Woorinen, Stratford, Lalbert, and Koo-wee-rup. The two assistant

stationmasters are Messrs. J. F. Illig and D. D. Stephens. A father-son combination at Nhill is Repairer L. Schwarz and Station Assistant R. L. Schwarz.

"Most of the local stores use rail for the transport of their goods from either Melbourne or Adelaide, and the four hotels have their beer railed to Nhill", Stationmaster Pitman said.

Last financial year, Nhill had an inwards goods traffic of 9,247 tons and outwards 29,899 tons. Together with 1905 passenger journeys and the parcels traffic there was a total revenue of \$220,924.

Main traffic is wheat from Noske Bros. Pty. Ltd. and the Grain Elev-

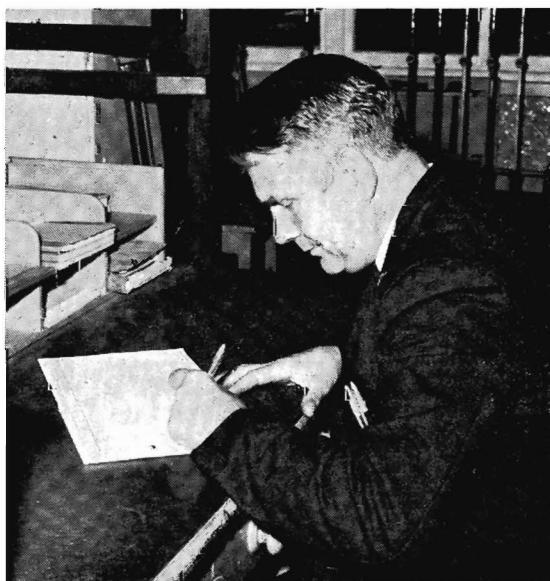
ators Board whose combined storages have a capacity of 365,000 bushels. Another storage is being built ready for the coming season. Two bulk oat sheds can hold 170,000 bushels.

An unusual consignment in this modern day of bulk movement is the large quantity of bagged wheat railed from the Noske mill. Bagged wheat is taken by train to Melbourne for shipment to the middle east and other overseas places that do not have bulk facilities. The empty bags are railed to Nhill for filling. Noske's also rail a wide variety of stock feed from Nhill.

Nhill has a plastic factory which sends plastic coats and trousers by



A.S.M. Illig withdrawing the staff for No. 25 rail motor.



Stationmaster Pitman making entries in the wagon book.



Station Assistant Schwarz places a washing machine, railed to Nhill, in the goods shed. Checking the consignment is Clerical Assistant B. W. Nicholls.



Gang 31 at Nhill (from left): Repairers A. Geier G. Cole, Ganger L. Schwarz, and Repairer N. Klemm.



Junior Station Assistant M. J. King arranging parcels.



Driver R. N. Crockett watches as Assistant Stationmaster Stephens passes the staff to Fireman J. Bakes.



Yard Assistant R. G. Morrow sets the road for the fast goods.

train to State Electricity Commission depots throughout the State. Other freight are field bins and augers, railed to South Australia and Western Australia, and about 5,000 bales of wool each year.

Recently, two wagons each containing three old-type horse buggies were railed from Nhill to Oakleigh. Apparently a collector of this type of carriage was able to find six vintage models in the district.

The name Nhill comes from the native "Nihill", meaning red clay. It is said that the name originally supplied by the blacks was "Nyell", meaning the abode of spirits.

The 63-mile railway from Dimboola to Nhill and the South Australian border was opened in January 1887 at a cost of \$514,112. Nhill's platform and station buildings were completed two years later by J. Paterson, and cost \$3,783.

DELAY EXPLAINED

I desire to express my appreciation of the courtesy of the officer on the Gippsland train which broke down at Nar-Nar-Goon on Monday night. His explanation of the delay was greatly appreciated. As an act of goodwill, its value can only be guessed, but various passengers expressed to me how greatly they had appreciated the train officer's explanation.

—Henry Hodges, J.P., Church Street, Maffra, writing to the Secretary

RAIL USERS SAY...

Irymple accident

In my capacity as Co-ordinator of the State Disaster Plan, I would like to express my sincere appreciation of the ready co-operation given and the effective work carried out by the Railways Department at the scene of a collision between a passenger train and a fully loaded petrol tanker at Irymple on April 22, 1969.

The prompt response and services rendered by the personnel concerned were greatly appreciated.

—N. Wilby, *Chief Commissioner, Victoria Police, writing to the Chairman*

Mildura train

I would like to highly recommend the behaviour of Conductor 221 (Mr. S. G. McKie—Ed.) on the Mildura to Melbourne train.

I am a Mildura High School student and I found that he was most courteous and helpful, and that he answered all my queries politely and as accurately as possible.

—D. Prowse, *writing to the Chief Traffic Manager.*

Geelong

In a letter to the Stationmaster, Geelong, the secretary of the Ballarat Fish Acclimatisation Society expresses appreciation "for the good service we are getting with our trout consignments in plastic bags and cartons."

From cement company

One usually writes to a Government or instrumentality only to ask for something or to complain about some real or imagined wrong; however such is not the case on this occasion.

I wish to compliment you on the efficient service my company has received from the Victorian Railways.

While all Branches have always been most co-operative, I must make special mention of Messrs. Weeks and Chandler (Commercial Branch) and Messrs. Waterhouse, Bells and Gainor (Trucks Office). To these people, nothing is a trouble, and time and again they have gone out of their way to make life a little easier for my staff.

I would hope that my own staff are as highly regarded by their customers as yours are.

—R. P. Jennings, *Sales Manager, The Victoria Portland Cement Co. Pty. Ltd., St. Kilda Road, Melbourne, writing to the Chairman*

Missing suitcase

I would like to thank you and your fellow assistants for the concern and time spent in trying to locate my suitcase. By now, as you well know, I have it in my possession again. I appreciate your efforts in what seemed a fruitless task.

—J. Smith, *Myrtle Grove, Regent, writing to S.M., Castlemaine*

Efficiency

My son, aged 15, on his way home from school and football training, misplaced his bag containing sports gear on the platform at Princes Bridge. Heidelberg station staff made inquiries, resulting in the bag being available within one hour. Only a small matter, but it reflects the efficiency and endeavour to give service.

As a Senior Captain with one of our domestic airlines and experiencing the problems which from time to time arise when dealing with the travelling public, I really appreciate the effort.

—C. Paranthoene, *Jalna Court, Eltham, writing to the Commissioners*

Missed train

I would like to thank you for the courtesy extended to me last Saturday when I missed the 8.30 a.m. train to Wodonga and you arranged to let my friends know at Bonegilla to meet "Intercapital Daylight" at Albury instead.

—L. J. Poynton, *Gipps Street, East Melbourne, writing to S.M., Spencer Street*

Caulfield

... We are deeply grateful to the Victorian Railways for the help and co-operation they have given us in transporting the milk powder from railway stations all over Victoria; we are especially grateful to the Caulfield railway station for the extra amount of work this involves ...

—(Mrs.) J. P. Ryan, *Hon. Secretary, Milk for India, writing to the Minister of Transport.*

Bank book

I would like to convey my very sincere thanks to you for returning my bank book. At the time, I really had no idea where I had lost it, but apparently it was either on Hawthorn station or en route to the city.

—(Mrs.) S. McConnell, *Manningtree Road, Hawthorn, writing to the Secretary*

Handbag lost

This is a small note of praise for the kindness shown by some of your employees yesterday.

I left my handbag on Flinders Street station, Platform 8, at 4.25 p.m.

Realizing this, I got out of the train at Ripponlea and asked the station assistant if he could ring Flinders Street for me. He was across the line in a flash and before I could reach the other platform the stationmaster (or was it a booking clerk) had rung and found that the bag had been handed in. I did thank them both for being so obliging, but think you also should be told.

The courtesy did not end there. I returned to Platform 8 where the men on the platform greeted me with smiles and told me they had taken the bag to Lost Property.

From here I retrieved it (with contents intact) from another efficient employee.

I am most grateful to everybody.
—Mrs. Nancy Eumson, *Banksia Avenue, Beaumaris, writing to the Commissioners*

Cheerful information

I called at your office to inquire about trains to Wagga, where we thought we must go to attend a funeral. As it turned out the journey was not necessary, but I wish to thank the officer on duty for the trouble he so cheerfully took to tell me what I wanted to know.

—H. K. Wood, *St. Laurence Park, Lara, writing to S.M., Lara*

Thumb caught

When my thumb was accidentally caught in a carriage door at Flinders Street station, I would like you to know that I received the utmost courtesy and first-aid from a member of your staff.

—J. G. Kidd, *Ridge Road, Kalorama, writing to the Commissioners*

Good parcels service

I should like to record my delight with your interstate parcel delivery service.

Last Tuesday, July 8, I delivered to McKinnon station a parcel addressed to my sister in Adelaide. The day after, I posted to her a letter telling her of this. On Friday I received from her a letter dated Wednesday 9, thanking me for her parcel.

However you did it—I thank you.
—L. Crawford, *Carlton Street, McKinnon, writing to the Secretary*

Film evening

The sixth annual railway documentary film evening, entitled *Railways Here and There*, will be held by the V.R.I. in the Institute ballroom, Flinders Street, on Wednesday, October 1, with two sessions—at 5.10 p.m. and 8 p.m. Like the others, it has been arranged by Mr. K. C. Findlay, of the Public Relations and Betterment Board staff, in conjunction with the State Film Centre.

A non-technical family programme, it will run for two hours. All films are in colour, and four will have their first screening in Victoria and possibly Australia. The films, in order of screening, will be: *Problems and Progress*—a dramatic progress report on the new Victoria underground line (Britain) and its 11 stations; *Queensland Railways*—behind-the-scenes activities of the Queensland Government Railways; *Invisible Signals*—a German film on train and tram ATC operations in many countries, and *This Week in Britain*—a quick look at transport history and a glimpse of the future.

The second part of the programme will consist of *The Driving Force*—an award winning film on the transition from steam to new forms of motive power and its effect on British railwaymen and passengers, and *Subways in Japan*—a review of the extensive railway facilities in several Japanese cities. The question is posed—could Melbourne ever be like the Japanese cities?

Tickets for both programmes will be available from the V.R.I., 3rd Floor, Flinders Street station building.

Medical flavour

There was a pronounced medical flavour in the retirement farewell given to Mr. Frank O'Brien, a boiler-maker welder at Newport Workshops. It was a tribute to his achievements in first aid, a field in which Mr. O'Brien has been active for 42 years. In V.R. first aid, Frank obtained his gold medallion in 1935, and altogether holds 42 certificates, gaining his last one this year. An instructor in first aid, he won the V.R. Senior Individual event in 1943, and has been in the finals practically every year since. Frank joined the Footscray Division of the St. John Ambulance Brigade in 1927, and at present is its Superintendent; in 1951 he was made a Serving Brother, and in 1963 an Officer Brother. Although retired, Frank will still be instructing St. John classes and Rescue Squads.

Where are The Seychelles?



Miss Houareau

This is a question that Miss Marie Houareau frequently encounters when she tells people that she comes from The Seychelles. Miss Houareau came from those pleasant islands to Australia five years ago;

joined the Department in 1967, and is a typist in the Commercial Branch.

The Seychelles are an archipelago of about 90 islands in the Indian Ocean, to the north-east of Madagascar. With a delightful climate, they are deservedly popular, with tourists, a popularity that must surely have been enhanced when, in the last century, General Gordon (of Khartoum and Technicolour fame) decided that one of the islands was the site of the Garden of Eden. The evidence that convinced the General was the existence on the island of a particular species of palm tree that, he firmly believed, originally grew in that Garden.

Miss Houareau says she is very happy with Australia, although, at first, the winters did seem cold after the mild weather of The Seychelles.

Darwin-Birdum line

Mr. J. Y. Harvey, a member of the Australian Railway Historical Society, is writing a history of the Darwin-Birdum railway line. He is anxious to contact Victorian Railwaymen who worked on the line from 1939-45. Mr. Harvey's address is 8 Marian Court, Blackburn, 3130 (Tel. 89-9852).



Mr. O'Brien (left) receiving a presentation made, on behalf of the men in the shop, by Mr. J. H. Brown, Superintendent of Steel Construction. At right is Mr. C. Britt, the chairman of the function. Also in the picture are a skeleton, a set of verses, a couple of spare (rubber) lungs that, it was suggested, could be traded in for new ones; at the right is a dummy performing a most unorthodox heart operation and blood transfusion.

Ambition realized

When Mr. A. J. Capper was a lad porter at Box Hill in 1922, his ambition was to become its stationmaster. He achieved that in 1954, and, except for 15 months at Tottenham Yard, was Box Hill's S.M. until his retirement. Mr. Capper's family has a close association with the eastern suburbs; his grandparents came from England in 1860 and took up a selection in the vicinity of Mt. Waverley.



Mr. Capper

On his retirement, Mr. Capper was farewelled at a very well attended function in a local hall. In October, he will be leaving on a trip to Japan, and, on return, hopes to do a lot of caravanning in Australia.

'Plane lands at' Shops

Mr. Jack Finlayson, a welder who worked at Newport Workshops for nearly 51 years, told *News Letter* that, in the early days, the Workshops were not fenced, and he vividly recalls the day when an aeroplane developed engine trouble and caused some alarm by making a forced landing on the site of the present Foundry. In his younger days, Mr. Finlayson won a number of professional cycling races, and, in 1929, held the record for Australia's fastest mile, and also the 20-mile road record.

Increased bonus

The bonus payable to staff who obtain a first year First Aid certificate has been increased to \$5.

Dandenong S.M. retires



Mr. W. T. J. Wicks (right) who retired as stationmaster at Dandenong is receiving a presentation made by Mr. M. Pimm, assistant stationmaster, on behalf of the staff. Mr. Wicks had 49½ years service, and was at Dandenong for the past seven. His retirement plans include trips to W.A. and Queensland.

State footballer



Mr. W. E. Stokes, (left) Workshops Manager, Spotswood, presents a watch to Mr. Lambe in the Fitting Shop.

Mr. Alfred Lambe, who recently retired as a turner at Spotswood Workshops after 42 years service in the Way and Works Branch, played football in the back pocket for North Melbourne (1922-28) and Footscray (1929-31). He also represented Victoria in an interstate football match against Tasmania in 1931.

"North Melbourne's Charlie Hardy was the best footballer I have ever seen, and was the ideal rover" Mr. Lambe said when recalling his football days. "Hardy was a master

at the place kick and could accurately stack a ball 60 yards."

Starting at Arden Street in 1927, Mr. Lambe worked for many years in special gangs before transferring to Spotswood.

Mr. Lambe has no special plans for his retirement except looking after his 20 canaries and finches. "But I'll be barracking hard for North Melbourne and hoping they will win a place in the 1969 finals", he said.

Printing apprenticeships

Printing Week, which was held last month, serves as a reminder that there will be vacancies in 1970 for apprentices in the V.R. Printing Works. The vacancies are for three apprentice compositors and one apprentice letterpress machinist. The Department's Printing Works are in Laurens Street, not far from North Melbourne station. Further details can be obtained from the Employment Officer, Head Office.

SPRING FREIGHT

Early spring sees the start of rail traffic in flowers, shrubs and so on. Noteworthy among these items are fruit trees sent from the Goodman nursery at Bairnsdale to all mainland states. The grower, Mr. A. S. Goodman, says that this year, apples are again the most popular fruit trees. Jonathan is just ahead of Delicious, but that old favourite, Granny Smith, is still very popular. Other trees in demand by home gardeners are peach (J. H. Hale) and apricot (Trevatt). N.S.W. gardeners, however, particularly in the Orange district, are preferring cherry trees.

"My family has been railing fruit trees from Bairnsdale for 60 years", Mr. Goodman said. "Seldom has there been a complaint about the service".

Seeds are another spring freight item. Arthur Yates and Co. Pty. Ltd. expect to rail from Reservoir over a million packets of various types of seeds in the coming months. Bulk supplies are railed from the N.S.W. headquarters of the company to Melbourne. Early indications are that Victorian home gardeners will be growing from seed, more vegetables than flowers, this season.

RETIREMENTS

ROLLING STOCK BRANCH

Bruce, H. J., Morwell, 31/10
Cuce, G., Sh. Shed, 26/10
Derzsy, J., Newport, 23/7
Dooley, E. C., Sth. Dynon, 9/10
Horsfall, S. R., Ballarat Nth., 28/10
James, F. C., Ballarat Nth., 22/10
Jurkowski, A., Sh. Shed, 9/10
Killick, H. L., T.L. Depot, 11/7
Kotchoff, D. A., Sh. Shed, 25/10
Kritikidis, E., Jolimont, 6/8
Lindau, L. J., Warragul, 23/6
Mayne, R. R., Head Office, 27/10
Matheson, J. D., Wangaratta, 6/10
Richardson, R. T., Newport, 3/10
Scott, L., E.R. Depot, 18/7
Spinks, L. J., Newport, 31/10
Stapleton, G. T., Newport, 13/10
Stirling, H. A. R., Nth Melbourne, 2/10
Swift, A. J., Bendigo Nth., 4/10
Tomlinson, A. E., Newport, 21/10
Ward, S. A., Jolimont, 1/8

TRAFFIC BRANCH

Calway, W., Spencer Street, 7/10
Dowling, T. R., Glen Iris, 3/10
Gansberg, T. K., Melb. Goods, 8/10
Holland, E. C., Melb. Goods, 5/10
Howell, E. G., Melb. Goods, 1/10
Taborsek, E., Flinders St., 27/6
Taylor, A. J., Northcote, 9/10
Travers, R. H., Morwell, 19/10
Williams, G. R. E., Oakleigh, 14/10

WAY AND WORKS BRANCH

Atkins, J. H., Ararat, 9/7
Barr, H. E., Flinders St., 5/10
Evans, T. R., Colac, 13/10
Gnech, F., Laurens St., 18/10
Godden, W., Clayton, 23/8
Hall, C. H., Wangaratta, 2/10
Hanks, W. J., Spotswood, 8/10
House, A. R., Flinders St., 2/10
Huber, E. C., Spotswood, 17/10
Johnson, A., Glen Thompson, 11/7
Kerr, J. L., Mildura, 4/10
Kervin, T. C., Typewriter Depot, 30/7
Kick, E. M., Dunolly, 11/10
Kuzminski, J., Flinders St., 11/6
Lant, E. V., Nth. Melbourne, 24/10
Lonic, R. A., Nth. Melbourne, 6/8
Maher, R. J., Seymour, 2/10
Mason, H. F., Bendigo, 23/8
Nichols, W. G., Bowser, 15/8
Prokopczyk, J., Warragul, 6/8
Vertzonis, N., Spotswood, 15/7
Walters, E. E., Ararat, 6/10
Wilson, C. H., Bendigo, 23/10
Young, J. E. K., Korumburra, 24/10

ELECTRICAL ENGINEERING BRANCH

Wright, B. A., Lighting & Power Division, 29/8

REFRESHMENT SERVICES BRANCH

Halabi, T. (Mrs.), East Melb. Hostel, 6/8

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH
Bland, W. H., Jolimont, 6/8
Halliday, G. H., Head Office, 29/7
Jukes, W. R., Ballarat Nth., 6/8
Meadows, L. M., Bendigo., 21/7
Orpik, J., Newport, 28/7
Sanger, A. W., Nth. Melb., 18/7
Tirris-Mikhailidis, G., Newport, 19/7

TRAFFIC BRANCH

Gunst, D., Flinders St., 28/7
Harberger, L. B., (Mrs.) Yaapeet, 15/7

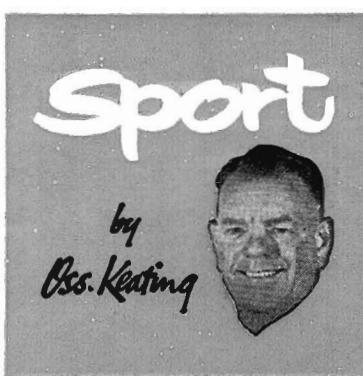
Harris, C. W., Melbourne Yd., 25/7
Jackivic, D., Glenroy, 11/7
Livy, F. J. L., Head Office, 10/7

WAY AND WORKS BRANCH

Juranic, J., Seymour, 6/8
Mazurek, K., Geelong, 8/8

STORES BRANCH

Phillips, D. W., Oil Store, Newport, 30/7



1969 Country Carpet Bowls Tournament

Again a good entry was received this year, and although this sport might not be quite so popular as it was a few years ago, I can assure you that those who are still playing it are just as keen and as skilful as ever. Centres represented were Ballarat, Benalla, Bendigo, Geelong, Korumburra, Maryborough and Seymour. An interesting fact that has emerged over the last year or two is that Benalla is now a strong challenger to the bigger centres in both the men's and women's sections. Winners in the ladies championship were Ballarat who just beat Benalla Gold 17 to 16 in the final. The men's championship went to Benalla Gold who beat Bendigo by only one point in a most exciting finish. In the consolation events Bendigo 3 proved too strong for Geelong in the ladies section and Ballarat 2 were able to hold off a strong challenge from Korumburra to win the men's section.

Our General President, Mr. M. L. G. McKenzie, assisted by the General Secretary, Mr. F. M. Mitchell, presented the trophies to the winners at the conclusion of play. Might I add that the help received from the large number of V.R.I. councillors who gave up their Sunday to assist the organizers contributed greatly to the smooth running of this fixture.

Golf

The V.R.I. Metropolitan Club went to Clifton Springs on Sunday, August 17, and played an 18 hole stroke event. Forty members, including ladies, from Geelong, Ballarat, Ararat and the metropolitan area participated, and the city players swept the pool. The winner was J. McMahon, with W. Farrelly as runner up. The ladies section was won by Mrs. P. Parkinson from Mrs. E. Crotty. Harry Hawke, who just a few weeks ago had a hole in one at Albert Park, showed that his approach shots are still first-class, by taking the trophy for the nearest the pin.

Hamilton V.R.I. Golf Championship

Hamilton Centre have now entered the golf circuit and it is pleasing to report that their first effort, on Sunday, August 10, at the Hamilton Golf Course, was most successful. The Hamilton ladies provided an excellent luncheon at the V.R.I. Hall prior to hit-off, and the 50-odd golfers who battled their way around this first-class lay-out at least started with the inner man well satisfied.

Perfect conditions prevailed, and some really good cards were returned, particularly by the winner of the A Grade scratch event, Allan Collins. Allan, as you know, is the hon. secretary of our Metropolitan Golf Club, and he had 78 off the stick for the 18 holes (handicapper please note). I believe that at this year's Country Golf Week our two *Ned Kellys* (Ike Dawkins of Benalla, and Norm Roberts from Geelong) are going to hand over both the horse and armour to Allan. They reckon he has earned both after this effort.

Other winners were I. Werner (A grade handicap), R. Deacon (B grade scratch) both from Geelong, G. Haby (B grade handicap) from Dimboola, and a local, R. Brown (C grade handicap) of Hamilton. The team event was won by a metropolitan trio, namely A. Collins, R. Wilson and J. Kennedy.

Council were represented by A. Collins, D. O'Donnell and A. Hargreaves who helped present the many trophies won during the day. The President of the Hamilton Golf Club, Mr. W. Grimes, extended a very sincere welcome to our Railway golfers. This co-operation by local clubs is much appreciated by our V.R.I. centres. Incidentally, the golfers were loud in their praise of the condition and facilities available on the course.

Now the Hamilton folk are already planning for next year's fixture which they hope to run a little earlier in the year.

Eastern Gippsland Golf Championship

The Eastern Gippsland Championship went to a local bloke this year, when Barry Williams of Traralgon returned a 117 off the stick for the 27 holes. Barry, who represented Victoria in the recent intersystem golf carnival, is a consistent winner at the Country Weeks, and would thoroughly deserve his win. Another Traralgon golfer, Bruce Gordon, won the B grade title, and Ted Ray, from Warragul, proved the best of the C graders.

The organizing committee were very pleased to see a large contingent of golfers from the Geelong V.R.I. Club competing. Perhaps it was only the lack of local knowledge that

prevented them from taking out a major trophy.

Norm Roberts (V.R.I. councillor) presented the trophies to the various winners at the conclusion of a very pleasant day's golf.

Basketball

As a prelude to the basketball carnival in October, the State squad has been playing some week-end games against provincial teams, the idea being to enable the players to get to know one another and to understand each others play. Their first match against a Geelong side that is recognized as one of the best in the Victorian country area, was a beauty, with Geelong just lasting long enough to win 71 to 69. Then, only a few weeks ago, we played at Ballarat a combined team from that city. This time, we came out on top winning 48 to 45. This was a great morale builder for the side, and they should arrive in Hobart a very fit and confident combination.

V.R.I. Chess Club

Winner of the V.R.I. Chess Club's B grade championship is Mr. G. J. Hietkamp, an electric train driver at Jolimont.

The main championship tournament is now in progress. The club is also taking part in inter-club competitions. So far, five out of 10 rounds have been played. In these rounds, the V.R.I. club has beaten Heidelberg (3-1), St. Kilda (3-1), Camberwell (3½-½), and Tivoli (3-1); round four was a bye.

Table tennis

The 1969 State championships, recently staged at the V.T.T.A.'s centre at Albert Park, proved the old adage—that if at first you don't succeed, then try again. Wally Lawrie, one of the best players produced by the V.R.I. Table Tennis Association for many years, finally broke through for a win in the singles championship after 11 successive attempts. Wal, who is a clerk in the Electrical Engineering Branch at Batman Avenue, had made the final on at least three previous occasions, but, until this year, could never quite win the big one. He is to be congratulated on his persistency and determination for, although he must have been disappointed many times, he never lost his enthusiasm or keenness and has played a big part in helping Victoria emerge as one of the strong table tennis States in the interstate carnival series.

In this year's singles title Wally defeated Mel Davey in the final and then, just to prove that this was no fluke, partnered Barry Smart to win



Mr. W. Lawrie, winner of table tennis championship.

the doubles championship from Mel Davey and Gino Roiter. In the country singles event, Kevin Smith of Horsham beat Gino Roiter, also from Horsham, and in the restricted singles (for unseeded players) Mick Carroll was successful when he beat Leigh Curson. The B Grade singles went to Bob Lengyel, with Arthur Wilson as runner up.

Popular Association President (and V.R.I. Councillor) Jack Crouch presented the L. J. Evans Shield and the individual trophies to the respective winners. The organising committee, with Graham Smith (also a V.R.I. Councillor) as hon. secretary, handled the tournament in their usual efficient manner, and were particularly happy with the record number of entries received. One aspect of the entries which caused the committee concern was the small number of metropolitan players who entered, compared with the number from the country. Perhaps the city slickers feel that their country cousins could be too good for them. Next year, as it is intended to stage the inter-system carnival here in Melbourne during August, the championships will be played either in May or June so that the Victorian team can be selected on current form.

Baseball—interstate representatives

No matter what sport you care to mention, I'm willing to bet that we, in the Railways, can produce someone who is top in his sphere.

In this respect, baseball, which is rapidly becoming a major sport in Australia, is no exception. As you read this the Australian Championship will have just ended at Straw Field, Royal Park. Included in the Victorian team were two railwaymen, Denis Smith, an engineer in the Machinery and Water Supply Division, and Allan Tanner a clerk in the Rolling Stock Branch at the E.R. Depot at Jolimont.

Dennis plays for Essendon (hooray for the Bombers) while Allan has been with Prahran for the past nine years. Their sporting careers have run along similar lines. Both earned interstate selection in 1968 and again this year; both were in the Victorian team that played against the Japanese side that visited this State earlier in the year; again both were selected in a representative team that played a combined American Marines side recently. Both helped their clubs to reach the A grade finals, Essendon beating Prahran in the semi-final and then being beaten by Melbourne in the grand final. Allan was good enough to win this year's batting average at Prahran, and Dennis finished third highest in the number of safe hits recorded in the A Grade competition. They would only need to produce this form to have a great chance of making the Australian squad that will compete in the Asian Series to be played in Manila during November, when teams from Japan, Formosa, Honolulu, and Korea, as well as Australia, will compete.

We wish both Allan and Dennis well in the interstate series and sincerely hope that by October they are packing their bags for Manila.

First Royal visitor

The first Royal visitor to Victoria was the Duke of Edinburgh, son of Queen Victoria and Prince Albert. He arrived aboard *H.M.S. Galatea* on November 23, 1867. To view the arrival, the Railways advertised "On Saturday, November 23, to view the maritime procession, trains will leave Melbourne every half hour for Williamstown. Fares 1st return, 1/6d.; 2nd return, 1/-". Many thousands of people made the journey.

* * *

Clunes

In a letter to the stationmaster at Clunes, the secretary of the Interknit Hosiery Co. Ltd. stated that at the recent general meeting of shareholders, a vote of thanks to the stationmaster and staff for help and collaboration during the year, was recorded.

VICTORIAN RAILWAYS

NEWS LETTER

OCTOBER



1969



V.R. CHAIRMAN COMMENTS ON

SHORT HAUL TRAFFIC

It is often assumed by many who are concerned with transport that all short haul traffic (under about 50 miles) is the exclusive province of the road motor.

This assumption is not correct.

Increasing traffic congestion in metropolitan areas makes road transport more and more costly. This applies particularly to long or heavy loads needing specialized vehicles. The rail, in many cases, can handle these loads at lower costs to the community.

Typical of this class of traffic was the railing of the long and heavy natural gas pipes from Somerton to Lyndhurst, a relatively short haul of 38 miles.

Other short hauls where rail has been chosen in preference to road are the transport in containers, of flour from Newport and Sunshine, and meat from Brooklyn, to Swanson and Appleton Docks for export.

Sand and screenings are examples of other freight suitable for rail cartage over short distances.

In the country, thousands of tons of briquettes are railed yearly from

Morwell to Maffra, Maryvale, and Warragul.

The New South Wales Railways runs a short haul service of unitized trains carrying containers between Balmain and the consolidation areas at Chullora and Villawood. Between the Campbelltown area and Balmain (about 35 miles), huge tonnages of export coal are carried in unitized trains of up to 2,500 tons; last calendar year, 2.6 million tons were hauled for this operation alone. Other short hauls of coal for export are made from the Newcastle field to the Newcastle waterfront.

Where sidings exist, no more handling is involved by rail than by road, and wherever the traffic warrants it sidings can be installed.

As road traffic increases, the advantage of rail transport for certain of these short hauls will become more apparent.

In many cases, the Railways are well able to handle this traffic at lower cost to the community and, at the same time, avoid increasing Melbourne's growing traffic congestion.

Gardenvale bridge hit again

Train services on the Sandringham line were suspended for half an hour from 6.45 a.m. on September 8, after the Nepean Highway railway bridge at Gardenvale had been hit by the load on an interstate road truck.

The road vehicle was carrying empty drums; the bridge was clearly marked with a sign showing that the clearance was 13 ft. 8 in. The Victorian limit for a loaded road vehicle without a permit is 13 ft.

Four trains on the Sandringham line were cancelled, which affected Broadmeadows trains, two of which were also cancelled. Fortunately, inspection of the bridge showed that no damage had occurred and trains were able to proceed at normal speed from 7.15 a.m. (See September *News Letter*, page 130)

Vandals fined

Vandalism on a Melbourne-Woodend passenger train recently cost three youths a total of \$314 fines and costs at the Melbourne Petty Sessions. Each was fined \$50 for wilful damage to railway property, and ordered to pay approximately the same amount for costs. The magistrate stated he considered sending the offenders to jail as vandalism of that nature was extremely difficult to detect, and when the offenders were detected it was his duty to impose a severe penalty.

New look for V.R. pamphlets

In the last decade, the Department's supply, quality, and range of instant information in attractive take-away form — its pamphlets — have been outstandingly improved.

Approximately 45 different pamphlets are available during the year, ranging from the tiny "luggage" (to fit into ticket envelopes) to the annual mini newspaper **V.R. News**. Most, however, are a standard 3½ in. wide by 8½ in. deep (that gives the most economical paper use for a variety of folds).

Long before shipping colsortia moved into containerization, the Department was telling customers how we could move their own containers, or provide our L.C.L.'s for their use, with a full colour pamphlet, titled **Rail. Pak**. Today's range covers employment, wagons, equipment, procedures, train services, fares, schedules, maps, bogie exchange, history, and Mt. Buffalo Chalet.

While more costly 4-colour pro-

ductions are generally used to meet specific requests, 2-colour pamphlets are printed for wide distribution to sell our services. To help put them in the hands of potential customers, metropolitan and main country stations have a pamphlet holder by the ticket window.

A new look is to be given the standard 3½ in. wide pamphlets that can be grouped in a series under the theme "Know your railways and how best to use them". It is believed that this approach will generate more interest in the subject and encourage people to keep the pamphlets for reference.

First to appear in the new 2-colour dress will be **All-lines tickets** and **MotoRail**, followed by **Interstate Services** and **Parcels**.

Two new pamphlets designed this year before the new series was conceived, cover railway land for lease and private sidings. Being prepared is another new pamphlet, dealing with the facilities and services at local metropolitan stations.

FRONT COVER

River diverted : At Heyington, the River Yarra has been diverted and is flowing in its new course below the rebuilt railway bridge. In the background can be seen the excavator that has cut the bank to enable the river to flow into its new course. At the other end of the diversion, another excavator had similarly cut the bank where the diversion joined the river. The job attracted an interested group of youthful spectators who lined the pedestrian footway on the bridge. The works were necessitated by the extension of the south-eastern freeway; the constructing authority is the Melbourne and Metropolitan Board of Works.

Railway inquiry

In the Budget speech last month, Sir Henry Bolte, the Premier and Treasurer, said that the Railways expected to lose \$13 million this financial year—accounting for almost three-quarters of the record Budget deficit.

Sir Henry told Parliament: "The deterioration in the railway accounts is a matter of concern, calling for a comprehensive review of the economics of the various elements of the railway system.

"However, we do not propose any adjustments to railway charges at the present time."

Later, outside the House, Sir Henry said he believed both fares and freights had reached an optimum level. Any increase at this stage was likely to be offset by a fall-off in business for the railways.

But Victoria could not afford to do away with its railway services, he said.

"Thousands of commuters are dependent on the Railways, quite apart from its freight services," he said.

"A review will take place in the near future.

"But we have got to get the right people to do it."

Sir Henry said that although Victoria needed the Railways, it was now a question of "to what degree the taxpayer can subsidise the railways".

"I believe the future of the service will mean being subsidised by the taxpayer," he said.

Sir Henry said it was essential to cut operating costs.

The State Government is expected to make an approach to the Commonwealth soon to get some of the revenue lost as a result of the wheat slump.

In his Budget speech, Sir Henry said the Railways expected to earn \$7.5 million in wheat freight this year—despite the fact that about 62 million bushels were being held in country silos.

The Wheat Board had already deducted rail freight from payments it had made to farmers for this wheat under the guaranteed price scheme.

"Thus, in effect, the farmer had already paid the freight on this wheat, but the money had not been received by the Railways," Sir Henry said.

"We are taking up this whole question with the Commonwealth authorities concerned, and we are hopeful it will be possible during the year to come to some arrangement to alleviate the financial embarrassment to the Railways of the present wheat position." (*Lionel Dunk in "The Age", 17/9/69*)

Slow but sure

Watching the painfully slow progress of the tortoise, few would realize that he must be one of the slickest PR operators outside Madison Avenue.

Just look at his record. Over 2,000 years ago, he shrewdly allied himself with the fast but rather brainless hare, and achieved literary immortality in one of Aesop's fables. Then he teamed up with Achilles and figured in a paradox propounded by the philosopher Zeno who produced an apparent proof that Achilles could never catch the tortoise.

And this untiring publicist crept into railway advertising after Mr. J. Leatham, of Bairnsdale, railed the season's first consignment of tortoises to Melbourne last month, for sale in pet shops.

Further supplies, up to 50 at a time are being sent at fortnightly intervals.

The tortoises are found in water holes and lagoons along the Mitchell River valley by Mr. Leatham.

September, he points out, is the time when tortoises begin to wake from their winter hibernation. Before the season ends, next autumn, Mr. Leatham will have travelled as

far as Maffra and towards Orbost for supplies.

The most popular sized tortoise has a shell of about 3 in. diameter, but Gippsland waters have many of nearly 9 in. They live for the first two or three years of their lives almost exclusively in water, with an occasional crawl on dry land. Hibernation takes place in soft earth under a small plant or weed. The tortoise buries itself up to the top of its shell before winter and emerges in September. Generally, those kept indoors as pets do not hibernate.

In their native habitat, tortoises eat mainly water snails and baby fish. A piece of meat as big as a pea, chopped into about 10 pieces, will feed a tortoise kept in captivity, says Mr. Leatham.

It is necessary for the tortoises to travel the 170 miles from Bairnsdale to Melbourne as soon as possible; the train is ideal, because the trip is completed in only a matter of hours, he added.

Mr. Leatham has a word of advice to punters who attend country shows and other places where tortoises are raced. "Put your money on the littlies; they're more energetic than the big ones."



Mr. Leatham and his son Henry, pack tortoises for the rail trip to Melbourne.

Concluding highlights of a talk given recently by Mr. G. F. Brown, Chairman of Commissioners, at a Seminar held in Perth by the Institute of Transport, Western Australian Section.

SPECIALIZATION

is today's function

OF RAILWAYS

Australia's railways can expect to see growth of traffic and a continuation of the process of specialization that has been the ultimate result of the impact on railways of the development of other forms of land transport.

Basically, the specialized job is mass transport, of either goods or passengers, between points of traffic concentration. Distance of haul is not as vital as is sometimes thought; if the conditions are right, rail can be the most economical haul for goods over quite short distances, as is proved by the new set-up for moving containers in trainloads to and from the Seatainer terminal at Balmain, Sydney.

"Mass" must not be confused with "bulk"; it is a wider term, and embraces, as well as the accepted bulk commodities like coal and wheat, heterogeneous commodities bulked together, in containers or vans, in sufficient quantities to warrant train-load movement.

Rail-road co-operation

Over the last 20 years, much rail traffic that was previously retail in character has been transformed into mass traffic by the increased co-operation that has developed between rail and road services. The bulk loading system, by which miscellaneous items of freight are collected by forwarders, loaded into rail vans for the line-haul portion of the journey, and then distributed by the forwarder at the terminal, represents the best combination of rail and road functions, each working within its most effective sphere.

The container, in its several forms, is a natural extension of this method that extends the scope of co-ordination to sea transport as well. The bulk rail van is, in the final analysis, simply a limited-access container.

A further recent development is the hire of individual wagons to a particular customer on an annual charge basis, it being the customer's concern to see that an even balance of loading is maintained and that the wagon achieves a rapid turn-round. So far, this type of service is being used mainly for palletized goods and small containers. This arrangement represents a return towards the original concept of railways in England in the 1820's; that is, a

means for wagons or carriages owned by various people to be joined together for haulage from point to point.

Specialization is the foundation of all economic progress, and in the Australian transport sphere the continued growth in co-ordination between the various media, and the increasing specialization that goes with it, can be only beneficial to the national economy.

So far as the railways are concerned, the process of specialization has involved not only the expansion of rail facilities where the conditions are right, but also withdrawal of rail where the conditions are wrong—that is, where the total traffic movement is too small to justify the overhead costs involved in maintaining a worthwhile rail service.

Both processes have been operating in Western Australia, where more miles of railway have been built, and more miles closed, than in any other State.

Effect on unit costs

The reduction in unit costs that can be achieved by the application of modern technology, the closure of poorly patronized branch lines, and an increasing density of traffic on main lines, can be clearly illustrated by statistics from Victoria.

From 1954-55 to 1966-67 the total contents ton-miles performed by the Victorian Railways increased from 1,426 million to 1,937 million, a gain of 36 per cent. In the same period rolling stock productivity, measured in ton-miles per ton of rolling stock capacity, went up by 27 per cent, and labour productivity, measured in ton-miles per employee on working expenses, by 38 per cent.

Money figures are more difficult to compare because of the continued decline in the value of money. But, the true decline in unit costs can be clearly seen from the fact that, while in the period under review the average railway wage in Victoria went up by 55 per cent, the cost per contents ton mile remained virtually unchanged.

In the same period, the average revenue per ton-mile fell by 4 per cent, explaining why increased productivity was not reflected in improved financial results.

Australian Railways co-operate

Since completion of the Albury-Melbourne standard gauge railway in 1962, there has been a radical change in the relationships between the seven Australian Government railway systems.

Before this, only South Australia and Victoria operated through (as distinct from connecting) train services and interchanged rolling stock, but with completion of the standard gauge line to Melbourne, followed by the introduction of bogie exchange, New South Wales and later the Commonwealth Railways, were brought into the sphere of daily rolling stock interchange. This year more than 35,000 wagons will be bogie exchanged at Dynon and Wodonga (Victoria) while traffic through the B.E. centre at Port Pirie (South Australia) is continually growing.

The subsequent major programme of standard gauge conversion and construction in Western Australia meant that all new general purpose rolling stock in that State had to be designed with possible future interchange with the other standard and broad gauge Systems in mind, and the Western Australian Railways became an active member of the wagon interchanging group of Systems at the end of last year.

The increased degree of interworking between the Systems has not been confined to those which can physically interchange rolling stock, and it was the initiative of the Tasmanian Railways that gave rise to the first combined rail-sea-rail operation, for the carriage of frozen products from Tasmania to Sydney.

The culmination of this growing spirit of co-operation has been the adoption of the title "Railways of Australia", and its symbol, as the means of bringing our unity of purpose before the Australian public. It is symbolic of this unity of purpose that the non-participating Systems gave willing consent to the "R.O.A." emblem being used for the new prestige trains which will shortly be operating into Perth.

The future

If Railways of Australia are to maintain their competitive position it is essential that their ability to act

in unison be pushed to the limits permitted within their political framework.

We must take, together, advantage of modern developments in every field and be prepared to go outside our own specialized field for information and advice. Typical examples of this thinking are the new hump yard being built in Melbourne, and the action taken in Western Australia to call in consultants to assist with several aspects of the design of the Kalgoorlie-Perth standard gauge line.

Australian railways of the future will vastly improve and be a more efficient transportation system, keeping pace with the economic and social growth of the nation. This gain in efficiency would be greatly enhanced if the railways were to be divorced from government budgets, given the ability to raise their own loan funds, and made responsible for their own financial destiny, being paid directly by governments for services, such as scholars', pensioners' and other concessions they provide as a community benefit.

Much more costly to the Railways than these concessions are, in fact, the country passenger services that

must be kept up to a reasonable standard of frequency and comfort irrespective of the steady increase in the number of private cars, and the commuter rail services that are essential to the continued existence of our major capital cities.

Because of the very nature of the latter services, and of the huge amounts of capital tied up in facilities that are idle for most of their life, it is impossible to run them at a commercial profit. But, if governments would realise that every dollar paid to a railway system to cover losses incurred on doing this job would save itself many times over in reduced demands on the public purse to do the same job by some less efficient means, it would be possible to meet the public's demand for fast modern public transport without destroying the financial stability of the railway systems that have to provide it.

In Victoria, taking suburban and country passenger services together, 44 per cent of our resources are needed to gain 33 per cent of our revenue, and the position is similar in New South Wales.

Federal Government aid

In the sphere of road finance, the Federal Government has accepted that the big cities, where most of

Australia's people choose to live, have peculiar problems that require special consideration. This realization must surely extend to its logical conclusion that special consideration must be given also to the problems of providing public transport in these cities. It is now quite some time since the Federal Government in the U.S.A. decided to refuse financial assistance to urban road improvements unless such improvements formed part of an overall public and private transport plan.

Conclusion

The responsibilities of the Railways in this highly competitive age are the ability to manage and to sell their product of moving people and material.

The developments that have taken place in transport in Australia since the war have gone a long way towards evolving an overall system in which each particular mode is doing the work for which it is best suited. This will ensure that we are all kept on our toes fighting for that traffic which is "rightly" ours or trying to take something from the other fellow; at the same time working, in the long run, towards even closer co-operation and co-ordination.

RAIL USERS SAY . . .

Geelong

As I use your railways frequently, I feel I must write and tell you how much I appreciate the courtesy and helpfulness of the staff at the Geelong railway station. It makes travelling a pleasure to receive such excellent treatment.

I also send parcels by train to my family in Melbourne. It is a pleasure also to deal with the parcels office staff. One young man, Richard Sloane, (I asked for his name) is very efficient. Nothing is too much trouble for him . . .

Mrs. M. E. Joksmovic, Orchard Street, East Geelong, writing to the Commissioners

Hostesses

While travelling from Melbourne to Harden, N.S.W., and back on *Intercapital Daylight* I was unable to visit the buffet car owing to a knee injury. I should like to commend the efficient help rendered by your hostesses on both occasions. Their courtesy, kindness and great help were very much appreciated.

I was very impressed by Mrs. Cheryl Calf who helped me on the return journey.

Her unobtrusive manner and

friendly approach were most appreciated. Not only did she bring me afternoon tea, but served me with dinner, and helped me in every way.

Then, as the train neared Spencer Street, she took care of my luggage and helped me along to the door, and did not leave me till she handed me and my luggage to my sister.

Congratulations to the Department on the neat appearance, courtesy, and efficiency of these girls.

Miss Mavis Pettitt, Alexandra Avenue, Canterbury, writing to the Secretary

Williamstown

I have been travelling on the Williamstown line for more years than I care to remember, and this letter is long overdue. I write to draw to your attention the high standard of service that, for many years, and most certainly at present, passengers have received from the railway staff at Williamstown. They are always courteous, cheerful, and helpful, and to anyone who does not take these things for granted they give a fine impression of a service that receives more adverse publicity than it perhaps deserves. *A. J. Payne, Osborne Street, Williamstown, writing to the Chairman*

Southern Aurora

I have just returned from overseas and had to travel from Sydney to Melbourne—I travelled by train and am so glad I did.

Along with a lot of other people I had an image of train travel as noisy, uncomfortable and generally something that was to be avoided at all costs. What a pleasant surprise when I boarded *Southern Aurora* and found myself in the lap of luxury, I was able to stretch out and enjoy a full night's sound sleep in almost complete quiet. The seating accommodation for daylight travel is better than my lounge chair at home (almost) . . . *David Street, Alice St., Clayton writing to the Commissioners*

Flinders Street

I wish to convey my thanks to the people concerned (First Aid staff—Ed.) who assisted me in a recent accident at Flinders Street station, and who gave such prompt, courteous and expert help.

Miss Margaret Cooke, Benalla, writing to the Secretary

Michael Robbins wrote *The Railway Age* (Penguin, 65c) for two types of people: for the teacher or historian who may have forgotten the revolutionary impact that the advent of the railway had on the nineteenth century, and to show employees of the transport industry the historical background of their activities.

Although Robbins traces the railway back to 1556, he concludes that the Liverpool and Manchester in 1830 was the event that opened man's eyes to the annihilation of distance that could be accomplished by the railway, and thus to the potential economic development that a railway system could produce. Such an awakening was the work of "George Stephenson and the Great Builders"—men who began with a copious endowment of character.

In the succeeding years, a railway network was established in England, and Europe, at a rate seemingly foreign to our level of industrial expansion today—"The Victorians who created the railways look like a race imbued with some demonic

energy."

Railways were built by three schools, the British, the Continental, and the North American traditions. The British tradition spread throughout the Empire, but not always in harmony with local wishes—hence Australia's gauge problem. The nature of the development of North America gave rise to another scale of construction, using less labour and materials than were available elsewhere. Of recent years the North American tradition has spread over much of the world, especially during and after the Second World War.

Robbins concludes his work with some thoughts about the possibility of a New Railway Age, where the railway must adapt itself to its new modern environment, and discusses a plan for railway success in the future. This book can give a railwayman much insight into his occupation and its historical development, as well as the future impact that his organization may have on society. (Reviewed by Gary Bettison)

RESERVATION BUREAU CHANGES

The Central Reservation Bureau will soon move to new quarters in the basement of Head Office, almost directly under its present location on the ground floor.

The new room, 84 ft. long with an average width of 23 ft., has been formed by the removal of partition walls between several rooms. An internal stairway with fireproof door has been built to connect it with the other sections of the Train Services Division on the ground floor just above.

The new room will provide adequate space for the 50 per cent increase of staff needed for peak periods, and will enable improved methods of operation and communication to be installed.

A new feature will be a specially designed circular desk, 14 ft. in diameter, at which 12 clerks will sit, each with his own telephone call indicator and switches. At the centre of the desk will be a rotating rack holding more than 3,000 booking diagrams. The rack will be in continual rotation, driven by an electric motor, so that it will not be necessary—as it frequently is now—for a clerk to leave his chair to obtain a diagram.

There will be individual auxiliary desks for six more clerks required for special traffic.

Mechanical ventilation will give frequent changes of air, while sealed windows and sound absorbing ceiling tiles will eliminate much noise. Lighting will be of a high intensity, and evenly distributed.

Communications

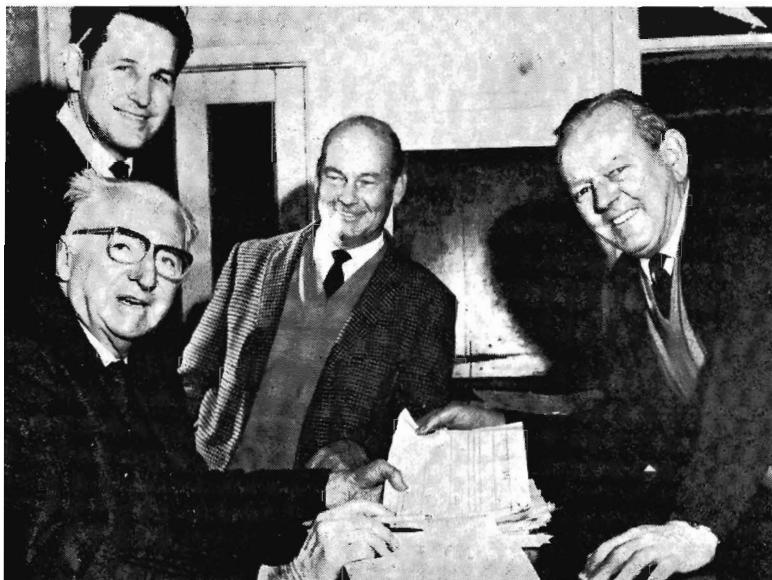
New and improved communications will considerably facilitate the work of the Bureau. Internal telephone facilities will give complete inter-communication between all the staff in the Bureau. This will expedite the handling of special bookings and peak traffic.

It will be possible to answer any call from any desk in the room. This will obviate one clerk being overloaded with calls while others are available to answer them.

All incoming calls will be answered strictly in the order in which they are received. There will be room for 24 calls in the queue.

Each clerk will have in front of him an indicator panel showing the number of calls waiting to be answered. The call at the head of the queue will be shown by a flashing light.

RAIL-ROAD CARRIER

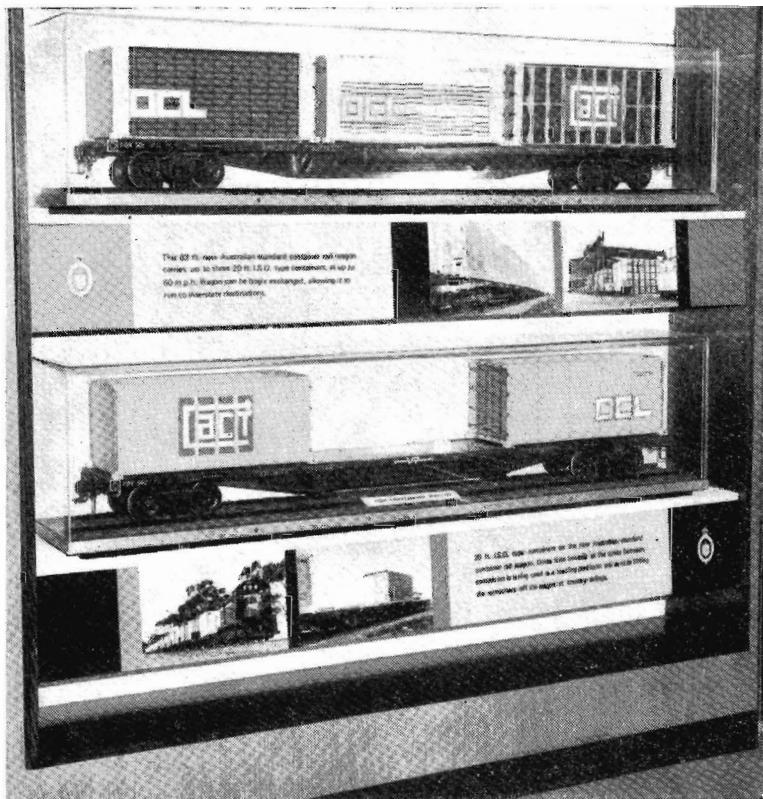


In these days of take-overs, mergers, and other forays made by the empire builders of the business world, it is quite a change to find some stable business that has been carrying on for generations. Such—a correspondent points out—is the firm of John Richards & Son, general carriers at Warrnambool. The firm has had a Richards in it from the time it was founded, and has carried goods to and from Warrnambool station for nearly 80 years. Many of its clients have been with the firm just as long. Recently, Mr. T. Richards retired from the firm, but the old name still has a place, as the new company is trading as John Richards Pty. Ltd. In the picture, Mr. T. Richards (lower left) is shown at Warrnambool station with the new proprietors, Messrs. P. Cook (top left) and M. Green (centre), in discussion with the goods clerk Mr. F. Selkirk (right). (Photograph: "Warrnambool Standard")

THE EAST-WEST STORY



Cameramen from the Commonwealth Government News and Information Bureau put the V. R. exhibit on film before opening day.



New 1/12th scale models of container wagons were on display for the first time at the Royal Show.

This year, the V.R. exhibit at the Royal Show featured *The East-West Story*, showing how the Victorian Railways play their part in moving passengers and freight from the eastern States across Australia. And the part is much bigger than many would think. Almost 700,000 tons of freight move between Victoria and South Australia, Northern Territory, and Western Australia each year. This is almost 150,000 tons more than the combined traffic of Queensland and New South Wales to and from South Australia, Northern Territory and Western Australia.

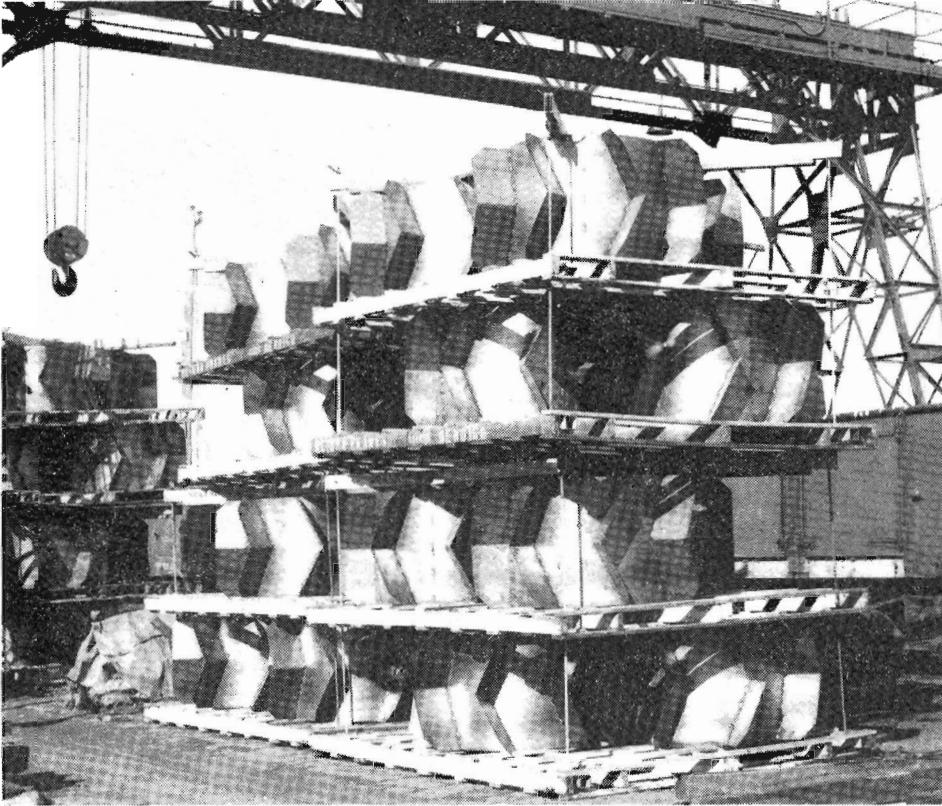
Other attractions at the exhibit included 1/12th scale models of rolling stock, free pamphlets, free identity discs for children, and the advisory section staffed by Commercial Agents.

This year's Show saw the 10th anniversary of the Department's model railway.

From a simple table-top layout in the exhibit at the 1959 Royal Show, when two trains ran (one on each of twin tracks), the model railway, with its replica rolling stock only 1/120th actual size, has grown to one of the largest of its kind in the world. Ten trains, automatically controlled, run through a landscape layout double its original size.

The railway's 278 items of rolling stock include 21 locomotives and 160 freight wagons.

VIEWS OF NEWS



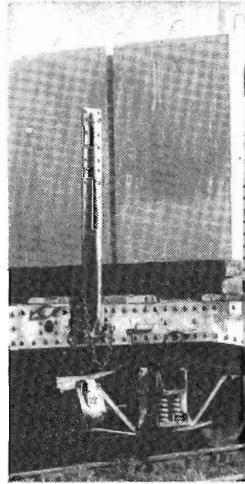
▲ **WATER WHEELS:** These Dethridge water metering wheels have been made by the State Rivers and Water Supply Commission at Bendigo, and railed to Melbourne for galvanizing. Afterwards they will be sent to irrigation districts throughout the State, for use by local farmers. About 2,000 are made annually.



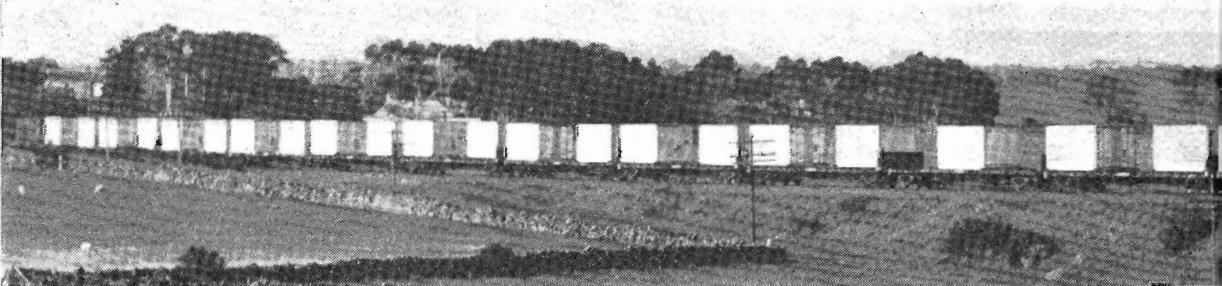
WEED POISONING
on the standard gauge

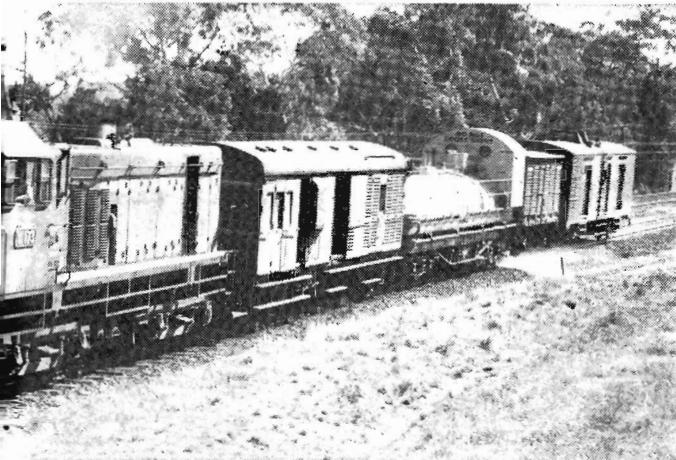


▼ **IN MELBOURNE**
They weigh 4 to

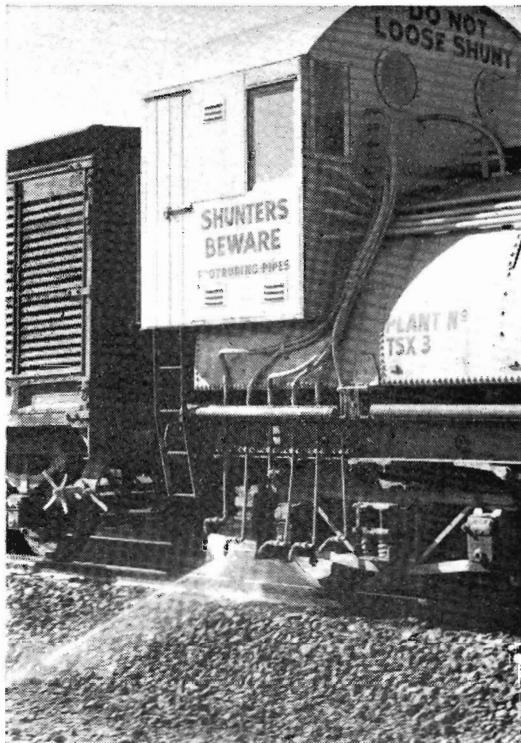


▼ **A CONTAINER TRAIN** is shown at Lethbridge, on its way to Mildura where it was loaded with dried fruit for export.

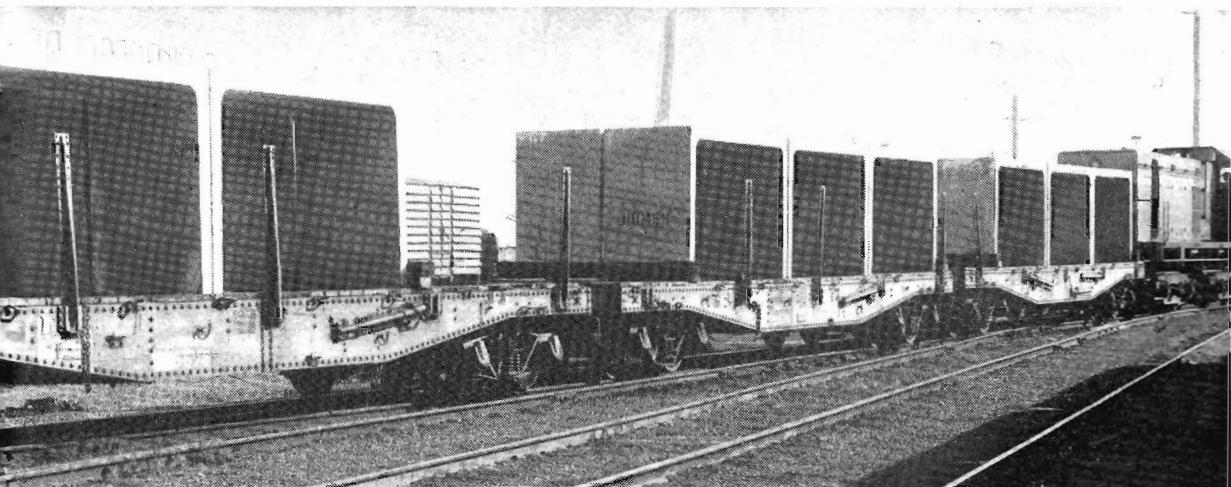




New South Wales Railways weed poisoning train recently operated in the Blue Mountains. Pictures show the complete train near Broadford, and a close-up of the spray equipment.



WARD concrete box culverts are on their way from Westall to Wangaratta. Each, are 10 ft. 6 in. long by 6 ft. 6 in. high, and are loaded on specially prepared wagons.



The British Institution of Mechanical Engineers held an international congress to consider guided land transport during the next 20 years, the most likely development of conventional rail systems, both between and within cities, and the possible introduction of completely new systems of propulsion, suspension, and guidance. Eighteen papers were presented by experts from Britain, Canada, France, West Germany, Japan, and the U.S.A. The papers were summarized by *Railway Transportation* as follows :

The great advantage of a railway over air or road transport is its ability to carry passengers direct from city centre to city centre without delays in traversing suburban areas. Nevertheless, if railways are to remain competitive in 1989, all-in average speeds of at least 80 m.p.h. over 100 miles and 110 m.p.h. over 300 miles will be essential.

For freight, today's fastest passenger schedules will probably be appropriate, although here the greatest scope for faster transit lies in reducing delays at terminals by containerization and advanced mechanical handling techniques.

British Railways' London-Manchester-Liverpool line, electrified with the 25-kV, 50-c/s system, offers the most intensive high-speed service at present available. It carries over 400 trains a day, the fastest reaching 100 m.p.h. and averaging 85 m.p.h. over 200 miles.

125 m.p.h.

With further improvement of the track and the introduction of continuous signalling within the driver's cab, maximum speeds of 125 m.p.h. on existing railways can be envisaged.

Moreover, pendular suspension or powered tilting of coaches—now on trial in Britain and elsewhere—may enable trains to negotiate curves at higher speeds than at present. However, modernization of existing railways is almost always limited ultimately by large-scale curvature of the route and passage through heavily built-up areas.

The 25-kV system itself is suitable for considerably higher speeds. On specially-built railways, speeds of 130 m.p.h. are already in regular use, and 150 m.p.h. is likely soon. Provided that multiple-unit construction is employed to minimize the current collected by each pantograph and to distribute the accelerating forces along the train, speeds well in excess of 150 m.p.h. are thought to be possible using current collection from an over-

head wire. A.C. voltages appreciably higher than 25 kV are unlikely to be employed because of the difficulties of providing electrical clearances and counteracting atmospheric pollution.

For speeds much in excess of 150 m.p.h. to be practicable, aerodynamic shaping of trains and aircraft methods of construction will be essential to keep down the power required, and fully automatic control will be essential for safety.

The interaction of air-pressure waves from opposing trains and their reflection from fixed structures will require thorough investigation, as will methods of insulating passengers from noise and sudden forces.

It is believed that an acceptable ride can be obtained at such speeds from steel wheels running on steel rails, but a limiting factor may be wear of wheels and track: tolerances will be very small and the cost of maintenance may be prohibitive.

Even at the substantially lower top speeds recently introduced in Britain, the cost of track maintenance is already very considerable, and the higher cornering speeds possible with pendular suspension will accentuate this problem.

In any case, since successive equal increments of speed require progressively greater power increments and yield progressively smaller time savings, the practical speed limit is likely to be set by economic rather than technical considerations.

ALTERNATIVE SYSTEMS

Diesel engines are unlikely to play any part in very-high-speed transport because of their poor power-to-weight ratios and unsuitability for continuous full-speed running, but gas turbines, without these disadvantages, may prove useful where a prime-mover carried on the vehicle is wanted.

The fuel cell—silent, an exceptionally efficient energy-converter, and

producing little atmospheric pollution—has attractions as a direct source of electricity, but experiments so far have been discouraging. Low-temperature fuel cells using aqueous acid or alkaline electrolyte and a hydrocarbon fuel are technically feasible now for railway traction, but would cost between four and eight times as much as traditional power units because of the expensive catalyst required to sustain the reaction at the electrodes. And whereas high-temperature fuel cells using, probably, a solid ceramic-oxide electrolyte at between 400°C and 700°C may eventually be competitive economically, they will not be technically practicable for at least 10 years.

Linear motor

The linear A.C. induction motor is a most promising development. Its main advantage for very-high-speed transport is an exceptionally high power-to-weight ratio resulting from:

(a) the 'rotor' element being fixed to the track,

(b) no restrictions on the motor's peripheral surface speed, whereas that of rotary motors is limited by centrifugal effects to some 144 m.p.h.

Other advantages are simple construction, independence of adhesion, and high thermal overload rating (losses being left behind in the reaction plate).

Experiments and economic calculations by British Railways have shown conclusively that, for maximum speeds around 100 m.p.h., the linear motor is not competitive with conventional A.C. electrification. At such speeds it consumes more electric power, and the cost of the reaction plate, whether steel or aluminium, is prohibitive.

Route-switching arrangements at junctions are costly also. The linear motor is therefore being considered

only for very high speeds—above 150 m.p.h.—on specially built tracks.

Hovercraft

Plans also exist for a tracked hovercraft driven by a linear motor and Hovercraft Development Ltd., a subsidiary of the National Research Development Corporation, has been granted \$2-million by the British government for construction of a full-scale test-track for such a vehicle.

An inverted U-shaped vehicle is proposed, straddling a concrete track of rectangular section. The first test vehicle will be about 50 feet long by 10 feet wide, will weigh 10 tons, and will be tested at speeds up to 300 m.p.h. on the 20-mile track. The main advantages of an air-cushion vehicle for very-high-speed travel are comfort for passengers; reduced wear of track and vehicle; and low bearing pressure on the track surface, which can therefore be relatively soft and so easily trimmed or filled to maintain flatness.

Another possibility is the *Magnarail* system, which uses the mutual repulsion of permanent magnets on the track and on the vehicle to provide complete levitation without expenditure of energy (unlike methods involving air cushions or electromagnets).

With the development of ferrites, (compressed ceramic materials based on ferric oxide), extremely powerful permanent magnets could now be mass-produced at an economic price. Propulsion and braking by a simple D.C. linear motor using the permanent magnetic field and a 10,000-A low-voltage D.C. supplied to conductors running beside the track-magnets is proposed.

RAPID TRANSIT

Most rapid-transit railways will probably continue to use third-rail electric systems at up to 1000 V D.C. during the next 20 years. The main developments will lie in partial or total automatic control and in higher rates of acceleration and braking.

The maximum acceleration that a standing passenger can reasonably withstand is about 4 m.p.h. per sec. This figure can be achieved with steel wheels on dry steel rails. Rubber-tyred trains have increased adhesion, but cost more in capital outlay and maintenance and consume significantly more power except where stations are very closely spaced.

In outer suburban regions where stations are two miles or more apart, maximum speeds of 80 m.p.h. and all-in averages of 50 m.p.h. will be practicable. Always, though, the advantages of higher speeds must be weighed against energy consumption increased much more than in proportion.

This energy consumption can be minimized by allowing a coasting

period between acceleration and braking (this also provides a 'make-up' margin after delays) and by siting stations on humps so that gravity assists braking and acceleration.

Faster loading

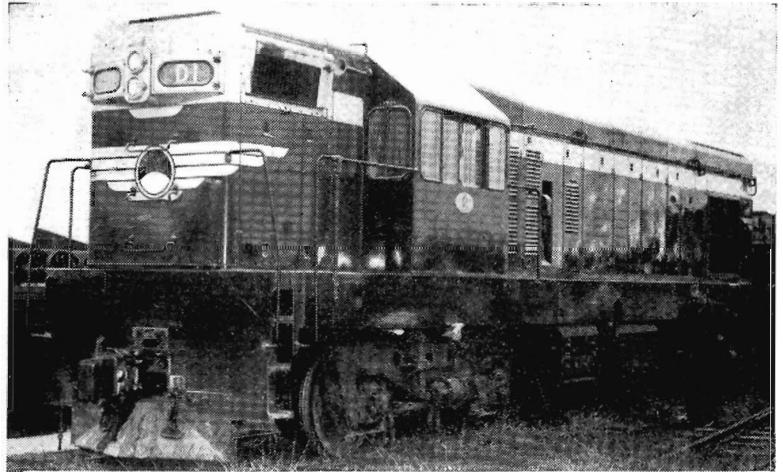
Potentially the most fruitful source of higher average speeds is faster loading and unloading of passengers at stations; but double-ender or multiple-platform stations greatly increase the cost of excavation underground, and there is also the implacable resistance of passengers to being treated as goods.

Monorails of either the suspended or beam-supported type are unlikely to be much adopted. They are expensive to install and maintain, have slow, inefficient route-switching arrangements, and at ground level or underground occupy more space than conventional duo-rails. Overhead monorails (which require lighter support structures than duo-rails) may however find occasional application for unbranched lines such as airport links, and in city centres where their ability to negotiate tight curves and steep gradients could be advantageous.

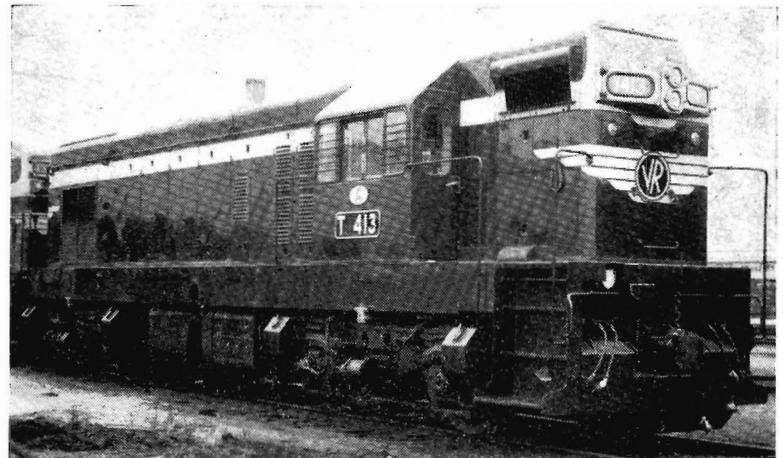
3 FT. 6 IN. TO 5 FT. 3 IN.

Diesel-electric locomotive D1 worked on the 3 ft. 6 in. track at Fyansford for Australian Portland Cement Ltd. until the private railway line between their quarry at Batesford and works at Fyansford was superseded by a conveyor system early

in 1968. When the new method of transport was proved to be successful, after 12 months trial, the locomotive was bought by the Department and delivery taken in May 1969. After workshop alteration, it joined the V.R. fleet as T413.



D 1 shortly after delivery.



T 413 after completion of alterations.

NEW SYSTEM FOR TRACKS

All six Victorian country districts have now started on the modern *production line* system for rail track maintenance. The last district to come under the scheme was Ballarat.

The new system, introduced in the Geelong district in 1967, provides for renewing sleepers and re-surfacing rail tracks by separate mobile gangs of up to 25 men attached to each district.

The gangs are being equipped with modern track maintenance machines to minimize the physical effort of staff, and increase the efficiency of the operation. Most of the equipment has been supplied.

Re-sleeping and re-surfacing gangs work progressively through their district, and it is intended to treat each main-line track and branch line once every three years.

Gangs live at camps in which sleeping and messing facilities are provided in converted railway carriages specially fitted to include some modern amenities.

The old-type disposition of track gangs is being re-organized to provide a fewer number of gangs for track patrolling and minor repair works only. A number of mobile utility gangs, of about seven men, equipped with motor transport and light mechanical plant, have been formed to carry out repairs of a heavier nature.

In re-arranging the track gang lengths, staff are being housed, where practicable, in towns which provide reasonable living conditions.

N. Z. TRAVEL

New Zealand train travel can now be booked at many Victorian railway stations by intending visitors, before they leave this State.

The booking facilities, which are reciprocal between the Railways of Australia and New Zealand Railways, apply also to railway-operated buses and ferries in New Zealand.

Bookings can be made six months in advance.

This travel facility will be of double benefit to tourists, as full travel arrangements can be made

before leaving home, and less ready cash will need to be carried on the trip.

The New Zealand bookings can be made at Flinders Street and Spencer Street stations, main country stations and at suburban stations handling interstate bookings, as well as at Victour and authorized travel agents.

On payment of fares, an authority is issued to tourists. On presentation of this authority at any one of a number of specified key railway centres in New Zealand, they will be issued with the necessary train, bus, and/or ferry tickets.

MORE TALLOW CONTAINERS

Another 25 portable bulk tallow containers will be built by the Department to cope with the increasing tallow traffic.

Each new container will have a capacity of 1,510 gal. (about six tons) of tallow and be insulated with polyurethane, encased in an external steel covering.

Tallow is loaded as a liquid at a temperature of between 130°F. and 155°F. Because it begins to solidify at about 110°F., insulation is desirable to reduce heat loss during transport.

The Railways have been moving bulk tallow for some years, and have now developed a satisfactory method of insulating the tanks to obviate reheating where long distances are involved. Results of experiments show that polyurethane-insulated containers reduce heat loss to about 10°F. over a 24-hour period, compared with 40°F. to 50°F. for non-insulated containers. Because of this, all existing tallow containers will be insulated.

Liquid tallow, an animal fat used in the making of soap products, is railed to Melbourne from Ballarat, Bendigo, Portland, Wangaratta, Warrnambool, Wodonga, and Colac.

GRAHAM GREENE ON TRAINS

Among the authors who enjoy train travelling is Graham Greene. In an interesting programme shown on Channel 2 last month, he referred appreciatively to the privacy he could have on trains, and added that he had completed one of his novels on a train and, on the same journey, begun another. The interview was recorded on the *Orient Express*.

SIGNAL ENGINEER'S VISIT



Last month, members of the Institution of Signal Engineers (Aust.) attended a meeting in Sydney where they heard the presidential address delivered by Mr. O. S. Nock, President of the Institution of Railway Signal Engineers (London). The Australian Institution has now been operating for 21 years. Mr. Nock, who is the Chief Mechanical Engineer of the Westinghouse Brake and Signal Co., had a flying visit from England, with his wife, not only for this particular occasion, but also to visit all mainland States of Australia, and inspect signalling installations. The picture shows Mr. Nock (right) discussing aspects of the V.R. centralized traffic control with Train Controller J. Thoms.

AMONG OURSELVES . .

One he didn't take

Many of the retirement farewells at the Electrical Depot Workshops (Spencer Street) have, over the years, been photographed by Mr. S. C. Whalley and published in *News Letter*. Recently, on his own retirement, he was given a farewell at the Workshops. Although, on that occasion, he didn't take the photograph, it was, however, taken by his son Len who is the Department's Publicity Photographer.

Photography, particularly of railway subjects with emphasis on steam, has been a lifelong hobby of Mr. Whalley, senior. He joined the Railways in 1941, and has worked at the Electrical Workshops for the past 21 years.



Mr. Whalley (*left*) receives a presentation made on behalf of his colleagues by Mr. W. B. Richardson, Assistant Distribution Engineer.

First aid competitions



Shown competing in the south suburban section of the district first aid competitions held last month are Sunshine No. 2 (*left*) and Electrical Engineering No. 3 teams. Competitors are (*left to right*): Sunshine No. 2—Messrs. J. Cochrane, E. Szoke, A. Ward (leader), N. Carroll, and T. Murchie (patient); Electrical Engineering No. 3—J. Holland, J. Vost (leader), A. Butt, D. Bishop, and R. Crosher (patient).

Wound in the lane



Receiving a retiring presentation made by Mr. L. A. Krausegrill (Distribution Engineer) on behalf of the staff, is Mr. H. Schober (*right*) a professional engineer at the Electrical Centre, Batman Avenue. Mr. Schober recalled that, when he began as an apprentice in 1921, at the Electric Light Station in Spencer Street (now the Electrical Workshops), they had to wind motors in a lane outside, as the buildings lacked the space to enable the job to be done inside. The Electric Light Station generated power mainly for local railway needs, using three turbine generators. Mr. Schober says he has so far resisted the call of the bowling green, but thinks he will take up the game in retirement.

Orchids and oratorio



Mr. Hicks reads proofs with Miss Marina Coombs.

If you happen to visit the V.R. Printing Works and notice a bunch of magnificent orchids on someone's desk, it's a safe bet that they came from the hothouse of Mr. Arthur Hicks, one of the compositors at the printery. A grower of orchids and other hothouse plants, for many years, he has a fine collection of over 300—mostly orchids, philodendrons and begonias.

Time has never hung heavily on Mr. Hicks's hands. He does a lot of church work; he is a lay preacher; makes tape recordings and shows films (often railway ones) for groups of all ages; and, for most of his life, has sung in choirs, including the Presbyterian Oratorio Choir. Another lifelong interest has been photography, but now it is mostly confined to making 35mm slides of flowers. Mr. Hicks has been with the V.R. printery for 17 years, and has also worked at the State Government Printing Works.

Privilege ticket authority cards for retired staff

Approval has been given for the introduction of privilege ticket authority cards for issue, on application, to retired railway staff who are eligible for privilege tickets in accordance with existing instructions. The cards will be used as authority for the purchase of privilege tickets for travel at half the ordinary fare on country rail journeys on the Victorian system.

This approval does not affect in any way the existing instructions concerning persons eligible for the concession. However, dependants of eligible retired staff are not to be included on retired member's authority cards, but must each be issued with separate cards.

The following conditions will apply:

- privilege ticket authority cards will expire at June 30 each year and will not be honoured after the date of expiry;

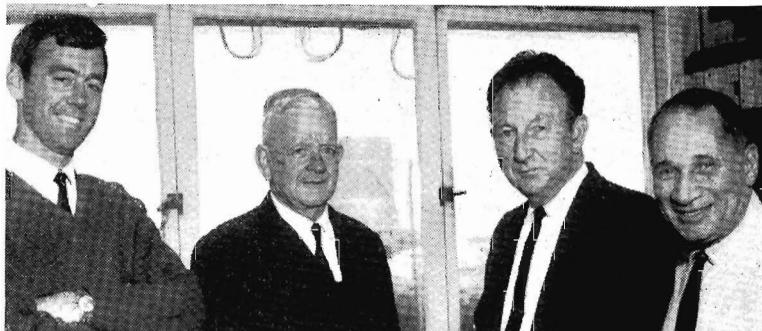
- authority cards may be renewed each year, after June 1, upon application to the Secretary for Railways;
- tickets may be bought only by the retired employee or a dependent who holds an authority card; the signature of such person will be obtained at the issuing station;
- the concession applies only in respect of journeys involving country travel;
- the existing conditions concerning minimum fares; change

of class; travel between Wodonga and Albury, and between suburban stations and stations on the Mornington line, will continue to apply.

V.R.I. band's success

The V.R.I. Brass Band has been taking part in a contest sponsored by the Australian Broadcasting Commission. Last month, at the final held in the Waverley Theatre, the V.R.I. band showed its excellent form by defeating the Mordialloc City Band to win the contest.

25 years at Geelong



Mr. Leo Evans, who retired recently, had worked at North Geelong for the past 25 of his 44 years in the Department. A first aid instructor, he had taken part in the V.R. competitions for 26 years, and, as Yard Clerk at North Geelong was on hand to apply his skill in first aid whenever it was required. A well attended farewell was given to Mr. Evans at the local V.R.I. In the picture (from left) are Messrs. D. McGann, C. Mahoney, L. Evans, and W. Tainsh.

RETIREMENTS

WAY AND WORKS BRANCH;

Brown, J. W., Flinders Street, 12/9
Cavuto, F., Special Works, 5/9
Christie, W. L., Head Office, 27/11
Collins, J. J., Laurens Street, 7/11
Comber, G. M., Caulfield, 28/8
Cooke, G. F., Head Office, 3/11
Douglas, W., Wodonga, 13/11
Heaphy, T., Flinders Street, 24/11
Jacovidis, C., Flinders Street, 14/11
Lewczenko, W., Flinders Street, 21/7
Mustow, D., Clarkefield, 14/7
Phillips, L. J., Murtoa, 19/8
Samon, J. W., Spotswood, 27/11
Schuravel, J., Warragul, 19/8
Smith, A. W., Benalla, 16/11
Voelker, A., Spotswood, 28/11
Wilde, C. S., Wangaratta, 7/9

ROLLING STOCK

Middleton, V., Newport, 25/8
Hardie, A. R., North Melbourne, 22/8
Wilding, C. E., North Melbourne, 27/9
Dennis, J. R., South Dynon, 22/7
Cheeseman, L. A., E. R. Depot, 28/7
Treloar, W. C., Geelong, 4/9
Hall, R., Newport, 1/11
Bartley, H. H., Jolimont, 6/11
Feeney, H. E., Jolimont, 6/11
Woinowski, J., Newport, 6/11
Gott, F. H., Shepparton, 9/11
Harding, P. H., Benalla, 10/11
Storace, E. E., Newport, 12/11
Hart, A. E., Newport, 24/11
Ray, A. R., Head Office, 26/11
Paul, J. T., Ballarat North, 27/11

Muller, J., North Melbourne, 29/11
Piggott, J. T., Ballarat North, 29/11
Porter, E. J., Newport, 26/11
Cooke, W. H., Warragul, 30/11

TRAFFIC BRANCH

Corbett, J. P., Dynon, 31/10
Horwood, A. P., Yarrowonga, 2/11
Otto, F. L., Head Office, 10/11
Lynch, E., Camberwell, 16/11
Phillips, J., Spencer Street, 25/11
Thomas, H. G., Benalla, 27/11
Kewin, J. E., Flinders Street, 29/11
Hivon, L. W., Oakleigh, 6/9
White, E. B., Spencer Street, 6/9
Leonard, H. T., Melbourne Goods, 1/9
McNeil, L. J., Melbourne Goods, 17/9

ELECTRICAL ENGINEERING BRANCH

McGorien, V. K., Distribution Division 12/11

REFRESHMENT SERVICES BRANCH
Hiscock, F.D., Advertising Division 18/4

STORES BRANCH

Moscov, C., Spotswood, 8/11

ACCOUNTANCY BRANCH

Sexton, J. G., Head Office, 19/9

Mayor

Last month, Mr. R. Dawes who has been a Williamstown councillor for the past seven years, took office as Mayor of Williamstown. Mr. Dawes, a production assistant in the Drawing Office at Newport Workshops, began his career at the Workshops in 1926 and, except for seven years at Ballarat Workshops, has been there ever since. His father and five brothers have all worked in the Railways. In his younger days, Mr. Dawes played senior football and cricket with Williamstown, and took part in V.R.I. wrestling.

Father and son in retirement

When Mr. S. L. (Stan) Bishop, telephone technician at Head Office, retired, he had the unusual distinction of joining his father in retirement. His father, Mr. E. S. Bishop, who is 92 years old, joined the Department about 1893 and, at the time of his retirement, was a signal adjuster.

Mr. Stan Bishop had been looking after the communication system in Head Office for 39 out of his 48 years in the Railways.

Booking clerks' dinner

The annual dinner of the Flinders Street booking clerks (and former booking clerks) will be held on Tuesday, October 28, at Concordia Hall, Ferrars Street, South Melbourne. Tickets (\$4.50 each, inclusive) are available from Messrs. J. Symons (auto. 1701), D. Shaw (1809), and J. McKain (1463).

* * *

The Victorian Railways' first timetable and fares schedule were advertised in Melbourne newspapers in 1859.

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

WAY AND WORKS BRANCH

McFarlane, R. V. C., Flin. St., 4/9
O'Farrell, J. T., Murtoa, 3/9
Rosmysl, S., Dandenong, 28/8
Wood, C. R., Bairnsdale, 2/9

ROLLING STOCK BRANCH

Orchard, R. B., Nth Melb., 23/8
Lucas, V. A., Nth Melb., 26/8
Castle, J. M., Nth Melb., 9/9
Vojtisek, B., Newport, 7/8
Cattonar, A., Nth Melb., 1/9

TRAFFIC BRANCH

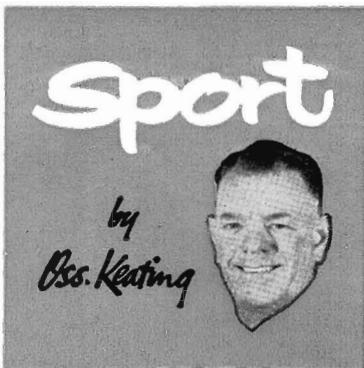
Ellis, R. J., Stony Point, 31/8
Fielding, H. A., Ripponlea, 11/9
Keer, L. S., Ballarat, 7/9

REFRESHMENT SERVICES BRANCH

Edwards, J., Mrs., Spencer St., 25/8

COMMERCIAL BRANCH

Wallace, A., Head Office, 1/8



1970 country weeks

The Commissioners have approved the following dates for the various V.R.I. country weeks during 1970 :

Bowls : February 16-20 inclusive, at the A.P.-V.R.I., Middle Park, and St. Kilda Greens. Entries close January 19.

Tennis : March 2-6 inclusive, on courts to be selected. Entries close February 2.

Cricket : March 16-20 inclusive, on turf wickets at Royal Park. Entries close on February 16.

Golf : September 14-17 inclusive, at the Rosedale Golf Club, Rosedale. Entries close August 17.

Golf

The round of metropolitan and country golf championships continues and, during the last month, the Dimboola V.R.I. conducted its annual Wimmera Championship on the Dimboola Golf Course. The championship was won by Alby Jack of Bendigo, who is having a great year. Alby who plays off a handicap of two, and is one of the longest hitters in the game, has won a number of amateur tournaments in the Bendigo area. The A grade handicap went to Alan Collins, from Melbourne, who collected his umpire trophy for the season. The way Alan is going, I reckon if he was entered in the Melbourne Cup, he would be worth having a few bob each way on. A couple of our old cricketing friends in Keith McPhee and Bill Thompson, both of Dimboola, took out the B grade scratch and D grade handicap events respectively. Altogether 70 men and 14 ladies played for the various trophies donated for the day. Messrs. R. Richards and A Collins, both councillors of the Institute, represented central council at this fixture, and assisted in the presentation of the trophies.

Country Golf Week

One thing you must say—at least we are consistent in the type of weather we arrange for Country Golf Week. This year was no exception, but at least the rain did hold off during the daylight hours, which meant that the competitors were able to complete four days of golf without getting a soaking—this being a decided improvement on recent years.

The welcome luncheon on the opening Monday was well attended, and our special guests included Messrs. W. Walker (Secretary for Railways), M. L. G. McKenzie, (General President of the Institute) and F. M. Mitchell (General Secretary). Mr. McKenzie extended a very sincere welcome to all players and hoped that they would have an enjoyable week of golf. Mr. Walker then declared the week open, and the first event of the 1969 fixture, 18-hole bogey, got under way. In spite of the heavy rain that had fallen during the previous week, the course was in tip-top condition, and some excellent golf was seen.

Eddie Brown, of Geelong, was the eventual winner, after turning in a card of 80 off the stick, which, with his handicap of 12, gave him a net 68. Unfortunately for Eddie, the handicapper was right on the job and immediately reduced his handicap to 11.

Probably the highlight of the afternoon was a hole-in-one at the 14th, by Alby Jack, of Bendigo. This, as you probably know, is the answer to every golfer's dream and it was good to see it happen to such a nice bloke. Tuesday morning's event was an 18-hole stableford, as well as the first round of the teams championship. The stableford trophy was won by Bill Thompson, of Dimboola, with a score of 39 points. It was Bill's first visit to our country golf week, although he has been a regular at cricket for some years, and I hope that he and the rest of the Dimboola gang attend for many years to come.

In the teams event, Shepparton beat Bendigo 2, Traralgon beat Dimboola, and Bendigo 1 defeated Geelong 2. Tuesday afternoon—a 9-hole stableford event—and this time Bob Morris, of Seymour, was successful with a score of 20.

In the teams semi-finals Geelong 1 beat Shepparton and Bendigo 1 defeated Traralgon. Wednesday saw the final of the teams championship being played between Geelong 1 and Bendigo 1. The odds-on favourites, Bendigo, ran out fairly comfortable winners, four games to one. Congratulations to Joe De Araugo, (captain and coach), Barry Miles, Alby Jack, Len Prior and Len Barlow, on a very fine win.

The 18-hole bogie trophy went to one of the real characters of our week in Ray Payley, of Geelong. Ray (or *Blue* as he is better known) has been coming down to compete for many years now, and this is his first-ever trophy. You good golfers better look out, *Blue* might just decide to take this game seriously.

Thursday, of course, is traditionally championship day, and this year we saw some real championship golf. Len Prior, of Bendigo, played a magnificent 27 holes, to record a 110 off the stick—2 over the card. In his great effort, Len three putted at least two holes, so with an ounce of luck he could easily have equalled the card. Still, this effort won him the state open, the country open and the country railway singles championships, so he should feel well satisfied with his day's work.

The country minor championship (handicap 14 and over) went to consistent Bob Morris who returned a 128 for the 27 holes. Norm Michington, from Shepparton, won the 27-hole handicap trophy with a fine net 105½. As previously reported, the V.R.I. Perpetual Cup and the F. M. Mitchell trophy was won by Joe De Araugo and his boys from Bendigo.

The presentation dinner was chaired most capably by Mr. D. O'Donnell, the Vice-chairman of the V.R.I. Sports Committee. Official guests included Mr. Commissioner L. A. Reynolds, Messrs. L. A. McCallum, (Vice-president, V.R.I.), R. I. Baggott, (Assistant Secretary V.R.I.) Messrs. M. L. Portingale and N. Williamson, (president and captain respectively of Rossdale Golf Club), Messrs. H. Fletcher and A. Collins, (president and hon. secretary of the V.R.I. Golf Club) and H. Francis, manager of Rossdale. The presentation of trophies was made by Messrs. Reynolds, McCallum and Williamson. The Rossdale Golf Club made a special presentation to Alby Jack of Bendigo, recognizing his hole-in-one effort—a gesture that was appreciated, not only by Alby, but by all present. So ended another pleasant country week. My thanks to Allan Collins for his assistance throughout this fixture, and to Harold Francis and his staff at Rossdale for the way they looked after us. Metropolitan trophy winners during the week were J. Holden, H. Adams, D. Nevill, H. Hawke, F. Conlin and K. Bowe.

Gone to Cuba

Mr. E. J. Szakall has left for Havana, Cuba, to attend a special congress of the International Federation of Fencing. The congress will decide where the 1971 world championships will be held. Mr. Szakall who is secretary and treasurer of the Australian Amateur Fencing Federation,



Playing in the Country Week golf tournament at Rossdale are (from left) Messrs. J. Bernard (Geelong), R. Coine (Dimboola), E. Nevill (Metropolitan), and H. Humphrey (Traralgon).

will put forward that organization's case for holding the championships in Melbourne. He will also be managing a small team of three fencers from the V.R.I. fencing club; two of them are already overseas. Mr. Szakall works with the Railway Construction Board at Head Office. (J.M.)

Bowls carnival

The following bowlers have been selected to represent the V.R.I. at the Intersystem Bowls Carnival in Hobart, from February 22 to March 6, 1970. From the country they are: M. T. Berry (Seymour), H. P. Boyle (Ballarat), T. Hindson (Bendigo), R. J. Laycock (Ballarat), A. W. Reaper (Yallourn), R. A. Sawers (Bendigo), D. E. White (Ballarat), and, from the metropolitan area, J. W. Brown, K. J. Bryce, A. Cowling, L. R. Duggan, T. H. Dykes, H. Heron, K. McElhinney, C. J. McFarlane, K. M. Pope, A. G. Sargeant, P. C. Smith, I. Stick, and E. J. Williamson. Emergencies, not necessarily in order of preference, are: Q. C. Aldridge (Violet Town); N. J. Bear (Morwell), T. F. Constable (Ballarat), R. C. Graham (Bendigo), E. S. Hubbard (Creswick), M. V. Kelly and W. T. Wild (both from Bendigo). Kevin Bryce, a clerk in the Accountancy Branch at Flinders Street, has been appointed manager, and Gordon Clark, Works Foreman at Seymour, has been appointed his assistant. A pleasing feature of this team is that the selectors have maintained the percentage of country representation, because it is my firm belief that our country bowlers are every bit as good as their city cousins.

Match at Donnybrook

Despite the atrocious weather, a crowd of about 300 went to Donnybrook on September 14 to watch the annual football match between teams from the Accountancy Branch sections at Flinders Street and Spencer Street. Before the ball was bounced, a 2-minute silence was observed in memory of the late Bill Merrifield who had organized these matches since their inception.

Flinders Street, 5-7, beat Spencer Street, 4-10. Best players for Flinders Street were Liddio Ricci, Mick Smart, and Frank Riordan; for Spencer Street, Jim Courtenay, Jim Brettag, and Lee White. Roger Barron was umpire, and the match was organized by Bob Rowlands, Harry Woodrow, and Des O'Donoghue. (J.M.)

MILEPOSTS IN V.R. HISTORY

1863

Contracts awarded for Bendigo to Echuca railway. Sandhurst-Epsom, May 20; Epsom-Echuca, August 4.

1864

Melbourne and Essendon Co.'s line closes. July 1; Bendigo to Echuca railway opened, September 19.

1865

Melbourne and Hobson's Bay, and Melbourne companies amalgamated as Melbourne and Hobson's Bay United Railway Co., June 30; St. Kilda company purchased, September 1.

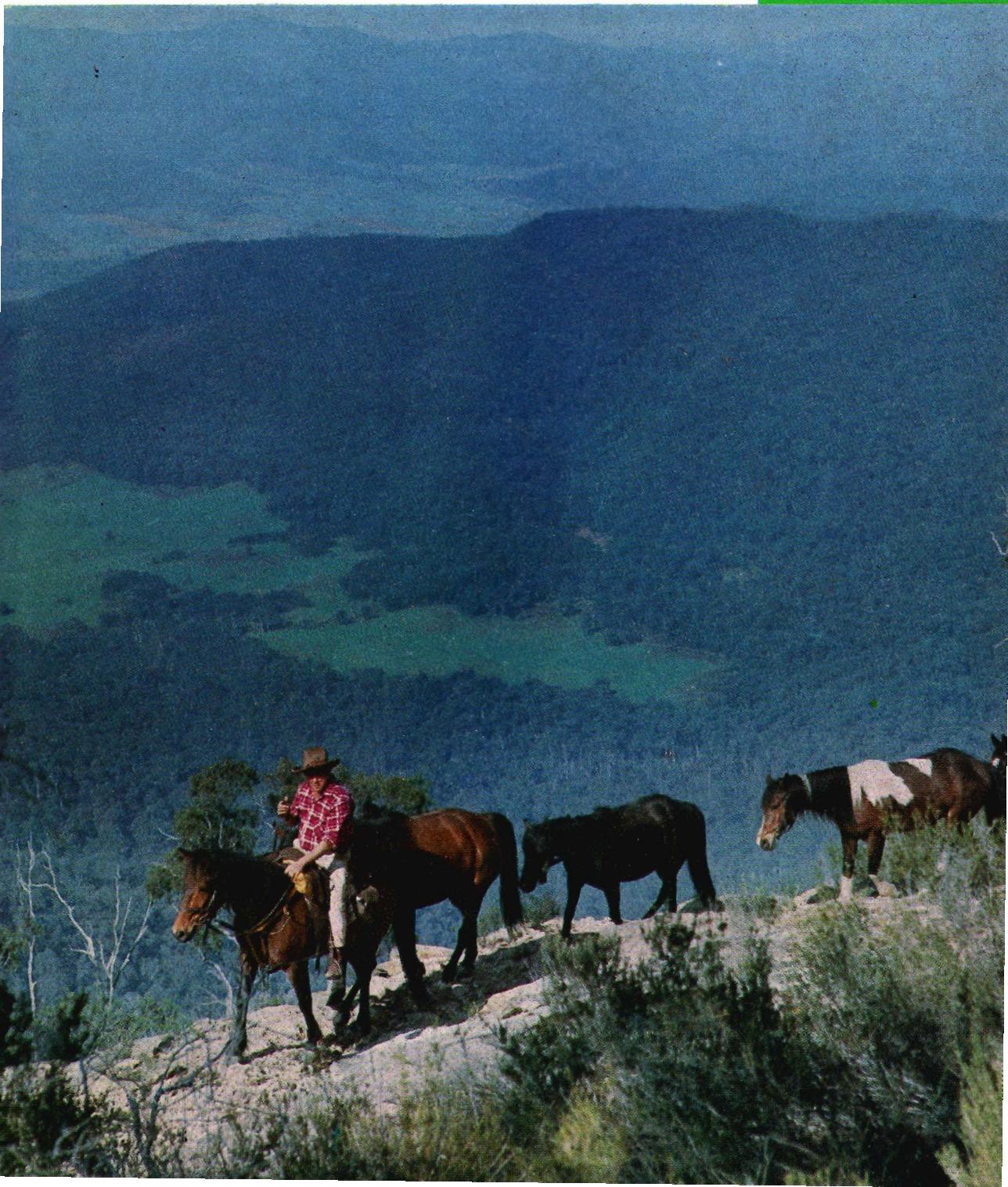
VICTORIAN RAILWAYS

NEWS LETTER

NOVEMBER



1969



CLEANLINESS OF TRAINS

Recently there has been some criticism in the Press about the cleaning of suburban trains.

Unfortunately, the Press did not seem interested in giving the other side of the story—that we are doing all in our power to keep the trains clean and attractive, and are planning better ways of doing it.

The facts are that all suburban trains are cleaned at least once a day. To clean trains while they are stabled, cleaning staff work day and night shifts, and mobile cleaning gangs visit out-stations. Trains in running are swept as opportunity permits.

In addition to the daily cleaning, more extensive attention is given periodically. About every three weeks, carriages pass through the mechanized washing plant that cleans the exterior, including windows. At the same time, the floors are washed. At intervals of about six months, the ceilings and upholstery are washed.

Improvements that are planned, or under way, include:

- the construction of platforms at Jolimont Workshops to

enable interiors to be given additional cleaning when the trains' running equipment is inspected—about every 4–7 weeks;

- the employment of female cleaners at some out-stations.
- more frequent painting of carriages, as funds permit;
- the installation of an automatic carriage washer in the Jolimont area, to improve the servicing of the Princes Bridge group of trains.

Some of our rolling stock is old, and even after washing and cleaning its appearance is not greatly improved. But these old vehicles are being replaced by well-lit, clean, and comfortable newer carriages. The materials used in these are not only attractive in appearance, but are less easily soiled, more easily cleaned, and therefore maintain their appearance better.

However, the speed with which this very desirable change can be made, depends almost wholly on the availability of the necessary funds.

NO SECOND CLASS

A term associated with rail travel for over a century will disappear following a decision made by Australian and New Zealand Railway Commissioners at their conference in Melbourne last month.

The term, *second-class*, will be dropped from railway usage.

The word, *economy*, will replace it.

The initial change-over date will be March 1 next year, when the new glamour express, *The Indian-Pacific*, will open the transcontinental service from Perth.

Because of a great volume of pre-printed tickets in stock, as well as a need for re-lettering carriages, the term *second-class* will be phased out gradually.

First-class will still be used to denote travel by the better services offered by the railways. It was a term railways have used since they began in Australia, in the middle of last century, and bequeathed to the language.

The word, *economy*, more truly reflects the nature of the service to which it referred. Mile for mile, rail travel is economical—even for first-class passengers.

MORE AIR RIGHTS

The Department is offering a 99-year lease of air-rights over the railway line at Jolimont. The area extends from opposite Powlett Street to Clarendon Street, and will involve building over the Jolimont railway station.

The area available is about 112 ft. wide and 700 ft. long. It is being offered in two lots and prospective developers can tender for one or both.

The railway tracks are in a cutting and the site would be an ideal piece of real estate, only a mile from the centre of the city. Development could be for any purpose for which a planning permit is available.

It is interesting to recall that, back in 1958, developers planned a \$20 million dollar block of flats over Jolimont station. There was to be a 15-storey building, half a mile long, with 2,000 flats. The project lapsed owing, it is understood, to problems of car-parking for the flat dwellers.

In addition to the air rights at South Yarra that have been leased to Ola Pty. Ltd for a proposed shopping centre, (*News Letter*, September 1969, page 131) the Department is now offering more land and air-space in the area, totalling 240,500 sq. ft., on a

99-year lease. Air-space, about 160 ft. wide, is available from Toorak Road for a distance of about 1,450 ft. towards the River Yarra, together with land at street level having a frontage to Toorak Road of about 50 ft.

The land and air-space is being offered in five lots, two of which are directly over the station which serves rail tracks from Dandenong, Frankston, and Sandringham.

This development also can be for any purpose for which a planning permit is available.

THE LADIES ARRIVE

Four female carriage-cleaners began work at Glen Waverley on October 20.

They freshen up carriages as much as possible during the turn-round interval at the terminal. Smoking compartments receive particular attention as they require the most cleaning.

If it is found that the cleaning of trains at Glen Waverley is successful the Railways will employ female cleaners at other suburban stations where there is sufficient terminal time. The female cleaners work a 30-hour week.

ANOTHER BRIDGE HIT

A *hit-and-run* road vehicle dislodged two girders at the Mt. Dandenong Road underpass, between Croydon and Ringwood East, on September 30. It caused the closing of the road until the next day, so that repairs could be effected.

Also, the speed of trains passing over the bridge was reduced to 10 m.p.h. until repairs had been made. All goods trains were cancelled.

FRONT COVER

The mountain ponies of Mt. Buffalo Chalet are being led by Groom Eric Higgins, from winter quarters in the Buckland Valley, to the plateau. The 12 horses began the long trip up the mountain about 10 a.m. on October 7. After being re-shod and groomed the horses were available for hire to Chalet guests.

The annual trek of the Chalet horses has been going on for many years. In fact, saddle horses took over the bush tracks, in the 1920's, from donkeys. Mr. Eric Higgins, who followed the famous Ernie Chalwell in 1952, does the shoeing, breaking in, riding and doctoring. As new horses are needed, they are specially selected from the surrounding district.

MALDON-SHELBOURNE LINE

The rail line from Maldon to Shelbourne, which was severely damaged by bushfires in January last, will not be re-opened, but arrangements will be made for the transport of wheat and other goods.

It is considered that, with the heavy losses sustained during the last financial year, the Department is not in a position to restore the line at an estimated cost of \$70,000, and keep the line operating when an annual loss of \$30,000 can be expected.

Before this decision was taken the Railways and the Grain Elevators Board agreed to provide for wheat as follows:

- The Grain Elevators Board will re-define the Shelbourne silo

area by agreement with growers i.e., so that growers who desire to do so can deliver their wheat to silos other than Shelbourne.

- The Railways will move by road all wheat delivered to the Shelbourne silo.
- The Railways will move any overflow of wheat in the re-defined Shelbourne silo area occurring during the harvest period.
- The Railways will give growers a service at Shelbourne during the harvest period which will be comparable with that given at other silos in the general area.

The Minister of Transport has asked the Transport Regulation Board to organize the necessary road transport for general movement of goods to and from the area previously served by rail.

a remarkable achievement, particularly in a small State such as Victoria where the longest rostered trip is to Mildura, about 350 miles, and a line on which the S class does not generally run.

NEW FREIGHT SERVICES

Freight trains will give a 5th-day delivery when the 2,459-mile Sydney-Perth standard gauge railway line opens next January.

The Commissioners of the Commonwealth and State Railways have agreed to introduce a daily trans-continental freight service from both Sydney and Perth; extra services will be added to meet the demands of traffic.

Three express freight services daily will run between Melbourne and Adelaide—at 10 a.m., 1.30 p.m. and 5.50 p.m. This will give 5th-day delivery into Perth, after vehicles have been bogie-exchanged at Port Pirie.

A daily freight service will leave Perth at 5 p.m. and arrive in Melbourne at 7.50 a.m. on the 5th day.

Traffic railed between Sydney and Adelaide, via Broken Hill, will have a 3rd-day delivery. This will be the same as that now applying to Sydney-Adelaide traffic, via Melbourne, because such traffic will be subject to one bogie exchange on either route. For traffic consigned via Broken Hill, a change of bogies will be necessary at Peterborough, in South Australia.

Traffic between Sydney and Perth, on the new trans-continental standard gauge line will, of course, have an uninterrupted run.

Only bogie wagons will be used on the new service, enabling freight trains to travel at passenger express speeds.

The new Sydney-Perth standard gauge line shortens by 267 miles the existing rail distance between the two capitals.

S CLASS CLOCK 2 MILLION

The S class diesel-electric locomotives made history last month, when S 302 "Edward Henty" clocked 2 million miles of train hauling, in a record-breaking 12 years.

This reduced by 2½ years the time taken for the only other V.R. locomotives to reach this mileage, four B class diesel-electric locomotives placed in service in 1952.

S 302 was one of the first batch of diesel-electric S class built by the Clyde Engineering Co. Pty. Ltd. in 1957-58; each one carries the name of a person famous in Australian history.

The first four—S300-S303—have the same numbers and names of Victorian pioneers as were carried on the four S class steam engines: Matthew Flinders, Sir Thomas Mitchell, Edward Henty, and C. J. LaTrobe.

These diesels were the third group of V.R. locomotives to be classed as

S. The first were goods steam locos built by the Phoenix Foundry, Ballarat, in 1883.

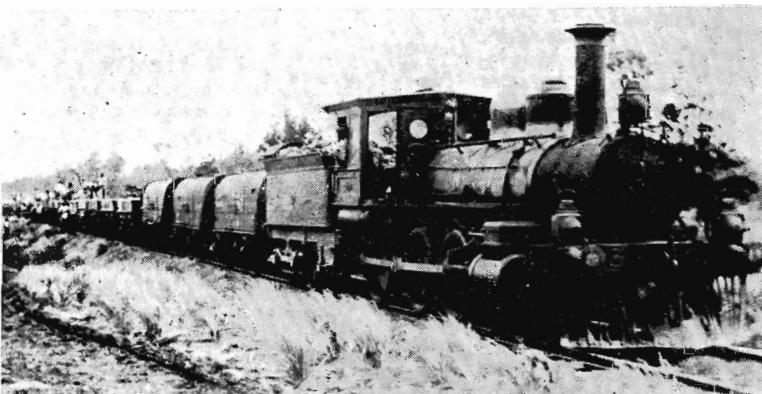
Second were the four famous steam engines built at Newport Workshops, just 40 years ago, and streamlined during 1936-38 to haul *Spirit of Progress*, to fit in with the blue and gold livery of the train.

Steam locomotive S302 was given the honour of heading the first run of *Spirit of Progress*, from Melbourne to Albury, on November 23, 1937.

With the coming of the diesels and the heavy cost of keeping steam engines in running order, the four S class steam locomotives eventually went to the scrapheap.

Steam locomotive S302, when broken up in July 1954, had run only 1,446,468 miles in its 25 years service.

For a locomotive to tot up 2 million miles after only 12 years operation is



Locomotive S 199 was one of the original S class. It is shown hauling a works train on the Mildura line, near Irymple, in 1903.

FIXTURE CARDS

The Department has produced a free 1969-70 metropolitan racing and trotting calendar card. Supplies became available from railway stations last month.

The popularity of the Railways football fixture cards over the past few years prompted its production.

As well as how-to-get-there information, the racing-trotting card clearly sets out venues month by month and, where possible, the main event to be decided. Interesting snippets about the clubs and races, and track circumferences and lengths of the straights, have also been included.

GOVERNOR'S VISIT



In the West Tower (left to right) are Mr. V. F. Wilcox, Sir Rohan Delacombe, Messrs. G. F. Brown, F. J. Garvey (Superintendent, Melbourne Yard), L. A. Reynolds, and E. J. Aston (signalman).



Mr. J. C. Crute (right), Manager, Melbourne Freight Terminal, removes a truck card to explain details of interstate traffic movement to the Governor.

It's safe to say that few more interested visitors have ever inspected the Dynon and Melbourne Yard areas than the Governor, Sir Rohan Delacombe, who went there on September 26. Hardly any part of this major railway complex escaped his keen attention and probing questions.

Sir Rohan also met and talked with many of the railwaymen who work there. The sections visited comprised the container, fruit and vegetable, motor car, scrap steel, and forwarding agents' areas, and the bogie exchange and hump yard.

Sir Rohan, attended by Captain E. J. K. O'Brien, A.D.C., was accompanied by the Hon. Vernon F. Wilcox, M.P. (Minister of Transport), Mr. G. F. Brown (Chairman of Commissioners), and senior railway officers, and, in the West Tower and hump yard area, by Mr. Commissioner L. A. Reynolds also.

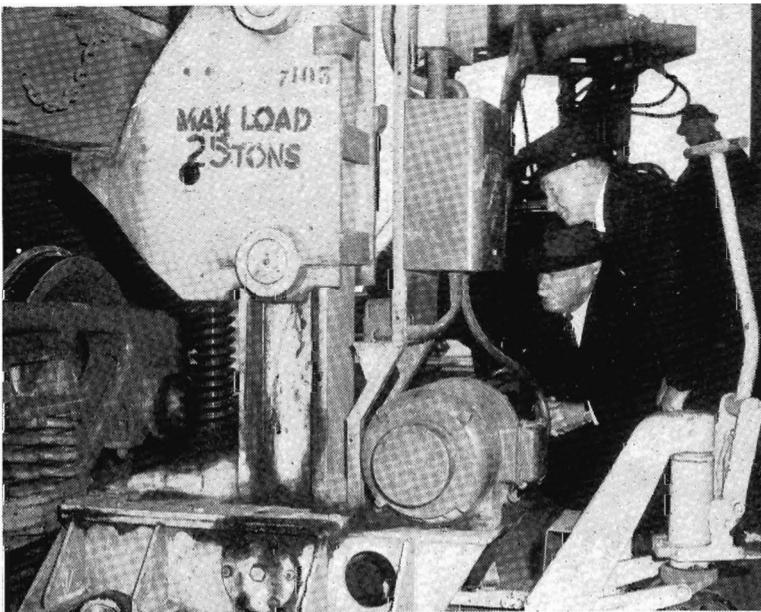
In a subsequent letter to the Chairman, Sir Rohan wrote:

"Thank you very much for organizing today's visit to the Railway yard and for giving so much of your time to take me around."

"It was all so interesting that I only wish that there had been more time."

"Please thank the members of your staff for the part they took in organizing the various demonstrations, in particular the Flexi-Van and the bogie exchange."

"The luncheon was most enjoyable. I am most grateful."



Watching a bogie exchange are Sir Rohan Delacombe and Mr. A. G. Patford, Manager North Melbourne Workshops (right).



The Governor poses for a snapshot being taken by Leading Running Gear Repairer M. Mausch.

The V.R.I. Council has been concerned with the difficulty of communicating with Institute members to tell them what has been happening within their Institute and what is proposed. It is hoped to overcome this difficulty by extending the coverage of Institute matters in *News Letter*.

This condensed version of our 59th Annual General Report is the first of a series of V.R.I. news items. We hope you will enjoy reading of what is going on in the organization to which you belong and contribute.

M. L. G. McKENZIE
General President

Financial

The Institute finished the year with a nominal profit of \$33. Money earned from various sources amounted to \$193,917, and money spent totalled \$193,884. Included in revenue were membership subscriptions \$81,050, payment by the Commissioners for services \$48,872, tobacco sales \$30,151, room rentals \$12,604, and billiard room revenue \$9,883. The cost of operating the Institute included salaries and wages \$113,135, educational costs \$12,549, library \$23,603, and tobacco and confectionery purchases \$26,424.

The balance sheet shows that the Institute's assets amount to \$336,430

and liabilities \$123,837, leaving the amount of \$212,593 representing members' funds which in the event of the Institute ever ceasing operations, would be distributed among members. Included in liabilities is the amount of \$41,000 which we still owe the Commissioners for buildings erected within the State, but against this debt the various country centres owe the Council \$62,196, not yet repaid by the centres for the buildings in which they operate. Other figures, interesting from a financial point of view, are the Institute's investments in Government and semi-Government loans amounting to \$21,452, the \$6,100 which the Institute has invested as an educational trust fund,

and the average interest of about \$1200 per year. This interest is used by Council in providing the V.R.I. scholarships for railway members who are studying subjects which, although outside the orbit of the Institute, will assist the scholarship holder in his chosen railway career. The Flinders Street Institute also holds in trust the amount of \$28,333 representing the money earned by some of our centre committees, and which must always be available for any Institute purpose the centre in question desires.

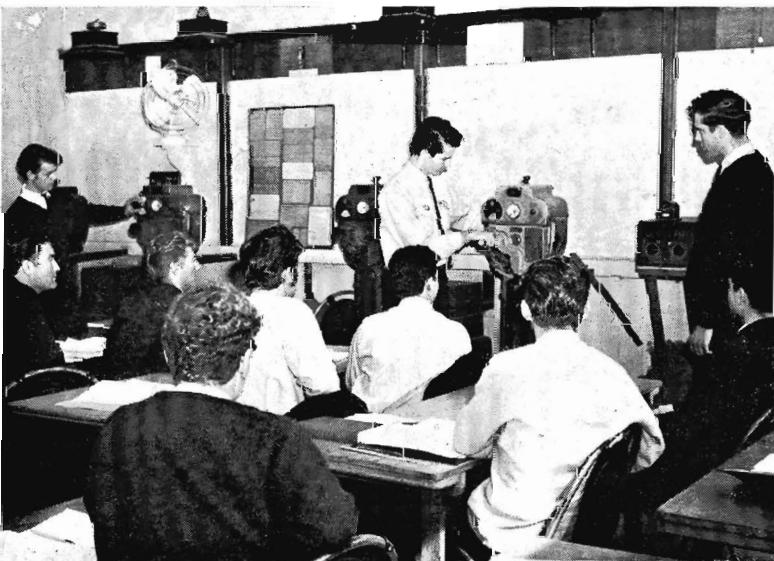
Membership

We lost, overall, 183 members during the past year, after recruiting 2444. Present membership is 15,467.

Through the generosity of the Commissioners, the V.R.I. has been supplied with a print-out from the computer payroll tape, and we now have a full list of those railway staff who are not members. This list will enable the Council to embark on a canvassing programme, firstly through a pamphlet and letter, and then backed up with a personal visit through the main country and metropolitan areas by the Sports Secretary. This plan is additional to the existing practice of all *new starts* being solicited through a personal letter and, again, the Institute's recruiting pamphlet.

Education

About one-sixth of Institute membership are undergoing educational training through the Institute's classes. There are 21 different courses available either orally, or by



A signals class at the V.R.I., Flinders Street.

correspondence from the Melbourne headquarters; country classes are held at 16 Institute centres. The usual annual examinations were held last November, and candidates who obtained the best results in their divisions were awarded monetary prizes donated by various railway and Institute-associated groups. For the first time in many years, the successful examination candidates were given their prizes at a special presentation night in the Flinders Street Institute in March this year. The Chairman of Commissioners was present and made the major presentations, assisted by the Institute's General President.

A further three scholarships were granted for this class year, making the number of current scholarships in the vicinity of 10. Unfortunately, since the inception of this scheme, some of the winners have resigned from the Department, and thus forfeited their right to enjoy this assistance.

Library

The Council bought 4,500 new books this year at a cost of \$6,500, and the country centres received about one-third of them. New book costs are an increasing problem to the V.R.I.; last year the average book cost \$1.36. It was kept at this figure only by careful buying in considerable volume at the right time. The average book cost next year could possibly go as high as \$1.80. This disturbs Council, mainly because a lot of money is spent on the library service, and not as many members use it, as could.

The present book stock is about 56,000, of which one-third is at our 23 country centres. The recently adopted practice of covering new books and still retaining their attractive dust jackets, seems to please our readers, and the book shelves appear much brighter. Attention has also been given to the handyman, hobbies, and technical sections of the libraries, and their patronage has delighted the Council. This section now includes almost every subject dear to the heart of those interested



A section of the V.R.I. library at Flinders Street; during the year 4,500 new books were bought.

in hobbies and handcrafts, including the do-it-yourself car owner.

The library in the Head Office was opened last November, and is operating quite successfully. In addition to the books being used, Council is very pleased with the number of new members recruited. The Head Office library is something the Institute has striven for since just after World War II.

Institute bands

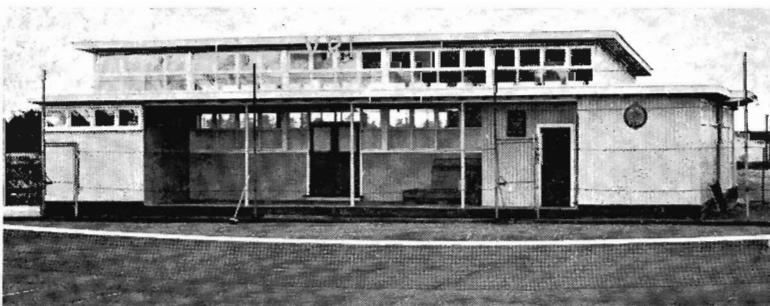
Both the brass and military bands upheld the name of the Institute in the commercial and competitive musical fields of Victoria. The brass section won distinction at the South Street competitions, winning three of the sections and gaining second place in the other. This band was only narrowly beaten in the 1968, A.B.C. radio contest, their victors being the Australian A grade champions. (Two months ago they won the 1969 contest—Ed.) The military section gave many performances at charitable functions, and the band

was proud that it was asked to play at the celebrations for the completion of 50 years of electric train running in Victoria, held at the Spencer Street station in October 1968.

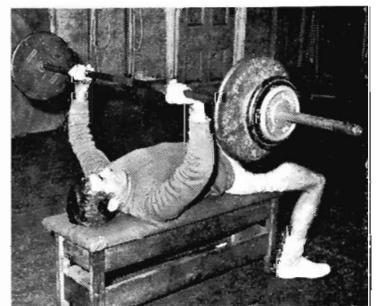
Recreational

Members' needs in this field were again met by the camera, chess, and wireless clubs, each of which held many interesting functions to delight the hearts of enthusiasts. The camera club again achieved success in inter-system photographic exhibitions held by the railways institutes of Australia. The chess club, although small in numbers, gave wonderful entertainment. It is of particular interest that the majority of players are Europeans, a section of the membership for which the Council has found it very hard to provide services. The club, reformed about 12 months ago, has already held A and B grade championships, and is doing very well in Melbourne chess competitions.

The elocution, dramatic art, public



The V.R.I. centre at Sunshine.



Weight lifting in the gymnasium.

speaking, and dancing classes are providing the members of railway families with cultural training, and are particularly enjoyed by the youngsters.

The Retired Railwaymen's Association has over 800 members who delight in renewing railway friendships, and generally drive and shunt trains all over the rooms and passages in the Flinders Street building.

The gymnasium, which enables members to acquire skill in boxing, judo, physical culture, weight lifting, and wrestling, has achieved reasonable success during the past year. Associated with the gymnasium is the sauna bath, the only Institute sauna in Australasia, and here again the means are available for the weight watchers, the over forties, those who just want to keep fit with a minimum of physical effort, and perhaps those who cannot get warm in Melbourne's winter. Even the wives of railwaymen and female staff, and in some instances husband and wife combinations, are accommodated with a minimum of expense. Council sincerely feels that the sauna is a worth while addition to the Institute's facilities.

Sporting

The V.R.I. was well represented in the business houses competitions in various sports, mid-week sporting competitions, inter-service matches, interstate carnivals and competitions. Sports covered were basketball, carpet and green bowls, cricket, fencing, football, golf, table tennis, tennis, rifle shooting, billiards, and snooker.

The outstanding sporting event during the year was in May, when Victoria was host for the first all railway systems Institute golf carnival. All mainland states plus the Tasmanian Institute and the New Zealand railway golf association participated. Wonderful weather and exact programming reaped their own rewards, and the 300 golfers and their ladies agreed that this particular carnival was a memorable one.

To make the picture complete, Victoria narrowly won the Commissioners' Shield and the Tintara trophy from Queensland, but the decision was in doubt right up to the second last day of the carnival. Other carnivals and sporting competitions in which Victoria performed creditably were the cricket in Perth when we finished third to Western Australia, with Queensland ahead, (only on percentage); the annual

billiards and snooker matches against the South Australian Institute; and big bore rifle shooting in the Trigg Shield competition between New South Wales, Queensland, and Victoria—our win being only the second since 1956, and our shooters were very happy with their success.

Carnivals in which Victoria will participate or conduct during the coming 12 months are basketball (Hobart, October 1969), bowls, (Launceston, February 1970), tennis (Perth, March 1970), and table tennis (Melbourne, August 1970). Country sporting weeks during the same 12 months will be bowls (February 16-20), tennis (March 2-6), cricket (March 16-20) and golf (September 14-17).

Country Centre activities

Each of the 23 operating country centres fulfilled their obligations in providing education and entertainment for our 5000 country members. From the more affluent centres, like Bendigo with its two new bowling greens, Geelong with its many improvements to the buildings, Seymour with tennis courts and men's and ladies' bowling greens, and Maryborough with its croquet lawns, down to the very smallest of centres like Wycheproof, with only 21 members and a converted Quonset hut for a building, yet whose table tennis team for the second year in succession

won the district table tennis competition shield; each has offered the local railwayman and his family the means of enjoyment and mutual entertainment.

The 24th Institute country centre will be established early in the 1969-70 financial year, when the Wodonga building is completed and officially opened. This fine building, costing about \$20,000, will be the central meeting place of approximately 200 railwaymen and their families. The building will be the first step in a two part plan that will eventually provide the Wodonga members with a very modern building comprising a social hall, billiard room, library, meeting rooms, kitchen, and other necessary facilities. The V.R.I. Council has every confidence that Wodonga will be a success.

Summary

The V.R.I. has had a reasonably successful year, during which revenue and expenditure, despite rising costs and other attractions to members, have been equated. There is no time or cause for complacency; there is much to be done, the challenge is always there to review the services that the Institute offers, and to meet the changing demands of the members. All the efforts of the Council in the ensuing 12 months will be towards meeting those demands.



Competitors playing in a Country Week competition.

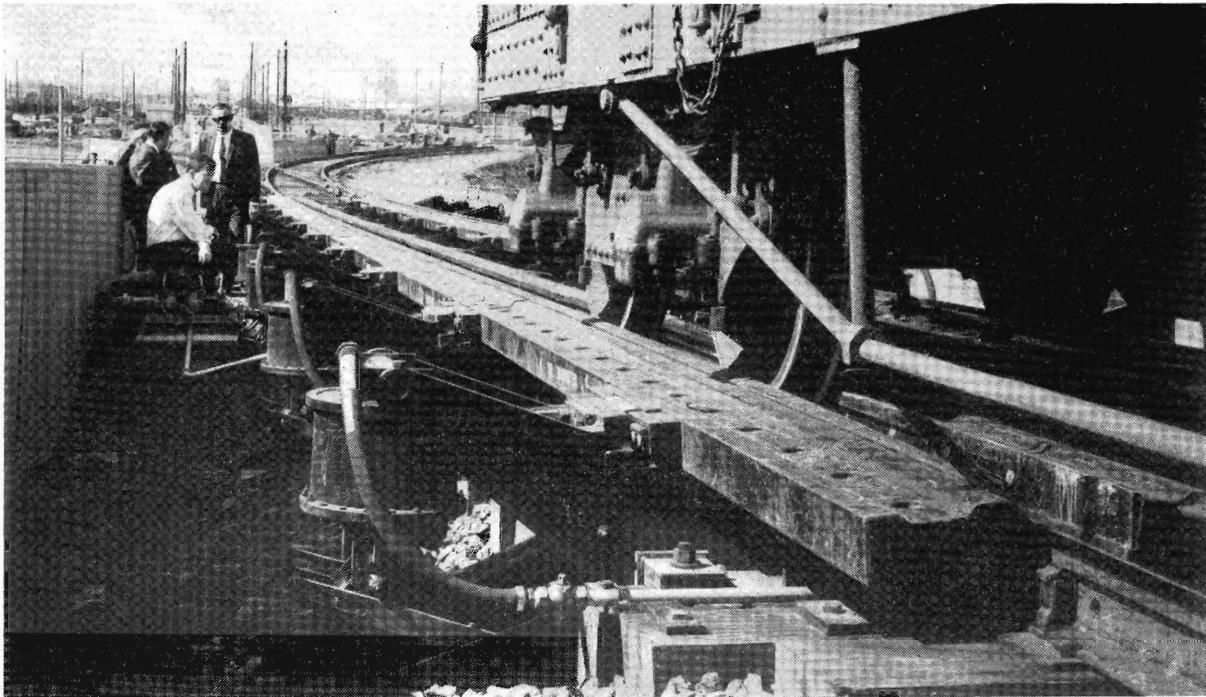


VIEWS OF NEWS

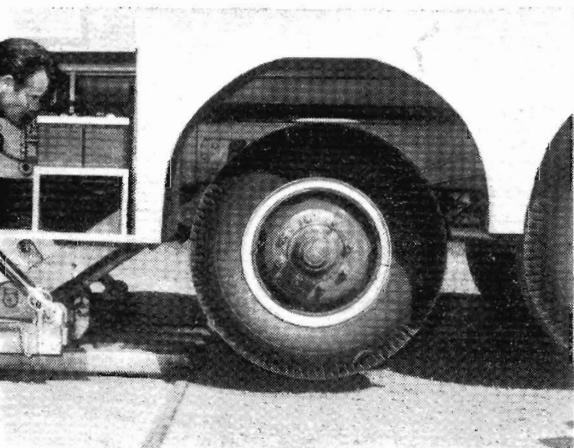
FIRST OVER THE HUMP: On October 13, the first train rolled over Australia's first automated shunting hump, now being built in the Melbourne Yard. A bird's eye view of the historic train, loaded with ballast, can be seen at the crest of the hump (centre of the picture).



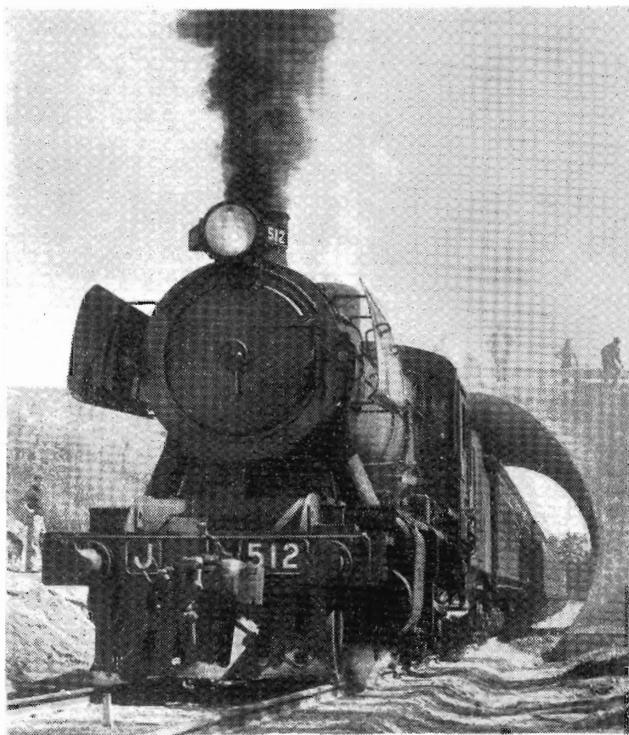
RAIL-ROAD: ... last month. ... manufacture ... give a demo ... where it w ... description



As the train moved down the hump, it passed into the primary retarders. When the hump is in operation, the retarders will grip the wheels of rolling wagons, exerting a braking force before they enter the sorting tracks.



For the first time, a rail-road bus ran on Victorian lines and by the N.S.W. Railways, it was on its way from the N.S.W. While in Victoria, the opportunity was taken to refer to railway engineers. The bus is shown at Somerton transferred from rail to road and back to rail. A full description of the vehicle was given on page 106 of July 1969 *News Letter*.



OLD AND NEW: A steam train passes through the newest form of tunnel on the Victorian system – the Armeo steel pipe (see *News Letter*, May 1969, page 67). The tunnel is on the Tyabb – Long Island Point line, and the train was run on behalf of the Association of Railway Enthusiasts.

FIRST-AID FINALS



The winning senior team, Electrical Engineers No. 1, treat patients (farmer and his wife, injured while firefighting) in the improvised material event. Third from left is Mr. Wain, winner of the senior individual section.



Bendigo Loco, winning team in the novice section, are treating the patients for snake bite and bullet wound.

Pleasant, sunny weather favoured the 58th State first-aid finals which were held last month at Mt. Evelyn.

The Challenge Shield was won by Electrical Engineers No. 1 (Messrs. R. R. Wain, G. L. Nicholson, P. J. Nanscawen, V. Hayes, and A. W. Marshall). This team also had the distinction of its leader, Mr. R. R. Wain, winning the senior individual championship.

In the novice competition, the Blackburn Shield went to Bendigo Loco (Messrs. A. C. Clough, J. Griffiths, N. Turner, M. Goldsmith, and R. Watson). The novice individual section was won by Mr. H. van Ginkel who, in addition, came second in the senior event.

Electrical Engineers No. 1 team will represent the Victorian Railways at the Australian railways competitions being held this month, in Perth.

The adjudicators were Douglas Donald, Hugh Johnston, D. Brownbill, G. V. C. Thompson, Doctors A. C. Reith, J. Peter Bush, J. H.

Cater, and Mr. V. Dwyer (First Aid Superintendent, State Electricity Commission).

Comments

In their comments on the work of the competitors, the adjudicators made the following points:

- the general approach to first aid was improving;
- in the transport event, some teams left an unconscious patient unattended, and more attention should have been given to supporting the head while the patient was lifted on to a blanket;
- teams should be practised in dividing the work so that two jobs can be done at once;
- patients must be thoroughly examined; in the senior improvised event, a log had fallen across the patient, but no one examined his spine for possible injury;
- injuries must be fully exposed before they can be treated;
- diagnosis, based on the history told by the patient, is very important; in the novice supplied event, the patient was not sufficiently questioned about the history of the accident;
- points are awarded for what competitors *do*, not for what they *promise* to do in the treatment of patients.

Dinner

At the dinner to competitors, awards were announced and presentations made by Mr. G. F. Brown, Chairman of Commissioners. Mr. D. D. Wade, (Chief Civil Engineer), who was chairman, proposed the loyal toast. Other toasts were proposed by Messrs. S. F. Keane (Chief Mechanical Engineer), I. G. Hodges (Chief Electrical Engineer), and A. W. Weeks (Chief Commercial Manager). Responses were made by Messrs. Wain, van Ginkel, Clough, A. L. Fuller (General Secretary, N.S.W. First Aid Corps), and Doctors Bush and Cater.

Indicative of the skill attained by railway first-aid men and the value of their work, was a letter read out at the dinner, by Mr. Wade.

"We, the undersigned Ambulance Officers, write to express our appreciation, through you, to a member of the Oakleigh station staff for his invaluable assistance to us this day.

"The gentleman concerned was in attendance at the goods yards at Oakleigh following an accident this morning in which a shunter was crushed and sustained a severed left upper leg.

"As a result of his efficient and capable attention to the victim, both

at the scene and in the ambulance during the journey to the Alfred Hospital, our efforts were rendered that much easier and the attention previously mentioned contributed markedly to the welfare of the unfortunate victim to the extent of

probably saving his life . . ."
 —G. Barnard, G. Campbell, (Victorian Civil Ambulance Service).

The officer concerned was Mr. A. Hinchliffe, assistant stationmaster, and holder of a V.R. gold life medalion for first aid.



Winner of the novice individual section, Mr. van Ginkel, is shown competing in the senior section in which he came second.

RESULTS

SENIOR TEAMS

1. Electrical Engineers No. 1
2. Ballarat Traffic No. 1
3. Bendigo Nth. Workshops No. 1
4. South Dynon Loco No. 3
5. Ballarat Nth. Workshops No. 1

NOVICE TEAMS

1. Bendigo Loco
2. Ballarat Nth. Workshops No. 3
3. Ararat
4. Head Office Works
5. Ballarat Nth. Workshops No. 2
6. South Dynon Loco No. 5
7. Spotswood P.W.M.D. No. 2

NOVICE INDIVIDUAL

1. H. van Ginkel, engineer, Head Office
2. T. H. Chafer, Boilermaker, Bendigo Nth. W.S.
3. H. Quick, train controller, Ararat
4. T. Murchie, E.T. driver, Jolimont
5. R. O. Smith, sub-foreman, South Dynon Loco
6. E. J. Mill, goods guard, Ararat
7. A. Maude, fitter, Ballarat Nth. W.S.

SENIOR INDIVIDUAL

1. R. R. Wain, welder, Elect. Depot W.S.
2. H. van Ginkel, engineer, Head Office
3. R. C. Lunnion, fitter, Ballarat Nth. W.S.
4. A. Phillips, fitter, tie with E. M. Sternberg, F/Aid Attdt., both from Ballarat Nth. W.S.

THREE RAILWAY AUTHORS

by JIM MORRISSEY

Last month was memorable for three Victorian railwaymen who received first copies of their recently published books. They are Messrs. Stan Kelly (a driver from Ararat), Les Borg (a compositor at the V.R. Printing Works), and Len Whalley (Publicity Photographer).

Eucalypts, by Stan Kelly, with text by G. M. Chippendale and R. D. Johnston, \$12, Thomas Nelson (Australia) Ltd.

Eucalypts, his latest book (*Forty Australian Eucalypts* was published in 1949) is the result of more than 20 years work. It contains 250 of Stan Kelly's detailed, accurate water colours reproduced faithfully in full colour, one page to each variety. The text comprises brief descriptions of the habitat and main characteristics of each species illustrated, together with their scientific and popular names. Notes and comments on the use of these species in cultivation are also given. There is a comprehensive index and a short list of reference books. The authors of the text work with the Forestry and Timber Bureau, Canberra, and

have specialized in the classification of eucalypts.

This book brings together for the first time coloured pictures of approximately half the known species of eucalypts (many of which have previously been illustrated only in line drawings) together with information on the natural distribution, etc. of the species. Although *Eucalypts* is intended primarily for the layman, it will also be of interest to professional botanists.

Stan Kelly has been in the Department for 33 years, and has lived in Ararat all his life. His favourite pastimes are painting in water colour, prospecting for gold, and searching for rare specimens of the eucalypts still needed to complete his collection of paintings.

The Maltese Song in Australia, by Manwel Nicholas Borg Union Press, Valetta, \$1.20

of his poems have been included in a text book for students at the Royal University of Malta. He is vice-president of the Literary Society for the Maltese in Australia, and is a regular contributor to various magazines and newspapers in Malta, writing essays, sketches, and articles of general interest.

Les has had an interesting career. Born in Malta in 1918, he worked first as a compositor, and later joined the editorial staff of the leading Maltese newspaper. In 1942 he enlisted in the Royal Air Force, and became a sergeant in the Special Investigation Branch. Later he was secretary to the Deputy Provost Marshal at the Middle East headquarters.

In 1949, Les emigrated to Australia, where he worked for some years with the Department of Supply, before joining the V.R. Printing Works in 1960. As well as writing poetry, he is keenly interested in colour photography.

Flashlights on Steam by Len Whalley, 64 pages on heavy art paper, 10 $\frac{3}{8}$ in. by 8 $\frac{1}{4}$ in., Ruskin Publishing Pty. Ltd. \$4.75



At Spencer Street, Mr. Kelly, who has just brought in *The Overland*, receives an advance copy of his book from Miss Liz McDonald, a representative of the publishers



Mr. Borg (right) shows a copy of his book to Mr. J. Muscat (Printing Works Accounting Officer) who also was born in Malta.

This fifth book written by Mr. Borg—usually known as Les—has just been published in Malta. It contains over 300 poems and has been acclaimed for its literary value. Les has been made an honorary member of the Academy of Maltese, and some



Mr. Whalley with some of the equipment he uses in night photography of trains.

This is a book that will be snapped up, not only by every rail fan, but also by many who appreciate good

photography. As its title indicates, it comprises a series of night shots of steam locomotives taken by flash.

The steam locomotive is one of the most impressive pieces of engineering ever made. Through the senses of sight, sound, and smell, it assaults the mind with jetting steam, flashing gear and beat of exhaust. To do this marvel pictorial justice, nothing less than the full tonal range of the camera will do—from the deepest black to the most brilliant white.

Night photography is the answer, as Len has shown in this photographic tour de force of the last days of steam in S.A., Victoria, N.S.W., and Queensland.

"Darkness", he writes in the preface, "provides the atmosphere through which certain exciting features are highlighted. For example, the sight and sound as a heavy goods train pulls out of town with the smoke lingering long over the moonlit landscape, the sounds of its progress drifting back on the cool night air, the station yard transformed into a fairyland of colour signal indications interwoven by dual ribbons of silver. Then there are the rows of wheels flickering through the beam of a car's headlights at a crossing. There is contrast of light and shadow too; in the loco depot the

dim shed lighting picks out the features of the nearest locomotive while in the background another moves out onto the turntable with steam screaming from open cocks."

This is the stuff from which the book is made, and all photographed against a backdrop of smoke, steam, light, and darkness.

Nearly every one of the 61 shots reveals some awesome aspect of the steam beast's power. But this dominating theme is humanized by shots of railwaymen going about their jobs—a lighter-up at work, a fitter adjusting a valve, a staff exchange.

When all pictures reach an equally high level of achievement, it is not easy to select any for comment. But especially striking are a 16½ in. by 8 in. shot of a 520 class steaming up the main street of Port Pirie, with an old 3-storey, iron lacework verandahed pub in the background; the naked power of two N.S.W.G.R. 38's storming out of the Marrangaroo tunnel with the *Through Mail* from Dubbo; a study of black on black; and, two R class racing, with trailing smoke, across a bridge near Melton. A notable achievement is a 38 class speeding at 65 m.p.h. near Goulburn. It was a panned shot taken from a car travelling at the same speed; hard enough to take in

daylight, it is fiendishly difficult at night.

This last picture reveals one of the problems of night pictures of trains. They're not easy. You need skill, considerable technical equipment, and unlimited patience.

As Paul Carroll says in the introduction: "Len Whalley captures authentic details of steam locomotives. He does more. He comes close—very close—to adding the feel, sound and taste of the atmosphere and environments in which the steam locomotive reigned. Only the rare combination of a skilled photographer and a man sincerely in sympathy with his subject could achieve so much."

Full technical details of each shot are given on the last page. The lighting used was, in some cases, electronic flash, but mainly varied from one to thirteen PF 60 flash-globes in 14 in. reflectors, wired in series through a capacitor system, and fired either manually or by an electrical triggering device. Some shots needed about 400 yards of wiring with 100 plugs to join it up.

Solely as a record, the book is valuable. Only about a quarter of the locos photographed are still running. Nearly half have been scrapped, and the rest are either in museums or awaiting scrapping.

RAILWAYS AND THE RACES

It was exactly 110 years last month since the Railways sought business from race patrons.

In Flemington's early days, many punters had to walk to the course (these days, some have to walk home—but for a different reason).

Special race trains first operated in October 1859, for a race meeting at Flemington. A small platform had been built on the Melbourne side of the bridge over the Maribyrnong River (then called the Saltwater River) between Footscray and South Kensington. From there, patrons walked about half-a-mile to the course.

While the special platform was being built, race patrons travelled to Footscray by regular train, and walked the two miles to Flemington.

A report at the time stated that 12,000 people were carried by the special trains on the first day (a little over a third of normal Cup Day traffic in present times). On several race days afterwards, traffic was so heavy that the Railways had to hire locomotives from the private Geelong Railway Co. to run extra race trains.

Later, a double-platform station was built at the river and remained in use for Flemington race meetings until the present direct line, via New-



Crowd at Flemington platform about 1870.

market, was opened in 1861—the year of the first Melbourne Cup, won by Archer—by the Melbourne and Essendon Railway Co. This line, however, was to close in 1864, and race traffic reverted to the original route until 1867, when the Victorian Railways took over from the private company and re-opened the line.

FREE RIDE

At Brooklyn last month, three newborn kittens were found on a bogie of a goods wagon from Echuca. How they had got there was a mystery. However, the locals found a cat that acted as a fostermother, and, according to latest accounts, the kittens are thriving.

Awarded prize



Last month, Mr. K. T. Moh was awarded the Hans Ernst prize for 1968, as the outstanding student in the final year of a full-time Mechanical Engineering Diploma course. Mr. Moh came to Australia from Hong Kong in 1966, and is a professional engineer in the Way and Works Branch. He joined the Department seven months ago, and has completed the fellowship course for his diploma, in three years instead of the usual five.

Handled big jobs

Meeting quietly spoken Mr. Tom Shaw, you mightn't realize that he has played a highly responsible part in many of the biggest track jobs done



in the Railways up to the time of his retirement as a roadmaster. He joined the Railways in 1925 and, after working as a repairer, was appointed a ganger in 1941, and special ganger five years after. Jobs in which he was concerned in a supervisory capacity included the relaying of the Scymour yard, the Gippsland line duplication (for which he also helped to select staff from European migrants), the South Yarra duplication, the standard gauge line, and the hump yard. When roadmaster

for the standard gauge line construction, he supervised up to 400 men, and was a guest on the first through passenger train. It is Mr. Shaw's proud achievement that he retained the friendship of every man with whom he has worked.

Thanks

Mr Alan Michalowsky of the Refreshment Services at Flinders Street wishes to thank all those railway men and women who so generously helped him on the occasion of his father's death.

Tribute to V.R.I.

A tribute to the educational facilities provided by the V.R.I. was paid by Mr. Stan Couch when he retired after 50 years 4 months and 21 days in the Railways.

Before joining the Railways, Mr. Couch was given permission by the Education Department to leave school at 12 years because his father and four brothers were serving in World War 1. After working with a local baker, he joined the Railways in 1919.

On starting with the Stores Branch, he realized that his education had

suffered, and took V.R.I. courses in English and Arithmetic, and then completed the courses for storemen.

"I owe the Institute more than I could ever repay. Without its help, I would not be retiring as Storeman-in-Charge", he said.

Stan was sub-branch secretary of the A.R.U. and delegate to its annual conference for the past 17 years. He played in the Traralgon and Benalla bands for years, and in the 'twenties trained many runners for the Traralgon Gift, was secretary of the local football club, and an official of the racing club.



Mr. Couch (left) is with Messrs. D. Robson, Depot Foreman, Traralgon (centre), and J. Griffin, Storekeeper, Laurens Street Depot. In the foreground is a motor mower that was presented to Mr. Couch. (Photograph: "Traralgon Journal")

Unionist

One of Newport Workshops' best known personalities, Mr. George T. Stapleton, office assistant, retired last month. He was at the 'shops for nearly 45 years, during many of which he played an active part in union affairs. He has been State vice-president of the A.R.U. since 1962, and secretary of the Combined Unions' Shop Committee for the past 12 years, and has also acted as State president and assistant secretary of the A.R.U. Although George says that his main hobby is bowls he will still find time for his other interests as a member of the Williamstown Historical Society and treasurer of the Williamstown and District Animal Relief Society.



The work of a local versifier was in evidence at the farewell to Mr. Stapleton who is shown (left) receiving a presentation made by Mr. R. Dawes.

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH
Ross, A. R., Bendigo North, 21/9

TRAFFIC BRANCH
Ellis, C. D. J., Ascot Vale, 28/9
Owen, D. K., Ararat, 27/9
McQueen, B. J., Spencer St., 10/10

WAY AND WORKS BRANCH
Barlow, R., C/o F'man Plumber
20/9
Farnell, N. J., Laurens St., 24/9
McMillan, R. K., Ararat, 23/9

RETIREMENTS

ROLLING STOCK

Binius, V., Newport 29/8
Briggs, A. J. W., Ballarat North, 25/12
Brown, G. H., Bendigo North, 5/12
Budge, R. T., Newport, 4/12
Holloway, A. J., South Dynon, 9/9
Jenkins, T., Bendigo Loco., 26/12
Jones, P. L., South Dynon, 2/9
Mahoney, G., Ararat, 2/9
Medvedo, W., Newport, 31/12
O'Neill, T. V., Newport, 15/12
Percival, W. J., South Dynon, 1/9
Quinlivan, L. R., Ararat, 5/9
Rawlings, W. L., Jolimont, 4/12
Robertson, J. F., Newport, 26/12
Robinson, L., E. R. Depot, 13/8
Rollfink, E., North Melbourne, 24/10
Turner, G. R., Bendigo North, 28/12
Wyatt, R. W., Newport, 20/12
Young, G. C., E. R. Depot, 28/8

TRAFFIC BRANCH

Cruickshank, R. E., Sth. Geelong, 28/11
Ayre, W., Glenorchy, 1/12
Ridgwell, E. L., Donald, 22/12
Tobin, W. J., Spencer Street, 4/10
Fowler, C. G. V., Caulfield, 30/11
Doherty, E. R., Melb. Goods, 3/12
Stiff, L. C., Geelong, 9/12
Wilson, O. J., Melb. Goods, 12/12
Herbel, J. F., C/- Metro. Sup't, 22/12
McInnes, D., Head Office, 23/12
Furuya, G. N., Geelong, 24/12
Barrett, R., Spencer Street, 26/12
Cumming, W. G., Yarrowonga, 26/12
Hrambamis, D., Melb. Goods, 4/9
Crocker, S. H., Ballarat, 15/8

WAY AND WORKS BRANCH

Barnard, G. H., Ararat, 24/12
Baughurst, W. J., Korumburra, 16/9
Exdell, T. T., S. & T., Flin. St., 7/12
Hanlon, M. J., Head Office, 30/9
Moore, J. W., Head Office, 12/11
Neal, B. H., Head Office, 17/10
Palmisano, F., Flinders Street, 19/9
Pollard, F. W., Warrenheip, 29/8
Rankin, A., C/- F'man Plumber, 20/10
Russell, J. G., Geelong, 25/12
Sulman, P., Anstey, 13/12
Ursini, A., C/- Metro. D. E., 28/12
Wittingslow, G. W., Ballarat, 10/10
Wright, F. A. T., Korumburra, 13/12
Wheaton, R., Geelong, 23/12

STORES BRANCH

Penington, W. J. S., Printing Works, 3/9
Garbat, M., Newport Workshops, 1/9
Northey, T. F. G., Head Office, 16/9
Bowen, N. M., Spotswood, 17/12
Forster, J., Head Office, 26/12

SECRETARY'S BRANCH

Petrie, A. J., Head Office, 23/1
Seedsman, L. A., Head Office, 13/12

ELECTRICAL ENGINEERING BRANCH

Boath, J., Testing Division, 23/12
Edwards, E. S., Testing Division, 15/12

REFRESHMENT SERVICES BRANCH

Renwick, J. H., Advertising Division,
16/9
Robertson (Mrs.) A. P., Spencer Street,
13/12
Reidy (Mrs.) M., Ballarat, 15/12

COMMERCIAL BRANCH

Hayes, L. W., Newport W'shops, 9/10



Tennis

As you know, the next Inter-system Tennis Carnival is to be held in Perth, during March 1970. The selectors seem to have come up with a pretty fair side, one which, I feel, gives us an excellent chance of bringing home the bacon.

Bruce Pearce, hard working hon. secretary, has been appointed captain, and others from the metropolitan area are John Bromley, Lin Butcher, Ian Gough and Keith Deayton. From the country Russell Booley (Geelong), Jim Forrest (Ballarat) and Wayne Schulz (Geelong) have been included. Popular president Maurie Barker (A.S.M., Broadmeadows) has been appointed manager—an ideal choice, I would say, as Maurie handled this job with great tact and efficiency in Sydney in 1968. Harold Jones from the Clothing Depot, Flinders Street, will be the assistant manager, and Frank Jones, of Little River, is going along as property steward.

Table Tennis

The dates for the 1970 Intersystem Table Tennis Carnival, to be played in Melbourne, have finally been settled, and the fixture will be held from Sunday, August 9 to Thursday, August 20, inclusive. Interstate teams will arrive on the Sunday and, after a manager's conference, will attend the welcome dinner. The presentation night and farewell dinner will be held on Wednesday, August 19, and participants will head for home the following day.

This game of table tennis is immensely popular in the country, and one of our newer sub-centres, Wycheproof, has just won their District Shield for the second year in succession—a particularly good effort. We expect to see some of these players competing in the 1970 V.R.I. championships.

Fencing

The Australian Championships have recently been held in Sydney, and the V.R.I. Fencing Club was well represented in most events. Quite a number of our members were included in the official Victorian team and the remainder entered as individuals. In the ladies foil, Christine McDougall was third and Megan Conlin reached the semi-finals. E. Simon filled second place and N. Archibald was third in the men's foil. Dr. A. Martonffy won his ninth sabre title, since 1952 he has contested every Australian title final in this particular weapon. Truly a remarkable record, and one that will take some beating. I think it speaks volumes for the organization and the coaching methods of our club, that so many of its fencers could do so well in the national titles.

Popular Secretary Andy Szakall is at present in Cuba attending the 1969 world championships. Andy has two reasons to be there, firstly to look after the interests of young Chris Clifford — a 16-year-old member of our V.R.I. Club, who is thought to be the youngest contestant ever to enter the world titles event—and secondly, to try to obtain permission for Melbourne to stage the 1971 World Fencing Championships. If anybody can persuade the controlling body to allow us to stage this fixture, then Andy is that bloke. Andy, a clerk in the Construction Board at Head Office, is without doubt one of the most dynamic figures in the Australian fencing scene, and is so well known and respected in world fencing circles that his presence at the international council table must enhance our prospects.

Cricket

It is with a deep feeling of disappointment that I have to report that there will be no Metropolitan competition during the 1969-70 season. Only four teams nominated, and the financial outlay required to run this competition successfully could not be justified with such a small entry. Surely there must be plenty of railway cricketers around the suburban area who would care to play the game under the ideal conditions provided at Royal Park. Perhaps next year, when a very definite attempt will be made to revive the competition, these fellows will make themselves available, so that the Commissioners' Cup will once again be competed for.

The A.P.I. match will be played this year at the St. Kilda Cricket Ground on Monday, December 1. It is hoped that we can field a strong side as the posties have a habit of winning this particular fixture. We



Playing in the A.P.I. versus V.R.I. golf are (from left) : F. Sharp (A.P.I.), R. Wilson (V.R.I.), R. Paley (V.R.I.) and K. Hellier (A.P.I.)

would like to see any off-duty railwaymen or retired blokes come along to St. Kilda and lend us some vocal support. Play is from 10 a.m. to 5 p.m. with lunch taken from 1 to 2. Cricketers desiring selection in the side to represent us in this match, must see that their applications reach me, C/- V.R.I., Flinders Street, no later than 10 a.m. on Tuesday, November 25.

Golf

On Friday, September 26, we were the guests of our old friends from the Australian Postal Institute at Northern Golf Club, the occasion being the annual golf match between the two Institutes. You might recall that about this time Melbourne was experiencing some pretty damp weather, but in spite of the persistent rain, the course was in quite good shape.

An 18-hole stableford event was played concurrently with the teams matches, and the winner of the V.R.I. individual trophy was Joe Hutchinson, of Geelong. Joe, who is a regular visitor to our Country Week fixture, is a very capable golfer and is always liable to pick up a trophy in any event in which he participates. The teams trophy, the R. L. Edwards Shield, remained in our hands after a check of the cards revealed that we had won 12 games to our opponents 5.

The battle for the Peewater Cup was a most exciting one and at the end of the days' play, the teams were tied with two games each and one all square. After a conference, the judges decided that a tie was the only decision possible, so this most valuable of trophies remained with the A.P.I., and our team captain, John Conlin, breathed a sigh of relief.

After the completion of play, an excellent meal was served in the

club house and a hearty welcome was accorded us by Mr. M. Smith, Assistant Director, Posts and Telegraphs. Mr. Commissioner L. A. Reynolds suitably responded on our behalf. Other guests from our Institute included Messrs. M. L. G. McKenzie, our General President, D. O'Donnell, Chairman V.R.I. Sports Committee, and F. M. Mitchell, our General Secretary.

RAILWAYS EARN MORE

Preliminary figures indicate that the total revenue of Australia's seven government-owned railways reached a record \$537.1 million for the year ended June 30, 1969.

The record represented an increase of \$5.4 million on the previous year's figure.

Capital value of Australian railways, at \$1,888.7 million, is \$145.2 million more than it was five years ago. Based on these figures, the Victorian Railways represented 24.3 per cent of the entire railway industry.

Australian railways have a real problem because passenger revenue remains constant in the face of rising costs. The railway industry believes some form of financial help is long overdue.

This particularly applies to suburban passenger services, where trains are geared to peak-hour demands that last only a few hours each day, many of the trains then becoming redundant. This intrinsically unprofitable operation is being enacted every day and is sapping the financial well-being of the industry. It is in this area that Australia's railways are urgently in need of financial assistance towards this social service.

VICTORIAN RAILWAYS

NEWS LETTER

DECEMBER
VR
1969



WE CAN HOLD THIS TRAFFIC

Next month, freight will begin moving over the new standard gauge line from Sydney through Broken Hill to Adelaide (by bogie exchange) and Perth, and in the reverse direction.

By offering Adelaide traffic an alternative route to that through Victoria, the new line could seriously reduce our valuable interstate traffic.

At the present time, we are carrying record tonnages between Melbourne and Adelaide, a large

percentage moving either from or to Sydney. A record of 61,525 gross tons was hauled for the week ended November 15. This freight can only be held by giving speedy and satisfactory service to our customers.

To do it, we need the complete co-operation of every member of the staff, particularly shunters, bogie exchange, maintenance, and operating staff.

With your help, we *can* hold this traffic.

ROAD VEHICLE DAMAGE

One of the four road vehicles that smashed into railway property during the week-end November 15-16, hit the same location twice. The incidents happened on the Saturday at Yarra Glen and North Melbourne, and on the Sunday at Carrum and Lilydale.

A van struck barriers erected to protect railway workmen at the corner of Arden Street and Lloyd Street, North Melbourne. The same vehicle, when returning from the opposite direction later, struck the barriers again, causing further damage. The vehicle did not stop.

At Yarra Glen, the road vehicle that struck and damaged the level crossing flashing-light signals did not stop, either.

The railway gates at Carrum were damaged when a motor vehicle slewed into them on the Sunday.

At Lilydale a 4½-ton grader ran down an embankment damaging about 30 ft. of track. Shuttle bus services were provided between Coldstream and Lilydale to ferry passengers to and from Healesville, around the obstruction, which was not cleared until late on Monday morning.

SEASONAL GREETINGS

TO ALL

"NEWS LETTER" READERS

AND

BEST WISHES

FOR THE NEW YEAR

NORTH-EASTERN LINE CENTENARY

It was exactly 100 years from November 11 that the building of the main north-east railway from Essendon to Wodonga was authorized by the Victorian Government. The Act was cited as *The North-Eastern Railway Act 1869*, and referred to Wodonga as Belvoir.

On several occasions, from 1866, suggestions had been made to incorporate the derelict 5 ft. 3 in. gauge Yan Yean tramway as part of the proposed north-east railway.

The 19-mile tramway had been built in 1855 by the Water Supply Commissioners to carry pipes and materials for the water supply line from Yan Yean reservoir to Melbourne. The line ran from the reservoir to near the present Exhibition Buildings in Nicholson Street. Built of a mixture of iron and wooden rails, it was worked with horses.

However, the track and area were considered unsuitable for the north-east railway.

Contracts for building the line, from Essendon to Wodonga, started in 1870, and it was opened in sections over two years—

- 1872: Essendon-Schoolhouse Lane (2¼ miles from Seymour)—Seymour-Longwood.
- 1873: Longwood-Violet Town-Benalla-Wangaratta-Wodonga. The building of the entire 182-mile line, from Essendon to Wodonga, cost \$1,977,696.

It was another 10 years before junction was made with the New South Wales Railways at Albury—on June 14, 1883.

The middle of the gauges between the two States, resulting in Victoria having 5 ft. 3 in. and New South Wales 4 ft. 8½ in. was not solved until 1962 when the N.S.W. standard gauge was extended to Melbourne alongside the Victorian broad gauge tracks.

SMOKING IN TRAINS

"I was interested to read the remarks made by the V.R. Chairman in the August *News Letter* regarding smoking in trains. I have been a pipe smoker for many years and I agree whole-heartedly with his views. Smoking compartments have always been a mess, and a health hazard. Imagine the state of affairs years ago when two brass spittoons let into the floor adorned each smoking compartment! We have come a long way since those days of travel—let's go further!"

*The time on the trip is short enough
To forego the pipe or cigarette puff—
Abstain on the train and save the
smoke,
To the greater content of the other
bloke.*

(T. Matthias, Warrigal Road, Surrey Hills, writing to the Editor)

STEAM SUBURBAN

Many railfan publications are of interest only to rail enthusiasts, but there are some that have a wider appeal. Such is *Steam Suburban* by R. Hudson. A 56-pp. 10 in. by 8 in. booklet with three maps and 51 illustrations, it brings together pictures of the early days of Melbourne's suburban system that will be relished by many of the *oldies* and, perhaps, quite a few of the younger people who may have no great interest in railways but can enjoy a period piece. The Victorian era, so chronologically close, is, from a psychological viewpoint, aeons away from the present turbulent times. The booklet is \$1.75, and available at city stationers or from Windsor Publications, P.O. Box 221, Prahran.

FRONT COVER

LOCO REPAINT: At South Dynon Loco Depot, Car Painters E. L. Shaw (*in cab*), B. Slattery (*left*), and D. Ainger are at work on an S class locomotive.

REVIEW of the year

The Commissioners' annual report for 1968-69 pointed out that the main cause of railway deficits was the concessions given by the Railways to various sections of the community. The report also showed the improvement in financial results that would follow if the Railways were recompensed for these concessions. A condensation of the report is given below.

A SOUND BUSINESS BASIS

Railway accounts have long concealed the assistance given by the State Government to sections of the community.

These include inevitable losses on suburban and country passenger services and special concessions to country industries and exporters.

The social services aspect of passenger services, particularly in large cities, is being increasingly accepted throughout the world.

While these social services and special concessions are continued as a matter of Government policy we strongly urge that we should be recompensed, not solely by way of deficit financing with its undisclosed cross-subsidization, but by the provision of funds to the extent

involved, as is done with other social service activities.

This would have the effect of putting railway financial affairs on a sound business basis and thus give the Government credit for what it is doing for each section of the community.

On this basis the results for 1968-69 would be approximately as shown below :

	\$	\$
REVENUE RECEIVED	100,528,000	
Treasury reimbursements for services rendered in pursuance of Government policy.		
<i>Operating loss on suburban passengers</i>	6,000,000	
<i>Operating loss on country passengers</i>	8,500,000	
<i>Concessions to country industries</i>	4,400,000	
	<hr/>	
TOTAL EARNINGS		119,428,000
<i>Working expenses</i>	111,216,000	
<i>Interest, etc. charges (including those applicable to passenger services)</i>	6,615,000	117,831,000
	<hr/>	
SURPLUS		<hr/> 1,597,000

FINANCIAL RESULTS

The financial result of operating the Railways during the year ended June 30, 1969, was (after charging interest, etc.) a deficit of \$17,368,232. The details of this are set out below, together with comparative figures for the previous year.

	1967-68	1968-69
	\$	\$
GROSS INCOME EARNED	99,393,515	100,590,879
WORKING EXPENSES CHARGED AGAINST INCOME	105,204,041	111,344,202
LOSS ON CURRENT OPERATIONS	5,810,526	10,753,323
Interest Charges and Expenses	5,377,276	6,221,177
Exchange on Interest Payments	118,683	105,518
Contribution to National Debt Sinking Fund	250,623	288,214
TOTAL INTEREST, EXCHANGE, ETC.	5,746,582	6,614,909
DEFICIT	11,557,108	17,368,232

The main increases in earnings were:

	\$
Goods	727,000
Passengers	248,000
Rentals and miscellaneous	222,000

Higher working expenses were principally due to the following increases:

	\$
Cost of wages awards ..	5,086,000
Higher cost of materials, etc. ..	1,632,000
Railway Accident and Fire Insurance Fund	376,000
Long service leave ..	223,000
Superannuation charges	179,000

However, savings in operating and maintenance costs amounted to \$1,493,000.

FINANCIAL CONSIDERATIONS

Loan expenditure

The total of interest-bearing funds expended on renewals and replacement since July 1, 1960, is now \$84,932,000. We emphatically repeat our warning against continuation of this alarming rate of overcapitalization.

The \$16,826,396 of loan funds allotted during the year were expended as follows:

	\$
Renewals and replacements	
Way and works ..	4,939,985
Rolling stock ..	6,059,630
	10,999,615
Capital	
Way and Works ..	5,435,496
Rolling stock ..	58,419
New lines	332,866
	5,826,781

General

An important development was the introduction from July 1, 1968, of a simplified rating scheme for intersystem goods. This coincided with the publication of the *Railways of Australia Goods Rates Book*, setting out the new method of intersystem goods rating, on the basis of a uniform classification and uniform intersystem mileage rates.

A further 46 industries were approved by the Minister of State Development as eligible for the special rates applicable to "Approved Decentralized Secondary Industries". This brought the total at June 30, 1969, to 638. It is estimated that these concessions are costing \$524,000 a year.

On January 26, 1969, passenger fares were increased from five to 10 per cent for suburban travel and

five per cent for country journeys. In addition, all stations within 32 rail miles of Melbourne were included in the suburban fare scale. This closely approximates the area now designated as the metropolitan area.

At the same time many of the fare anomalies which had arisen over the years because of fixed percentage fare increases were corrected.

Grade separation

Twenty per cent of the cost of grade separation works is charged to railway loan funds.

The only source from which the Department can meet the interest payments on these funds is the savings, if any, resulting from the grade separation works.

Many of the projects—particularly in country areas—result in no saving to the Department, and a survey of a representative sample of metropolitan and country works has shown that an average payment of 7.5 per cent of the total cost is the most that can be justified by the savings accruing to the Department at the present time. This will decrease in future.

Mention is made of the number of accidents due to road vehicles running into the sides of trains or damaging the underside of railway bridges. In many instances the owners were not insured and we are therefore unable to recover damages.

Stores

The value of stock held at June 30, 1969, was \$8,436,959—\$1,169,650 greater than at the same time the year before.

Issues and sales from stock were valued at \$35,057,575, the turnover rate being 4.15.

Materials reclaimed for sale or re-use realized \$875,657, only slightly less than in the previous year.

RETROSPECT

Following on the disastrous January bushfires, the staff tackled the difficult job of restoring services quickly with commendable endeavour and resourcefulness.

It is not generally realized that each summer we burn or poison 9,000 miles of firebreaks along railway lines at a cost of \$500,000. We also make an annual grant of \$30,000 to the Country Fire Authority for its assistance. Our efforts contrast sharply with the lack of preparedness by many property owners adjacent to railway lines, who last year made only 648 miles of firebreaks.

Once again, the staff responded magnificently to the emergency that

resulted from the *Southern Aurora* accident, and many other individuals and organizations worked to assist the victims, without counting the cost. We have appropriately expressed our appreciation and thanks.

To keep things in perspective, it is well to remember that during the last 50 years only 11 passengers have died in Victorian Railways accidents. In the same period well over 7,827 million passenger journeys have been made.

Two new short, but important, railway lines were built during the year. One runs from near Welshpool to Barry Beach, the site of the natural gas off-shore drilling marine terminal. It is three miles long, including sidings.

The other line branches from the Stony Point line mid-way between Tyabb and Hastings, and runs for about 1½ miles. Initially, it will handle traffic to a new fertilizer works, but it will be able to meet the requirements of any new industry in the proposed industrial complex in Westernport's Long Island area.

Traffic has been conducted over both lines since April 1969.

In addition, a 1½-mile extension was built from the Appleton Dock sidings to the new Swanson Dock container terminal and opened for use on March 14, 1969, to take containers to the country in anticipation of the arrival of the first container ship.

COMMERCIAL ACTIVITIES

The strength of our commercial agents was increased during the year both numerically and by special training. However, our marketing activities are again being critically reviewed.

A wider coverage of the State has

A special public version of the annual report is being produced, and a limited number of copies will be for sale. Well illustrated, with many colour plates, the report will be available from the Senior Clerk, Public Relations and Betterment Board, Room 98, Railway Administrative Offices, 67 Spencer Street, Melbourne, 3000. The price is \$1 a copy, postage included.

been provided, and staff have been able to spend more time in discussion with potential rail users in the city and the country as well as with our present patrons. They have been supported by the specialist staff available to advise on packaging and freighting problems generally. By this means we have gained valuable new traffic and retained that which might otherwise have been lost. Nevertheless, significant rate reductions have been necessary over an extended area near the borders to combat the activities of so-called interstate operators.

Despite mounting road competition, interstate tonnage continues to increase and now is 29.6 per cent of our total tonnage. Competitive rates must be backed by a high standard of service, frequently involving guaranteed delivery times.

A gratifying feature is the considerable co-ordination between road and rail, with an increasing number of road operators using rail for their city to city hauls. Record tonnages are being handled by this forwarding agent method and the highways are less congested as a result.

Good autumn rains ended the drought which had dogged Victoria

for much of the previous year, and the harvest was plentiful.

Although the wheat harvest for 1968-69 of more than 94 million bushels was about 67 million bushels in excess of the previous year, the quantity exported (11.4 million bushels) was even less than the previous year's figure of 17.7 million bushels. We were thus deprived of valuable revenue we would otherwise have earned from railing wheat to the seaboard.

The first overseas container ship *Encounter Bay*, berthed at the new Swanson Dock container terminal on April 10, 1969. Since then a further four container ships have berthed at the terminal. We moved 1894 containers to and from the five ships.

The principal items have been canned fruit from Victoria's Goulburn Valley and South Australia's Riverland area, dried fruit from the Sunraysia district, and flour, rice and milk products.

As indicated elsewhere our wagon construction programme anticipates the increasing overseas container traffic.

Superphosphate traffic showed a welcome increase of 32,447 tons over the previous year, but this was well below the 1966-67 figure.

Traffic in cement, motor cars and motor car bodies also increased.

PASSENGER OPERATION

It is not surprising that the rapidly increasing car ownership is reflected in the diminishing number of passengers travelling by train.

Peak hour suburban services are still heavily patronized, particularly to those stations 10 miles or more from the city; the longer journeys from and to the newer residential

	1968-69	1967-68	1966-67
Total goods and livestock tonnage	11,315,916	11,115,953	12,074,550
Average haul per ton of goods (miles)	168	160	160
Total ton miles (goods and livestock) in millions	1,903	1,776	1,937
*Average miles per wagon per day	33.22	34.04	33.75
*Average ton miles per wagon per day	347	323	340
Average tonnage (net) per loaded wagon mile	15.58	14.08	14.91
Average ton miles (net) per goods train hour	3,754	3,607	3,701
Contents load per goods train mile (tons)	298	286	305
Percentage of empty wagon mileage to total	32.98	32.53	32.45

* Based on the number of goods vehicles actually available for service.

HIGHLIGHTS

Revenue . . . \$100,590,879

Of each \$1 earned:

58c came from goods and livestock

34c came from passengers, parcels and mails

5c came from refreshments and advertising

3c came from miscellaneous

Working Expenses \$111,344,202

Of each \$1 spent:

74c went on wages and payroll tax

1c went on coal, oil, etc., for locomotives

3c went on electrical energy for traction

13c went for other materials and services

7c went on pensions and long service leave

2c went on other expenditure

Route mileage open for traffic . . . 4,190

Average number of staff employed . . . 27,203

Total capital invested \$476,000,000

Tonnage of goods & livestock carried . . . 11,315,916

Passenger journeys:

country . . . 4,078,208

metropolitan . . . 140,787,697

Principal commodities carried:

Wheat . . . tons 1,688,839

Briquettes . . . 1,028,234

Fertilizers . . . 913,925

Cement . . . 765,361

Iron and steel bar, rod, etc. . . 661,461

L.C.L. Containers, 591,665

Firewood, timber, wood-pulp . . . 362,378

Coal . . . 274,409

Livestock . . . 278,491

Motor cars and bodies . . . 225,330

Flour, bran, pollard, etc. . . 200,883

Barley . . . 190,557

Petrol . . . 181,699

Oats . . . 168,920

Sand, stone and gravel . . . 142,063

Beer . . . 136,613

Wool . . . 140,230

Locomotives:

diesel-electric . . . 237

diesel-hydraulic electric . . . 35

steam . . . 72

Passenger carriages 1,783

Wagons, vans, etc. 23,424

Fuel consumed:

distillate, tons 61,933

coal " 2,131

areas bring their own problems. Our peak hour traffic is becoming heavier and more trains, track duplication and improved signalling must be provided at a considerable outlay, although maximum use is made of them only for the few peak hours each day.

Unnecessary train and carriage mileage on off-peak suburban journeys was avoided by matching services to the demand and running trains with fewer carriages.

Nearly 300,000 passengers travelled on special suburban and country trains organized in conjunction with sporting and other events.

From August 1968, passengers travelling to Mildura were able to have their cars transported by the same train. This began as a three days a week venture, but now operates on an "as required" basis, six days a week.

During the year, the frequency of the direct service between Melbourne and Canberra was increased from four to six days a week.

PROGRESS

Motive power

Six 2,000-h.p. diesel-electric locomotives for main line goods and passenger services are on order. These will be our most powerful diesel-electric locomotives.

A further twelve 1,050-h.p. diesel-electric locomotives were purchased. All are designed for goods and passenger traffic on main and branch lines, but five are also equipped for hump yard operation at controlled low speeds.

In addition, six 750 h.p. diesel-electric branch line and shunting locomotives were added to the fleet.

This will complete the dieselization programme.

Passenger stock Suburban

Five *Harris* type suburban motor coaches were completed in our workshops during the year.

Tenders were invited for the supply of a number of additional trains, each to consist of six modern design carriages with more seating and greater overall capacity than the present 7-carriage trains.

These longer carriages will provide more passenger comfort than existing ones. New features on these trains will be pressurized ventilation, carriage heating, and remote-controlled doors. There will be as much window area as possible, good lighting, and quiet operation.

Country

Two air-conditioned, self-propelled

rail-cars, capable of speeds up to 70 m.p.h., have been ordered for use on the Ararat-Hamilton-Portland line, and the first one should come into service about the middle of 1970. The exterior will be of polished aluminium, and the luxury seating will be reclining type.

Two more second class air-conditioned sitting carriages were provided for *The Overland*. They are jointly owned by the South Australian Railways and ourselves.

Freight vehicles

New wagons built in our workshops during the year were:

Type	Number completed
52-ton flat wagons (for coiled steel traffic)	6
55-ton flat wagons (for container traffic)	32
49-ton flat wagons (for steel plate and general traffic)	25
50-ton flat wagons (for container and general traffic)	18
50-ton open wagons (for general merchandise)	110
50-ton cement hopper wagon (aerated discharge)	1
40-ton tank wagons	10

The thirty-two 63-ft. flat wagons specially designed for overseas containers are of low-alloy, high-tensile steel with an economical tare weight of only 19 tons. A further 113 such wagons will follow.

Seventy-six 53-ton capacity low-alloy steel grain hopper wagons have been received from the contractor, a further 24 being required to complete the order.

An additional 25 insulated tallow tanks are to be built and three bogie container wagons are to be provided for tallow traffic from Portland.

At the close of the year, we had in service 1,766 wagons suitable for bogie exchange. The extent to which bogie exchanging has become an established part of operations is apparent from the year's tally of 34,791 vehicles dealt with at South Dynon and 3,152 at Wodonga, compared with 25,107 and 2,756 respectively, two years ago.

Melbourne Goods Terminal re-arrangement

The first major stage of this most important project to provide an automatic railway classification yard with hump shunting and automatic control of points and retarders was completed on December 2, 1968, with the official opening of the West Tower and Melbourne Yard's new arrival yard. Despite the difficulty of completely altering the layout of this

major marshalling yard while maintaining operations, the work has proceeded according to schedule.

Control of the arrival yard is now effected from the West Tower and the automated classification procedures will follow.

The inadequacy of the old arrival yard for today's longer diesel-hauled trains has been overcome by the 17 tracks in the new yard, the longest being 2,062 feet.

A new 4-storey building, the East Tower, was completed and houses the Assistant Yardmaster in charge of the make-up of departing trains.

A contract has been let for the automation of the Yard and some of the equipment has already been manufactured and delivered, while preliminary site work is in hand by the contractor. Meanwhile, the hump construction is proceeding.

At the same time the goods shed facilities are being modernized and expanded, so that they will be able to cope effectively with the expected volume of traffic.

Permanent way

Among the track machines purchased this year were three ballast regulators, two sleeper spacers and two sleeper renewing machines.

In the course of work during the year, 365,000 cubic yards of ballast, 505,000 new sleepers and about 30,000 tons of new and reclaimed rail were used. Two hundred and eleven miles of track were relaid.

Geelong line duplication

Work has continued steadily on this project which will enable us to operate improved services between Melbourne and the State's second largest city. Two tracks are now available between Melbourne and Werribee as well as between Corio and Geelong, enabling much better timekeeping on the line. The duplication between Werribee and Little River is proceeding.

Communications

The rebuilding of the telephone pole line to Serviceton continued. When the final section between Dimboola and Serviceton is completed, Melbourne will be linked with the South Australian border by the latest transmission equipment. The necessary components are on hand.

Advertising and public relations

For some years railway messages have been broadcast daily over country stations. This year we ventured into country commercial television stations, which we are satisfied provides a wider coverage.

A steady stream of inquiries from potential rail users has confirmed the value of our continued newspaper

advertisements highlighting the wide range of articles, some of them unexpected ones, which come and go by train.

THE FUTURE

Can Melbourne afford to wait much longer for its underground railway loop? Many who have made a study of passenger transport are convinced that an early start is imperative, and many cities overseas are leading the way. Finance for the project remains the only barrier.

We now face the paradox of an excellent harvest but relatively little movement of wheat for want of export sales.

Uncontrollable rising costs, due in large measure to wage and salary increases, are another concern. We will maintain our intensive efforts to eliminate wasteful procedures and ensure that all our operations are conducted as efficiently as possible.

PERSONNEL AND ADMINISTRATION

Wage increases granted by the Commonwealth Conciliation and Arbitration Commission were spread over the many railway grades and in a full year they will involve us in additional expenditure of about \$6½ million. Additional amounts were authorized by way of State incremental payments, salaried increments and service grants. These are estimated to cost \$925,000 in a full year.

The average annual payment, including overtime and penalty payments, to all officers and employees, including juniors, was \$3,073 compared with \$2,884 the previous year.

Management Services Division

Projects undertaken by Work Study and Methods Officers during the year reflected the confidence shown in the Division by the various Branches, and the detailed and systematic approach to a diversity of problems has had beneficial results.

Use of the General Electric 415 computer was extended progressively in stores inventory control, accounting and costing to all of our 57 storehouses, and to the processing of statistical information, earnings, tax and superannuation records and to the solving of engineering problems.

Industrial safety

Safety practices and performances were under constant and critical review. The reward was a continuation of the downward trend in the number of industrial accidents.

Education

We are vitally concerned to combat

obsolescence in our trained staff and advantage was taken of the many opportunities in today's educational structure for attaining new skills and learning to use new techniques.

Many senior officers were nominated to living-in courses in higher business management and many others attended shorter study courses and seminars of a specialized nature.

The Victorian Railways Institute, which continued to provide tuition in railway subjects, is investigating the modernization of its teaching facilities.

Overseas visit

As guest of the Austrian Federal Economic Chamber, Mr. J. K. Brodie, Acting Engineer of Maintenance, left on April 19, 1969, to attend a permanent way exhibition in Strasbourg, France, and was later given an insight into the working of the Austrian Railways. He returned on June 19, taking the opportunity to briefly look at railway operations in the United Kingdom and North America on the way.

Staff thanked

In a vast enterprise it is essential that there be a high degree of co-operation by the staff at all levels. We thank the staff for the way they carried out their work during the year, particularly where they had to contend with unusually difficult situations encountered this year.

APPRECIATION

Mt. Buffalo Chalet

Last Sunday we returned from our holidays at *The Chalet*, Mt. Buffalo. This is our third holiday there and each time we have been very pleased with everything there. We have two children and they are so well catered for, that it really is an enormously enjoyable holiday for us . . .

I feel that I must also mention a special thanks to Mr. Bill Marriott, who is really the life and soul of *The Chalet*.

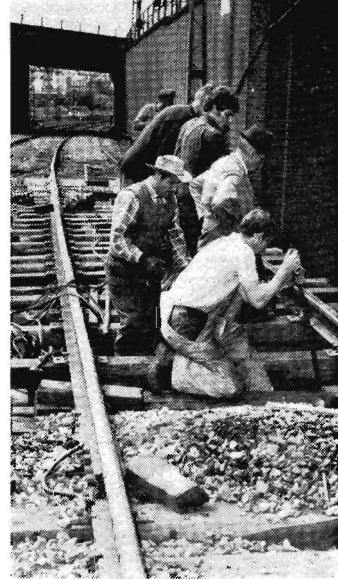
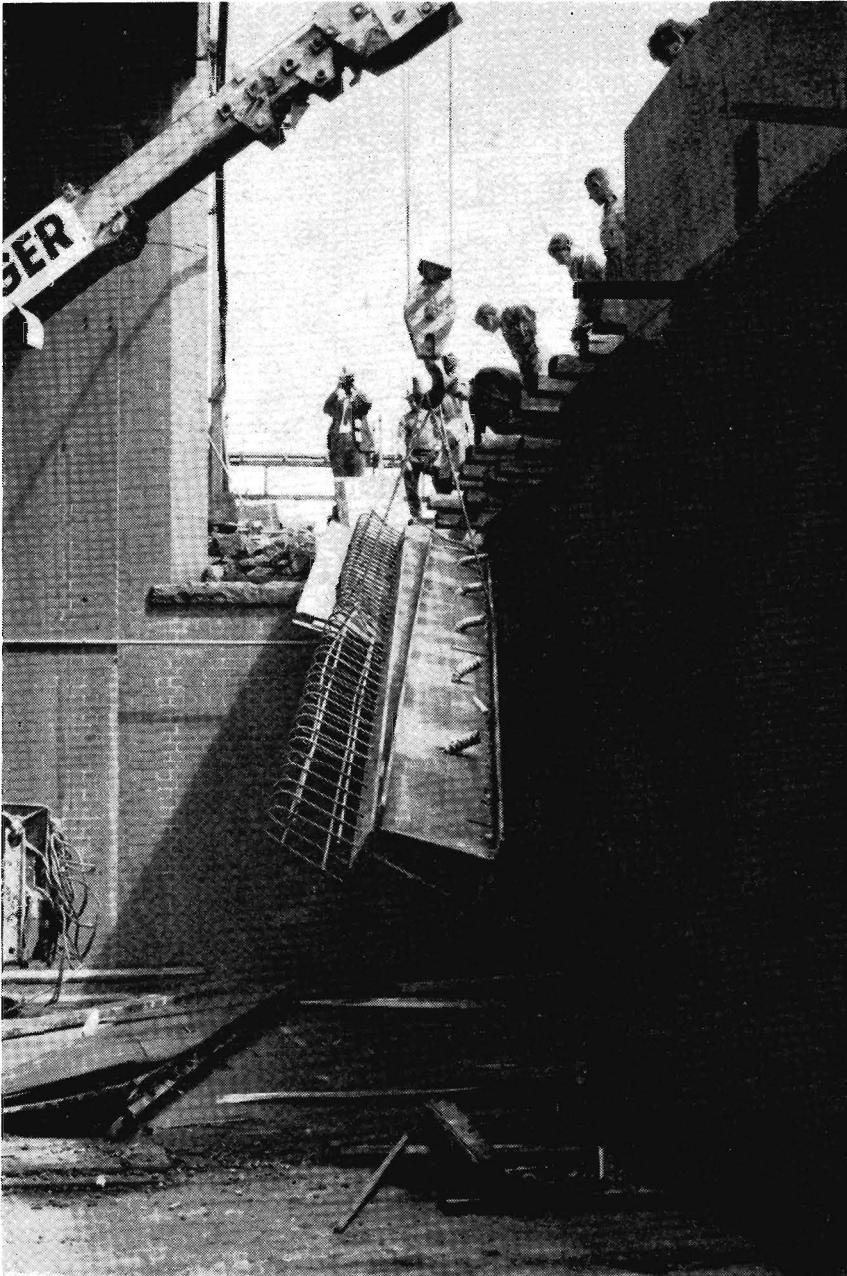
—(Mrs.) G. Simons, Russell Street, Balgowlah, N.S.W., writing to the Commissioners.

Geelong

Will you and your staff please accept my warm thanks for your courteous and efficient service given to me this month (September) while I was on circuit duty at Geelong. I appreciated your kind attention.

—Judge T. G. Rapke, writing to the Stationmaster, Geelong.

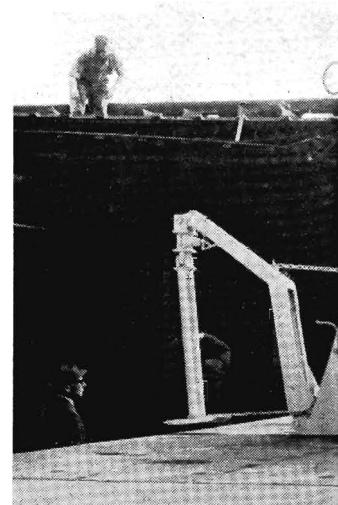
ANOTHER BRIDGE HIT

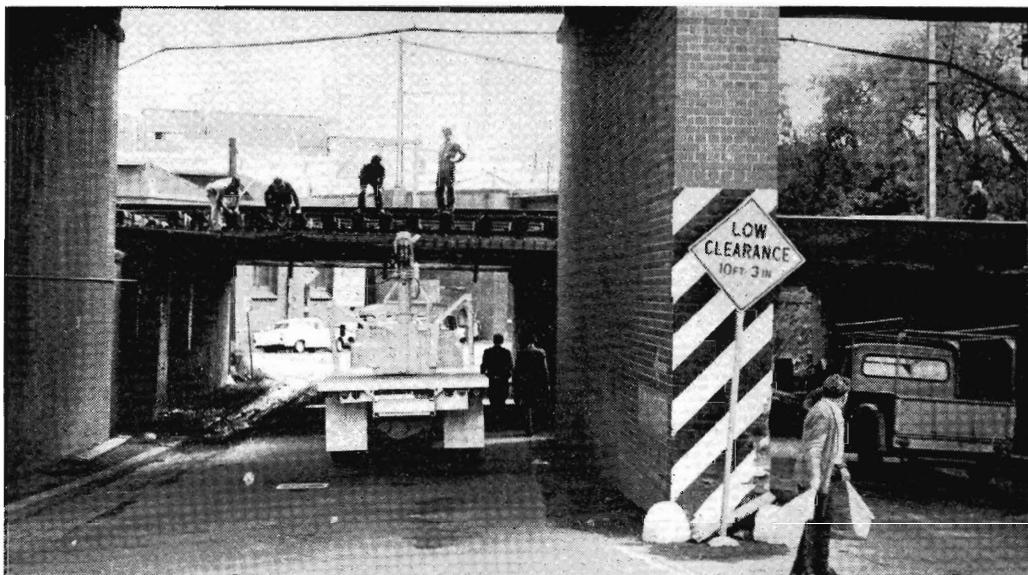


Lending point to the concern with which road vehicles are occurring on November 6 with South Kensington. Two railw were knocked off it, both we time. The section of track wa

Pictures show the crane w

"Victoria could face a serious road vehicles carrying over-h to drivers of all road vehicle height limit." (Chairman o

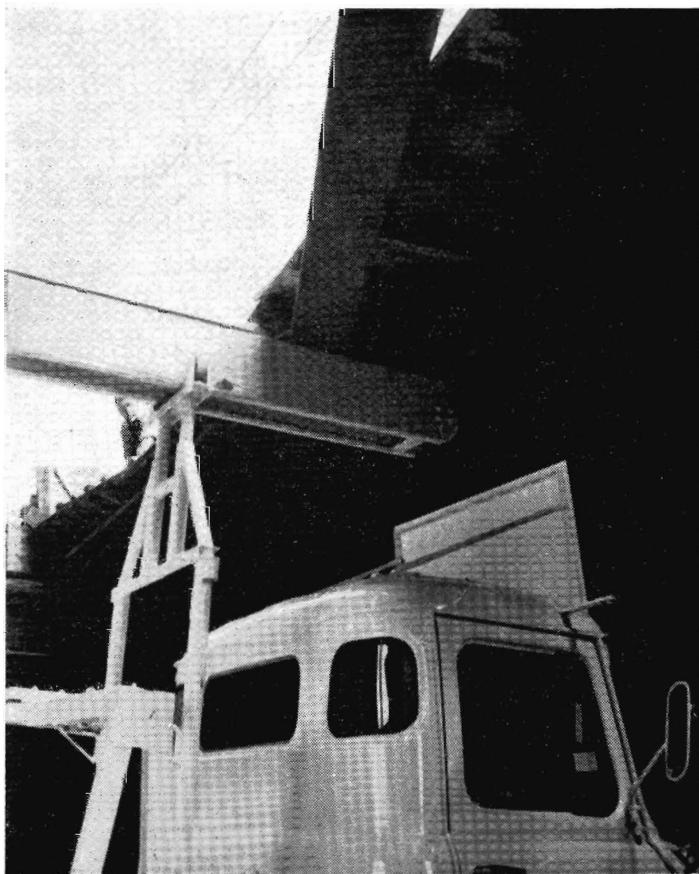
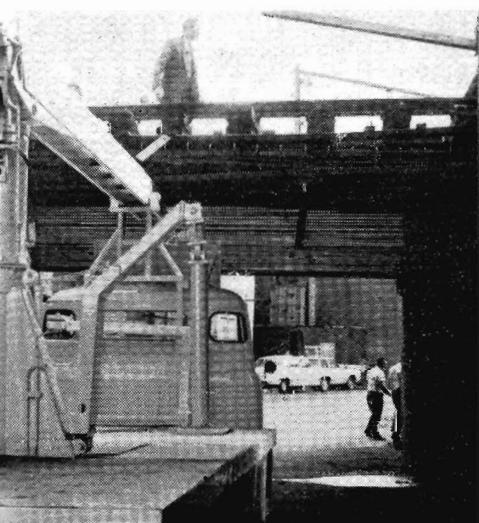




expressed by the Commissioners at the frequency of damaging railway bridges, was the accident that a mobile crane hit a rail bridge at Lloyd Street, when two men who were working on the bridge at the time, one was taken to hospital and one remained there for some time for about five hours.

under the bridge, and repairs being made.

incident if drastic action is not taken to police heavy loads, and bring home a sense of responsibility to the drivers though their loads may be within the legal limits (Commissioners in September "News Letter")



SOIL FOR CRICKET PITCHES

More than a thousand yards of Merri Creek soil, for making turf cricket pitches, are leaving Broadmeadows, by train, for Mildura, Ballarat, Ararat, Cobram, and other Victorian, Riverina and interstate cities and towns.

The entire movement should be completed by the middle of next month.

The soil is supplied during spring and summer in readiness for the following cricket season, because in winter and wet weather, it becomes greasy and cannot be worked.

Merri Creek soil has been used for making top-class cricket pitches for more than a century. It is particularly suitable because of ideal binding qualities.

When making a new pitch, the area is dug to a depth of about 6 in. and filled with Merri Creek soil. Generally, the only follow-up needed is a top dressing at the start of each season.

It is thought that Merri Creek soil came from an early volcanic overflow. It derives its name from the creek's course that it follows.

Railer of the soil, Mr. Frank Trippit, said that it is found only in seams up to 10 ft. wide.

"After 20 years in the business, seams are easy to pick," Mr. Trippit said. "In the Somerton-Campbell-field area, where I work, the soil has a depth of about 18 in.; below this is bluestone".

The family business of supplying Merri Creek soil for turf cricket pitches was started by Mr. Trippit's father, who was a curator of sporting arenas for 42 years—27 at Melbourne University and 15 at North Melbourne Cricket Ground.

Mr. Trippit estimates that nearly 300 Victorian cricket grounds use this type of soil for their pitches.

Merri Creek soil has been travelling by train for the past 20 years.

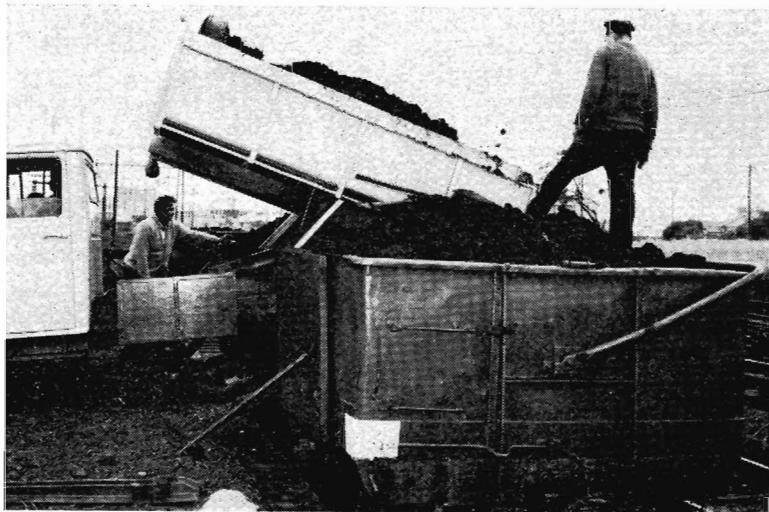
"For the long hauls to country and interstate destinations, it's the only way to move it" Mr. Trippit added.

Commenting about the quality of pitches that Merri Creek soil made, Mr. Joe Plant, South Melbourne Cricket Club coach, said that it was the best type yet found for cricket pitches.

"It is a freak soil, that grows grass well and will stand heavy rolling. There is no problem about flaking, which means that play is not affected," Mr. Plant said.

The New South Wales brown Bulli soil is the nearest in quality to Merri Creek soil.

"However, no one would seriously consider making a top-class turf pitch with a substitute, when Merri Creek soil is available", added Mr. Plant.



Merri Creek soil being tipped into a wagon at Broadmeadows. Soon it will be used to make first-class turf cricket pitches.

FAIR PLAY?

The Commonwealth Government recently voted a record \$1,252m. for use by the States on road construction.

The same Government votes \$65m. annually to maintain and operate all Department of Civil Aviation airports and navigational facilities, recouping only \$25m. in air navigation charges, fuel tax and miscellaneous DCA revenues (since then air navigation charges have been increased by 10 per cent).

No such Commonwealth money, excepting grants made for special projects such as the Sydney-Perth standard gauge line, is directly paid to the railway industry, still a considerable carrier of the nation's freight and passenger traffic.

Australia's seven railway networks constitute by far the country's biggest single industrial undertaking. They employ more than 122,000 people, and in 1967/68 paid out more than \$371m. in salaries and wages.

In an era of highly competitive international trading, efficient railways are of vital importance to the export effort of a country like Australia where inland productive regions are widely separated, and some 70 per cent of exports are from the rural sector.

The introduction of new or up-graded railway lines and modern operating equipment is particularly

important at this stage in Australia's development.

Mineral production is booming, the use of door-to-door container services is increasing, and there is a pressing demand for rapid, urban transport systems.

Railway expansion is geared to the amount of money available for rolling stock, maintenance, plant and equipment, signalling and communications, stores and materials and the like.

Traditionally cast in a public utility role, with all that is implied in providing essential services, and with a freight rate structure tailored to assist both domestic and export industry, the government-owned railways, inevitably, are not profit-oriented. Their expectation of profit is decreased by the impost of capital debt and other statutory charges that must be met; yet the Australian railway industry consistently manages to earn more revenue than it pays out in working expenses.

In 1967-68 for instance, Australia's seven government-owned railway systems earned \$531m., but paid out \$485m. in working expenses. Railway surpluses or deficits are related more to their capital debt obligations than to their efficiency.

Surely the railway industry has earned the right to the same kind of assistance regularly accorded its competitors.



Witchetty grubs in a tree that has just been cut.

TO CONFERENCE BY TRAIN

The management of the Portable Division of Fire Fighting Equipment considered that interstate train travel for their representatives to attend a sales conference in Sydney would be equally time-saving and more economical than air travel.

Consequently, a team of seven left for Sydney by *Southern Aurora* on Monday, November 3.

After attending the conference,

the team returned by *Southern Aurora* on Tuesday, November 4, and arrived in Melbourne early the next day. The one-day exercise made no greater encroachments into working time than would have been made by air travel.

The sales team presented the crew of *Southern Aurora* with fire extinguishers as souvenirs of the occasion.



Sales team from Fire Fighting Equipment present train crew of *Southern Aurora* with fire extinguishers.

THEY TASTE LIKE COCONUT

Thousands of witchetty grubs are leaving Gisborne and Macedon railway stations, to meet the Christmas demand from holidaying anglers. The grubs travel by passenger train mainly to Shepparton, Robinvale, Echuca, Mildura, Swan Hill and Castlemaine, and to sports stores in Melbourne, Northcote and Bell.

Gisborne residents, Messrs. Arthur Rosier and Des Tranter, working as a team, have been grubbing for the past 12 years. They work in the Trentham area, under licence from the Forests Commission. The grubbers need to collect about 300 witchetty grubs each day, to make their unusual job a paying proposition. Up to 200 grubs have been found in one tree, but generally each grub tree averages 50.

The two men grub-hunt for almost nine months of the year, but holiday periods bring big demands from anglers yearning to land large cod or trout. When the men arrive at one of their secret grub spots, suitable eucalypt trees are selected by a giveaway bulge in the trunk which indicates the likely presence of witchetty grubs. After felling and cutting the tree, the grubs are collected. They vary in size, with the biggest about 4 in. long.

Witchetty grubs take seven years to develop from the egg to maturity, when they bore out of the tree as a beetle. In that time, the grub has eaten away much of the wood and rendered the tree useless except as poor quality firewood.

About 200 live grubs, insulated in green sawdust, are placed in each carton for the rail journey. Packed this way, the grubs will stay alive for up to three months. Separation is necessary to prevent the grubs killing each other.

"We've never had any worries about the Railways getting them there," Arthur Rosier said. "Besides, witchetty grubs, travelling by train, cost only half normal parcels rate."

Both men said they had eaten witchetty grubs, but preferred more conventional food.

"They taste like coconut," said Arthur Rosier.

"Washed down with an ale, they're not so bad", added Des Tranter.

"Some people particularly at Robinvale and Mildura, swallow the grubs live," continued Arthur. "But the best way to enjoy a meal with a difference is to grill them on a piece of tin, with the fire underneath."

Beg pardon, sonny?

It was in the early 'twenties, and the lad porter at Camberwell was checking the tickets of passengers waiting on the platform. He approached a short, elderly man and asked to see his ticket.

"Beg pardon, sonny?" said the little man with one hand cupped to his ear.

The youth tried again, with the same result. And again. Giving it up as a hopeless job, he moved on to the next group.

On returning to the booking office, the lad was told who the little man was. He was "Billy" Hughes, then Prime Minister of Australia.

This was one of the many amusing

stories recalled by Mr. A. J. Taylor when he retired after being Northcote's stationmaster for the past 15 years.

While working as an operating porter on the Meringur line, he married, and as he was well known, the couple received many wedding presents. Unfortunately a lot of the well meaning people had the same idea. So the couple found they had about two dozen cockatoos and galahs among their presents.

An exciting incident in Mr. Taylor's career occurred at St. Albans, about 20 years ago, when a deranged man started shooting at people with a .22 rifle. Mr. Taylor approached the man, talked quietly to him, and eventually was able to disarm him.

Best wishes



At Newport Workshops, Shift Supervisor D. O. Jones (left) gives a card conveying the best wishes of his colleagues to Watchman Leo Hayes who recently retired after having been watchman for the past 10 years out of the 28 years he was at the Workshops. (Photograph: E. Reid)



At Northcote, Mr. M. J. McKeagney, assistant stationmaster, (left) is making a presentation on behalf of the staff, to Mr. Taylor, retiring stationmaster.

From Thailand



Mr. Anek Kungsadal, a foreman in the Tool Centre Shop of the State Railway of Thailand, is on a tour of the Australian railways to study the use of machine tools and other engineering practices on the Australian systems. He is shown (left) in the Tool Room at Newport Workshops, watching Apprentice Fitter and Turner S. Chioda operating a thread grinder.

Led the parade



When Mr. T. W. J. (Jimmy) Fleming retired after 34 years as a metal polisher in the Electroplating Shop at Newport, he told *News Letter* that he had the distinction of leading the parade at the Showgrounds when night trotting began there over 20 years ago. He had drawn No. 1 in the first race and therefore led the parade. In those days, he owned and trained a number of trotters. Mr. Fleming (second from left) is discussing one of his last jobs with some of his friends at the Shop.

* * * *

Another good thing about telling the truth, you don't have to remember what you say.

POMBORNEIT CENTENARY

On January 1-2, 1970, Pomborneit will be celebrating the centenary of education in the district. As many of the pupils who attended the Pomborneit school (No. 1031) were children of railwaymen, the local organizing committee would like any railway people who are interested to contact the Publicity Officer (Mrs. R. Boyd), c/o School Centenary Committee, Pomborneit.

TRAIN PESTS

In the Camberwell Court recently, three youths who pleaded guilty to using indecent language on a train travelling between Auburn and Camberwell stations, were each fined \$20. One of them was also charged with offensive behaviour, and fined \$10.

* * * *

In the Kyneton court, a man who had caused a disturbance on the Melbourne-Bendigo train was convicted on three charges, and sentenced to a total of three months imprisonment.

FROM R.S.P.C.A.

"Complete co-operation is afforded the Society's officers by the Livestock department of the Victorian Railways. Mr. G. Caldwell and his staff are thanked for unflinching courteous help." . . . *from annual report of the Royal Society for the Prevention of Cruelty to Animals*

NEWS LETTER REGRETS TO RECORD THE FOLLOWING DEATHS

ROLLING STOCK BRANCH

Dixon, H. W., North Melb., 19/10
Ficarra, G., Jolimont, 28/10
Pickett, L., Shelter Shed, 23/10
Rakovitis, A., Newport, 16/10
Rice, H. H., Ballarat Loco, 28/10
Rowe, R. J., Bendigo North, 19/10
White, R. I., Ballarat North, 22/10

WAY AND WORKS BRANCH

Michalowsky, R. L., Balranald, 8/10

TRAFFIC BRANCH

Eklund, W. D. K., Flinders St. Yd., 20/10
Krzywda, A., Spencer Street, 18/10
Moran, T., C/- Metro. Supt., 3/11
Quinn, C. D., Melbourne Goods, 15/10
Scrivener, W. A., Bendigo, 18/10
Turcato, G., Melbourne Goods, 3/11

REFRESHMENT SERVICES BRANCH

Duggan, F. (Mrs.), Dining Car Depot, 30/10

RETIREMENTS

ROLLING STOCK BRANCH

Dimowski, M., Geelong, 31/10
Grant, C. J., Newport, 24/10
Lentini, G., North Melbourne, 31/10
Love, H., South Dynon, 29/9
Moloney, T., North Melbourne, 25/10
McKay, L. J., Jolimont, 11/8
Nestor, T. F., Ballarat North, 30/1
Putnins, R., Newport, 5/11
Ray, W. C., Newport, 17/11
Siefert, D. W., E. R. Depot, 3/9
Southern, E. T., Geelong, 26/10
Sutton, H. A., Bendigo Loco, 10/10
Turner, H. P., Jolimont, 3/1

WAY AND WORKS BRANCH

Brennan, J. F., Jewell, 22/1
Carson, W. D., Donnybrook, 3/1
Cowan, E. J. T., Korumburra, 24/1
Egan, M., Leongatha, 26/1
Fields, R. A., Kooloonong, 7/1
Langer, W. G., Oaklands, 3/9
Laterza, M., Special Works, 28/10
Marodi, J., Korumburra, 23/10
Moloney, F. J., Wodonga, 27/1
Morley, J. G., South Dynon, 4/1

Qualtrough, H. E. M., Bendigo, 25/1
Quick, S. H. J., Glenroy, 30/1
Ratcliffe, W. L., Spotswood, 14/1
Robertson, J. D., Numurkah, 3/10
Vacirca, G., Flinders Street, 31/12
Watson, W. F., Yallourn, 8/10
Woolley, G. F., Head Office, 16/1

TRAFFIC BRANCH

Barber, T. H., Dynon, 3/1
Hartigan, G. B., Spencer Street., 7/1
Lunn, A. E., Head Office, 11/1
McLean, P. J., C/- Metro. Supt., 7/1
O'Rourke, L. T., Traralgon, 29/9
O'Toole, B. I., Geelong, 23/1
Pearce, H. J., Ballarat, 28/1
Power, C. (Miss), Flinders St., 7/10
Zoides, G., Melbourne Goods, 18/1

STORES BRANCH

Sullivan, E., Head Office, 9/10

REFRESHMENT SERVICES BRANCH

Despard, H., Bookstalls Division, 28/10
Eller, S., Dining Car Depot, 25/3

MONEY FOR WORKS

Increased train capacity on the Sydney and Adelaide lines, another 200 modern freight wagons, more diesel-electric locomotives and continuation of up-grading country rail tracks are features of the Railways 1969-70 loan works programme.

The State Treasury has allotted \$16,686,000 for works.

The largest amount will be used to relay 195 miles of country lines and 5 miles in the metropolitan area to give smoother train running.

Modernization of the Melbourne Yard will be continued and will near completion this financial year; Australia's first automated hump shunting incorporated in the yard will operate early in 1970. This project will provide a speedier freight service to and from country areas.

Works on the Box Hill group of lines will also be continued. Two more tracks will be provided between Jolimont Junction and Richmond and an extra track from Burnley to Hawthorn. The existing third track will be extended from East Camberwell towards Mont Albert. These projects, together with the underground rail loop, will eventually mean increased peak-hour services and more express running.

A continuous programme of rolling stock renewals must be carried out to replace outworn and obsolete locomotives and vehicles—to meet today's intensely competitive conditions, which call for higher speeds and specialized equipment, tailored to meet the needs of particular classes of traffic.

A further 200 bogie rail wagons of various types, but with the emphasis

on the booming container traffic, will be built at the Newport, Ballarat North, and Bendigo North Workshops.

Deliveries of diesel-electric locomotives, already on order, will start this financial year.

Crossing loops, to be built on the Sydney line (at Tallarook) and the Adelaide line (between Kiata and Salisbury) will permit better time-keeping and allow more trains to run on the intercapital routes. These loops are essential because record-breaking freight tonnages are being carried on these lines, and traffic is expected to increase further.

In other works, the Geelong line duplication will be continued.

Grade separation for which the Railways are the construction authority will be completed at Somerville Road, Yarraville, and Princes Highway, Traralgon, while new works will start at North Road, Huntingdale.

The programme of boom barrier and flashing light installations at level crossings will also continue. Already 69 crossings are equipped with boom barriers and 310 have flashing lights.

Included in the loan programme is a special allotment towards buying two air-conditioned rail-cars for the Ararat-Portland line and for seven replacement carriages for *Southern Aurora*.

In conjunction with the South Australian Railways, new carriages will be built for *The Overland*. Included will be a club carriage which should be added to the train's consist in 1970.

1970

Terminating dates of Pay Fortnights shown in **Blue**
 Public Holidays shown thus — ○
 (Good Friday 1971 — April 9)

1970

	JANUARY					FEBRUARY					MARCH						
Sun.	...	4	11	18	25	1	8	15	22	...	...	1	8	15	22	29	...
Mon.	...	5	12	19	26	2	9	16	23	...	...	2	9	16	23	30	...
Tues.	...	6	13	20	27	3	10	17	24	...	...	3	10	17	24	31	...
Wed.	...	7	14	21	28	4	11	18	25	...	...	4	11	18	25	...	...
Thur.	1	8	15	22	29	5	12	19	26	...	...	5	12	19	26	...	...
Fri.	2	9	16	23	30	6	13	20	27	...	...	6	13	20	27	...	...
Sat.	3	10	17	24	31	7	14	21	28	...	...	7	14	21	28	...	...
	APRIL					MAY					JUNE						
Sun.	...	5	12	19	26	...	3	10	17	24	31	...	7	14	21	28	...
Mon.	...	6	13	20	27	...	4	11	18	25	...	1	8	15	22	29	...
Tues.	...	7	14	21	28	...	5	12	19	26	...	2	9	16	23	30	...
Wed.	1	8	15	22	29	...	6	13	20	27	...	3	10	17	24	...	...
Thur.	2	9	16	23	30	...	7	14	21	28	...	4	11	18	25	...	...
Fri.	3	10	17	24	...	1	8	15	22	29	...	5	12	19	26	...	...
Sat.	4	11	18	25	...	2	9	16	23	30	...	6	13	20	27	...	...
	JULY					AUGUST					SEPTEMBER						
Sun.	...	5	12	19	26	...	2	9	16	23	30	...	6	13	20	27	...
Mon.	...	6	13	20	27	...	3	10	17	24	31	...	7	14	21	28	...
Tues.	...	7	14	21	28	...	4	11	18	25	...	1	8	15	22	29	...
Wed.	1	8	15	22	29	...	5	12	19	26	...	2	9	16	23	30	...
Thur.	2	9	16	23	30	...	6	13	20	27	...	3	10	17	24	...	...
Fri.	3	10	17	24	31	...	7	14	21	28	...	4	11	18	25	...	...
Sat.	4	11	18	25	...	1	8	15	22	29	...	5	12	19	26	...	...
	OCTOBER					NOVEMBER					DECEMBER						
Sun.	...	4	11	18	25	1	8	15	22	29	...	...	6	13	20	27	...
Mon.	...	5	12	19	26	2	9	16	23	30	...	...	7	14	21	28	...
Tues.	...	6	13	20	27	3	10	17	24	...	...	1	8	15	22	29	...
Wed.	...	7	14	21	28	4	11	18	25	...	...	2	9	16	23	30	...
Thur.	1	8	15	22	29	5	12	19	26	...	...	3	10	17	24	31	...
Fri.	2	9	16	23	30	6	13	20	27	...	...	4	11	18	25	...	...
Sat.	3	10	17	24	31	7	14	21	28	...	...	5	12	19	26	...	...

Institute News

REFLECTIONS ON THE ROAD

"Service". That's a word with many meanings. To some it brings to mind "obligations", without allowance for the pleasure of helping others.

It is this feeling of pleasure that comes to the small parties of V.R.I. Councillors when they give up their personal leisure time to be at the annual meetings at the 24 country centres.

Such contact between country and city is good for the V.R.I.; for 34 per cent of the Railway staff are in the country.

Railwaymen—their wives and families, too—are involved in service

to the community, not only by virtue of their employment, but for the part they often play in shire and community affairs.

As we travel to these country functions to meet and talk with the local V.R.I. people, we realize how quietly and unobtrusively these people give service to others.

At one country centre recently, two young railwaymen arrived on temporary transfer—15 minutes before the V.R.I. annual meeting. The informal mateship of the Institute found them friends immediately.

Forty two per cent of the Institute members are under 40 years; and it is this age group for which V.R.I. service is being tailored.

Further reflections from Institute travel reveal that country members are more prone to act positively than to question and criticize.

At this time of the year, the need for service is even more dominant; we face the demands of the festive season, and the hazards of bush fires.

To you all from Council—a Very Happy Christmas and a prosperous New Year.

—A Councillor

captain of the first eleven and president of the club. Ron has been a valuable member of three V.R.I. intersystem carnival sides, in 1953, 1957 and 1969.

Basketball

A full report of the Intersystem Basketball Carnival recently held in Hobart will appear in the January issue of *News Letter*.

Accommodation

Country members who are thinking of competing in country Bowls, Cricket or Tennis Weeks, and require accommodation in Melbourne should write to me, C/- V.R.I., Flinders Street, or give me a ring on Auto. 2445. Bed-and-breakfast accommodation is available to V.R.I. members at two leading Melbourne hotels at a greatly reduced tariff. This concession applies throughout the year, and country members coming to Melbourne for holidays, over-night stays, etc., should take advantage of this service.

Golf

The 13th North-eastern V.R.I. Golf Championship, played recently at the Golden Vale Golf Course, Benalla, was won by our old friend Jack Manning with a score of 78 off the stick. Jack, one of the most consistent golfers in the service, is always a strong competitor at Rosedale during our country golf week, and has been a regular member of our teams in inter-system golf carnivals. Other major winners in the tournament were G. Armstrong, who won the J. H. Jupp Memorial Trophy, J. Dawkins, who took the A grade handicap (no comment), and R. Wapling—the B grade handicap. The organization was in the capable hands of Peter Hale and his committee who handled the fixture with their usual efficiency.

SPORT

by OSS. KEATING

It looks as if one of the essential requirements for an appointment to the Rolling Stock Safety Section is a keen interest in sport—one has only to look at the record of three members of this section to realize this. First, there is Bill Cox (Ex Sub-foreman, Jolimont Workshops). Bill played football for Prahran in the late 'forties, and then with Essendon Stars in the Essendon District League. When his playing days were over, he maintained his interest in the game by becoming a trainer with Old Scotch Collegians. After seven years with Collegians, he transferred to the Essendon League Club and is now head trainer with the mighty bombers. Bill also had a pretty good turn of speed in his younger days, and, in 1953, won the Heidelberg Athletic Club's St. Andrew's Gift.

Then there's Bert Clancy, a foreman fitter at Newport Workshops. In his younger days he played junior football with Spotswood, and then with Ararat, but of later years he has concentrated on bowls. In his third year at the game, Bert has won a place in Newport's A 4 team—a pretty good effort for a player with only three years experience. Off the bowling green he spends a good deal of time looking after the wishes

of the ratepayers of Williamstown, having been a member of the local municipal council since 1964.

Finally, there's Ron Jenkins who before joining this section, was a sub-foreman car builder at Ballarat North Workshops. Ron has starred at cricket, baseball and lacrosse, but cricket was his first love. He played with Williamstown Sub-district Club from 1949 to 1951, with Footscray District Club in '51, '52 and '53, and then transferred to Kilsyth. During his period at Ballarat, he played with the City Club, and on his return to Melbourne returned to Kilsyth where he is at present

Rope quoits



Six Railwaymen at Bairnsdale have combined to form a very successful rope quoits team. Last year, they were runners-up in the competition held by the Bairnsdale Rope Quoits Association, and, in 1967, won the grand final. Here the team shows some of the trophies they have won: (from left) A. Russell (captain), T. Nall, C. Campbell, D. Stevens (vice-captain), K. Thompson, and D. McPherson.

NEWS LETTER INDEX FOR 1969

HERE is a page reference to selected items in the January to December issues.

	Page		Page		Page
Accident, <i>Southern Aurora</i>	34, 35	Future rail developments	.. 154	Safety of train travel	.. 34
Air transport subsidies	.. 131	Gas leak, V.R. helps	.. 13	<i>Second-class</i> to be dropped	.. 162
Annual report		Gold train from Bendigo	.. 38	Serviceton station	.. 116
Award received	.. 115	Golf, inter-system carnival	.. 84	Signal box, Dudley Street	.. 28
1968-69	.. 179	Governor's visit	.. 164	Signals, interference with	.. 35
Apprentices		Graphite for points	.. 82	South Yarra, land develop- ment	.. 131, 162
Displays	.. 83			Specialization by railways	.. 148
Vacancies	.. 115	Inquiry into the Railways	.. 147	St. Kilda line, history	.. 135
Australian railways' earnings	.. 176			Standard gauge	
Authors, three railwaymen	.. 172	Jolimont, air rights	.. 162	Achievements	.. 77
				Loading records	.. 122
Bees by train	.. 82	Lighting of suburban stations	.. 98	State Coal Mines close	.. 26
Billy Graham Crusade	.. 71	Locomotives		Strawberries go by train	.. 99
Bridges		Bought from Australian Ce- ment Ltd.	.. 155	Teleprinter section expands	.. 122
New Armco type	.. 67	New vigilance control	.. 122	Tickets	
Road vehicles hit	130, 146, 162	S class clocks 2 million	.. 163	Bus-train	.. 108
Brown, G.F., paper read at transport seminar	132, 148	Six more diesels ordered	.. 114	New all-lines	.. 18
		Lost property book at Nu- murkah	.. 101	Timber tramways	.. 10
Car park survey	.. 66	Luggage lockers, additional	.. 35	Tobacco goes by train	.. 92
Chairman comments on				Tortoises go by train	.. 147
Apprentices	.. 50	Maldon-Shelbourne line closed	163	Tracks	
Cleanliness of trains	.. 162	Melbourne Yard scheme		Aerial surveys	.. 123
Fires	.. 18	Locos for hump yard	.. 2	New maintenance system	.. 156
Loan funds anomalies	.. 66	No. 4 Shed opens	.. 2	Trains	
Press complaints genuine?	98	Pneumatic tubes	.. 38	<i>Southern Aurora</i> accident	34, 35
Road vehicles hitting bridges	130	West Tower opens	.. 3	<i>The Overland</i> , improve- ments	.. 86
Short haul traffic	.. 146	Modern railway system, needs of	132, 148	Timekeeping of	.. 115
Smoking in trains	.. 114	Morse, end of	.. 55		
<i>Southern Aurora</i> accident	.. 34	MotoRail service increased	.. 51	Underground railways, Over- seas opinion on	.. 51
Subsidy unacknowledged	.. 2				
Sydney-Perth rail link	178, 82	New Zealand rail bookings	.. 156	Vandalism, etc.,	35, 131, 146
Charlton Station	.. 119	Nhill station	.. 138	Victorian Railways Institute	
Cleaners, female, for carriages	162	Numurkah, new station build- ing	.. 100	Annual report	.. 165
Clock at Spencer Street	.. 114	Officers' Conference	.. 58	Prizes presented	.. 70
Complaint by Donald paper	.. 50			Vigilance control for loco- motives	.. 122
Containers		Pamphlets, new type	.. 146		
Early in 1842	.. 115	Perth services	.. 163	Wagons	
New line to wharves	.. 51	Picnic, South Dynon Loco	.. 39	Fire attack	.. 52
Overseas trends	.. 115	Ponies from State Coal Mine	.. 42	For containers	.. 59
Show	.. 118			Walhalla line, short section may be opened	.. 102
Tallow	.. 156	Races and the Railways	.. 173	Walkie-talkies used	.. 71
V.R. in 1925	.. 120	Rail-road bus, N.S.W.	106, 168, 184	<i>Weekly Notice</i> 75 years old	.. 107
Credit cards	.. 123	Railway system, needs for modernity	.. 132	Wheat harvest, Minister's tour	23
Current, electric, varieties of	.. 58	Refreshment rooms, 50 years of operation	.. 90		
		Reservation Bureau changes	.. 150		
Dog lost	.. 98	Rochester station	.. 36		
		Roses go by train	.. 86		
Electric street railway terminus	75	Rosstown Junction Railway	.. 75		
Electric current varieties	.. 58	Royal Show exhibit	.. 151		
Electrification, 50th annivers- ary	.. 96				
Fares, new	.. 22				
Female carriage cleaners	.. 162				
Fires damage railways	.. 19				
First-aid finals	.. 170				
Fixture cards, racing	.. 163				
<i>Folklore of the Australian Rail- waymen</i> reviewed	.. 87				
Freeways					
In U.S.A. (letter)	.. 99				
Railway on	.. 82				
Wreck cities	.. 99				